

LIFE-SAFETY INFORMATION

APPLICABLE CODES
NFPA 101 LIFE-SAFETY CODE 2015

OCCUPANCIES MERCANTILE (CHAPTER 31) & STORAGE (CHAPTER 42); CONSTRUCTION TYPE II B
MIXED OCCUPANCIES (REFERENCE CHAPTER 6)

OCCUPANT LOAD FACTOR (REFERENCE TABLE 7.3.1.2)

MERCANTILE: 1,000 S.F.	30 SF GROSS / OCCUPANT =	30
STORAGE: 5,000 S.F.	500 SF GROSS / OCCUPANT =	10

TOTAL OCCUPANTS = 40

CLASSIFICATION OF HAZARD OF CONTENTS
(REFERENCE: OCCUPANCY CHAPTER AND 6.2.2; SPECIFY LOW, ORDINARY, OR HIGH) ORDINARY HAZARD

CONSTRUCTION TYPE(S) (REFERENCE: CHAPTER 8, TABLE A.8.2.1.2 AND COMMENTARY TABLE 8.1 IN HANDBOOK)
II B

MINIMUM EXIT SEPARATION DISTANCE FOR REMOTELY LOCATED EXITS
(REFERENCE: SECTION 7.5; SPECIFY 1/2 OR 1/3 DIAGONAL DISTANCE OF AREA SERVED)

1/2 DIAGONAL = 66 FT SECTION 42.2.4.1 LOW AND ORDINARY HAZARD STORAGE OCCUPANCIES & SECTION 31.2.4.2 FOR BUSINESS ALLOWING FOR SINGLE MEANS OF EGRESS

MAXIMUM DEAD-END CORRIDORS (REFERENCE: OCCUPANCY CHAPTER AND TABLE A.7.6) 50

MAXIMUM COMMON PATH OF TRAVEL DISTANCE (REFERENCE: OCCUPANCY CHAPTER AND TABLE A.7.6) 75

MAXIMUM TRAVEL DISTANCE TO EXITS (REFERENCE: OCCUPANCY CHAPTER AND TABLE A.7.6) 200

EXTINGUISHMENT REQUIREMENTS SPRINKLER (NOT REQUIRED)

DETECTION, ALARM, AND COMMUNICATION SYSTEMS NO 2.9.4.1 - 42.3.4.4

ALLOWABLE HEIGHT AND BUILDING AREA PER IBC EQUIVALENT CONSTRUCTION TYPE

BUILDING CODE

APPLICABLE CODES
IBC 2015

OCCUPANCY M (MERCANTILE), S-1 (REPAIR GARAGE); MIXED OCCUPANCIES (IBC 2015 CHAPTER 10)

OCCUPANT CALCULATIONS (TABLE 1004.1.2)

MERCANTILE	1,000 S.F.	30 GROSS/OCCUPANT	30 OCCUPANTS
S-1 REPAIR GARAGE	5,000 S.F.	500 GROSS/OCCUPANT	10 OCCUPANTS

TOTAL OCCUPANTS 40 OCCUPANTS

CONSTRUCTION TYPE(S) (TABLE 503)
II B (SECTION 503)

ALLOWABLE HEIGHT AND BUILDING AREA LIMITED BY TYPE OF CONSTRUCTION

MAXIMUM HEIGHT IN FEET (SECTION 503 & 504, TABLE 504.3)	55
MAXIMUM NUMBER OF STORIES (SECTION 503 & 504, TABLE 504.4)	2
MAXIMUM AREA IN SQUARE FEET, NON-SPRINKLED (SECTION 503, 506 & 507, TABLE 506.2)	17,500

WIND SPEED DESIGN REQUIREMENTS

THIS BUILDING SHALL BE DESIGNED WITH IBC SEC 1609 AS A FULLY ENCLOSED BLDG USING THE FOLLOWING INFORMATION:

WIND DESIGN DATA:

DETERMINATION OF WIND LOADS SHALL BE IN ACCORDANCE WITH IBC SEC 1609.3 (1), (2), OR (3) DEPENDING ON THE RISK CATEGORY

WIND SPEED V_{ult} = 143 MPH (IBC FIG 1609.3(1))

NOMINAL DESIGN WIND SPEED V_{asd} = 111 MPH ($V_{ult} \times (0.6)^{1/2}$)

RISK CATEGORY: CATEGORY II BLDG SURFACE ROUGHNESS = C

TOPOGRAPHIC FACTOR = 1 EXPOSURE = C

INTERNAL PRESSURE COEFFICIENT (ASCE 7-10 TABLE 26.11-1): ± 0.18

LIVE LOADS (IBC SEC 1607)

GARAGE, AUTO (IBC TABLE 1607.1):	40 PSF
OFFICE LOBBIES & CORRIDORS 1ST FLOOR	100 PSF
OFFICES (IBC TABLE 1607.1):	50 PSF
ROOF LIVE LOADS (IBC TABLE 1607.1):	20 PSF UNIFORM, 300 LB CONCENTRATED

SNOW LOADS (IBC SEC 1608):

GROUND SNOW LOAD (IBC FIG 1608.2):	5 PSF
------------------------------------	-------

FLOOD ZONE INFORMATION

BASED ON THE LSU FLOOD MAP THIS PROPERTY IS IN ZONE X BASE FLOOD.

FIRM, COMMUNITY NO. 22103C04#5F DATE: 04/30/2008

FLOOD ZONE: X BASE FLOOD ELEVATION: N/A

LIFE-SAFETY LEGEND

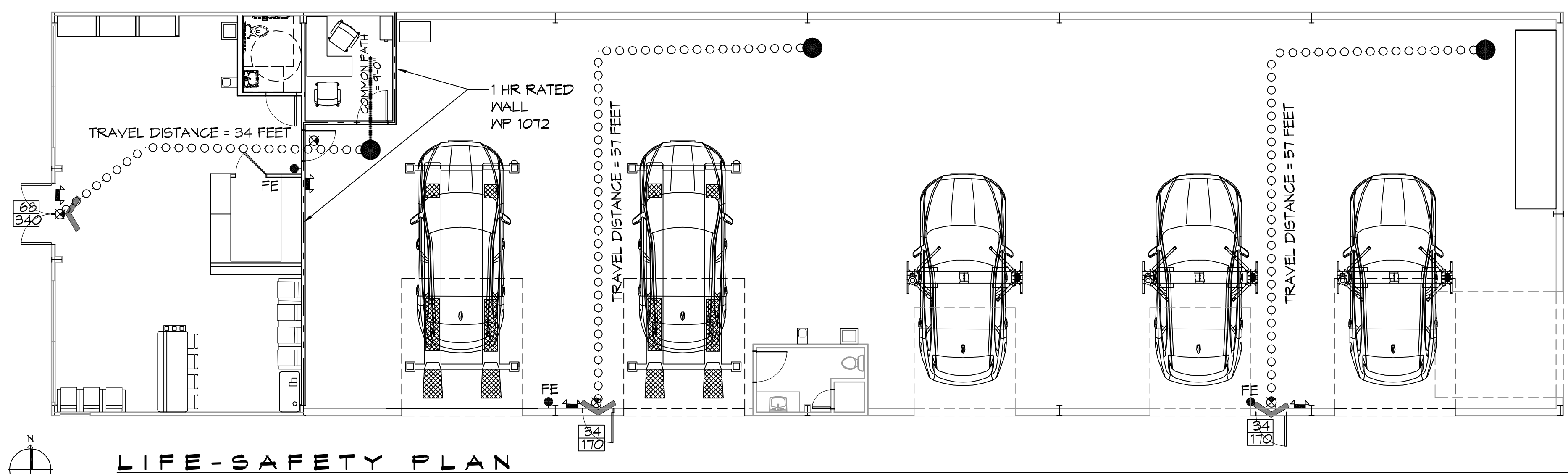
SYMBOL	DESCRIPTION
	EXITS
	DOOR WIDTH/EGRESS CAPACITY
	EXIT LIGHT
	FIRE EXTINGUISHER W/ MALL MTD BRACKET
	TRAVEL DISTANCE
	DECISION POINT

PROJECT STATISTICS

EXISTING 6000 SQ. FT. BUILDING WILL BE USED FOR A REPAIR SHOP

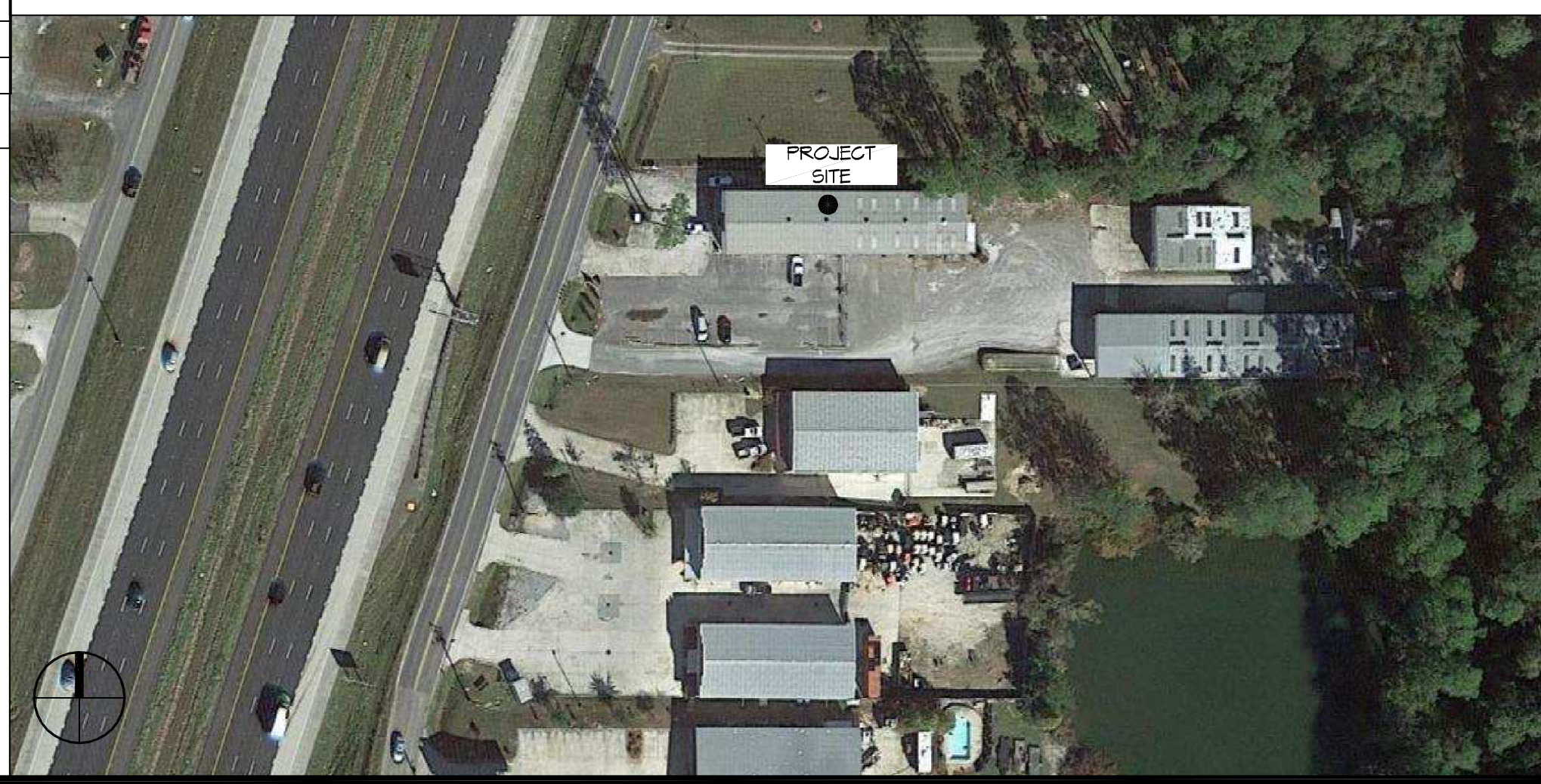
PROJECT LOCATION:
T22 EAST I-10 SERVICE ROAD
SLIDELL, LA.

OWNER:
DOMINATE FLEET LLC



LIFE-SAFETY PLAN
SCALE: 1/8" = 1'-0"

VICINITY MAP



GENERAL NOTES

- ALL MATERIALS AND WORK, INCIDENTAL TO THE CONSTRUCTION OF THIS PROJECT, SHALL CONFORM TO ALL GOVERNING CODES, AND REGULATIONS OF AGENCIES IN AUTHORITY.
- CONTRACTOR SHALL PROVIDE ALL PUBLIC PROTECTIONS NECESSARY AS REQUIRED BY LAW.
- THE DRAWINGS AND ANY SUBSEQUENTLY ISSUED ADDENDA, AMENDMENTS OR SUCH CHANGE ORDERS APPROVED BY THE OWNER AND THE CONTRACTOR ARE PART OF THESE CONTRACT DOCUMENTS.
- DO NOT SCALE DRAWINGS. CONSULT WITH THE ENGINEER REGARDING ANY ITEMS IN THE CONTRACT DOCUMENTS THAT REQUIRE CLARIFICATION.
- TRASH SHALL BE REMOVED FROM THE SITE NOT LESS THAN TWICE MONTHLY.
- THE GENERAL CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO COMMENCING WORK AND REPORT ANY AND ALL DISCREPANCIES TO THE ARCHITECT.
- CONTRACTOR VEHICLES AND EQUIPMENT NECESSARY FOR CONSTRUCTION MAY BE PARKED ON THE SITE. OTHER VEHICLES PARKED ON THE SITE REQUIRE THE OWNER'S PERMISSION.
- ALL MATERIALS/EQUIPMENT SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS. WORK NOT CONSISTENT WITH MANUFACTURER'S RECOMMENDATIONS WILL BE REJECTED BY OWNER/ARCHITECT.

SHEET INDEX

SHEET #	SHEET TITLE
G101	GENERAL INFORMATION SHEET
G102	ACCESSIBILITY INFORMATION
C101	SITE PLAN
A101	FLOOR PLAN
A102	BUILDING ELEVATIONS
P101	PLUMBING PLAN
E101	ELECTRICAL ONE LINE DIAGRAM
E102	LIGHTING AND POWER PLAN

DAMMON ENGINEERING, INC.
LOUISIANA & MISSISSIPPI

Chief Engineer: Brian Mistich, PE
554 Old Spanish Trail
Slidell, LA 70668
www.dammonengineering.com
info@dammonengineering.com
PH: 985.649.8832

REVISIONS	DATE	DESCRIPTION

SEAL:

MERKE CAR CENTER

T22 EAST I-10 SERVICE ROAD
SLIDELL, LA 70668

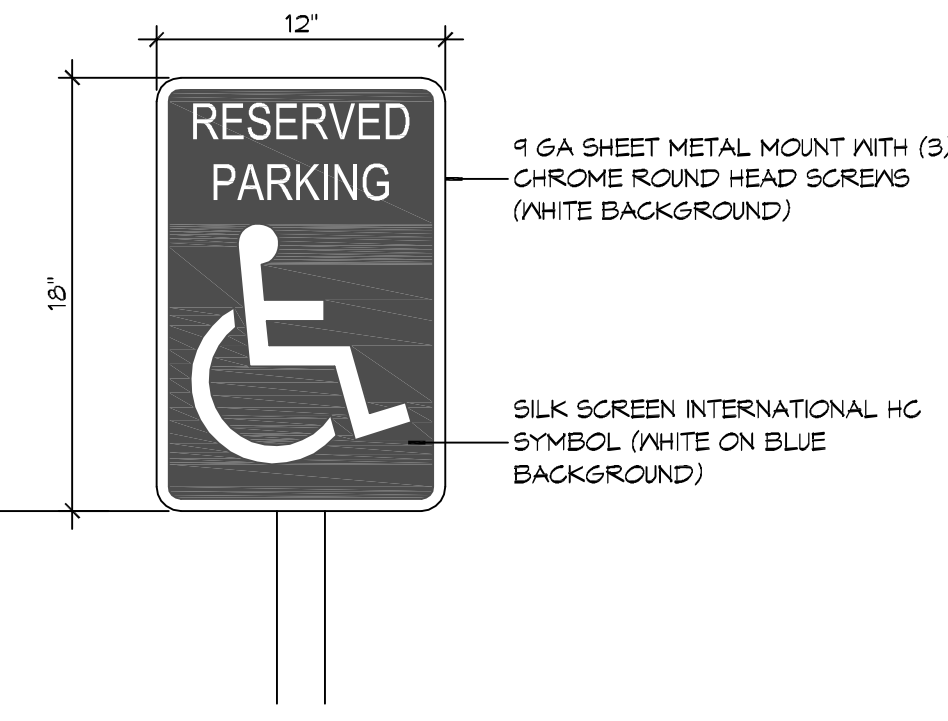
JOB No: 2442 DATE: 07/07/2021
DRAWN BY: JAS CHECKED BY: CKD

SHEET TITLE:
GENERAL INFORMATION SHEET

DRAWING NUMBER:
G101

SHEET No: 1 of 8

FILE NAME: J:_Customer\2445 - Mendenhall Center\2445 - Mendenhall Center.dwg DATE: 07/07/2021 3:55:53 PM



1 6A SHEET METAL MOUNT WITH (3) CHROME ROUND HEAD SCREWS (WHITE BACKGROUND)

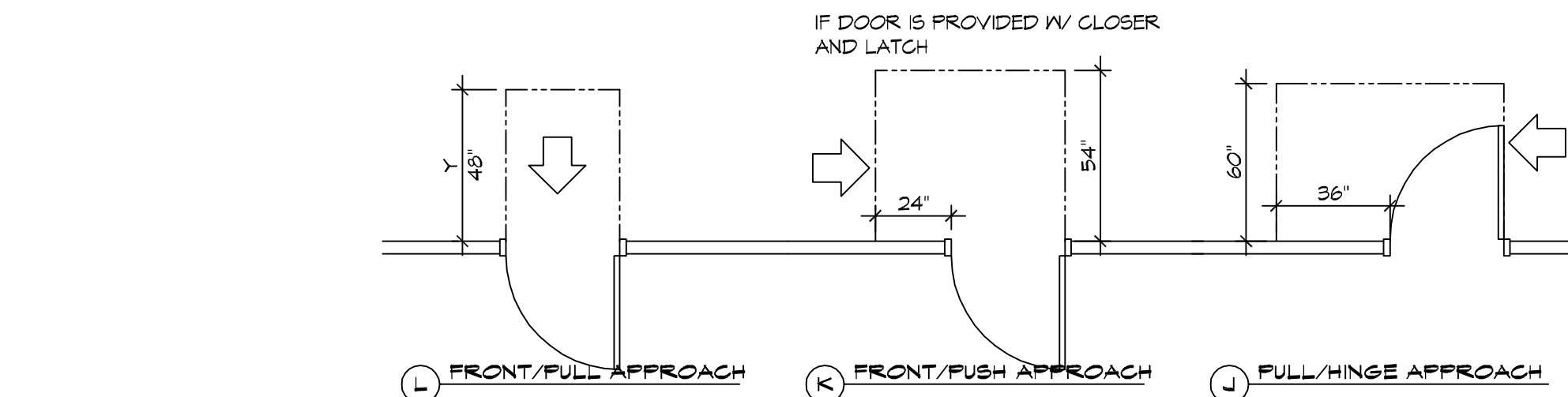
SILK SCREEN INTERNATIONAL HC SYMBOL (WHITE ON BLUE BACKGROUND)

2X2 STEEL TUBE - CLOSE TOP END AND GRIND SMOOTH- PAINT WHITE

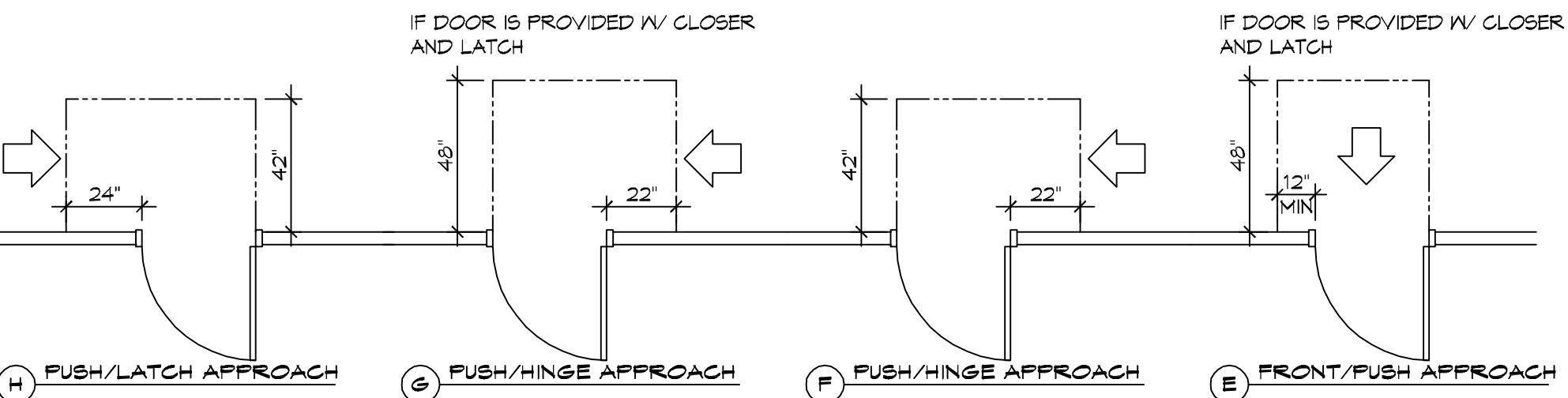
CONCRETE BASE TO BE CENTERED ON ACCESSIBLE PARKING SPACE (TYP)

2 ACCESSIBLE SIGN

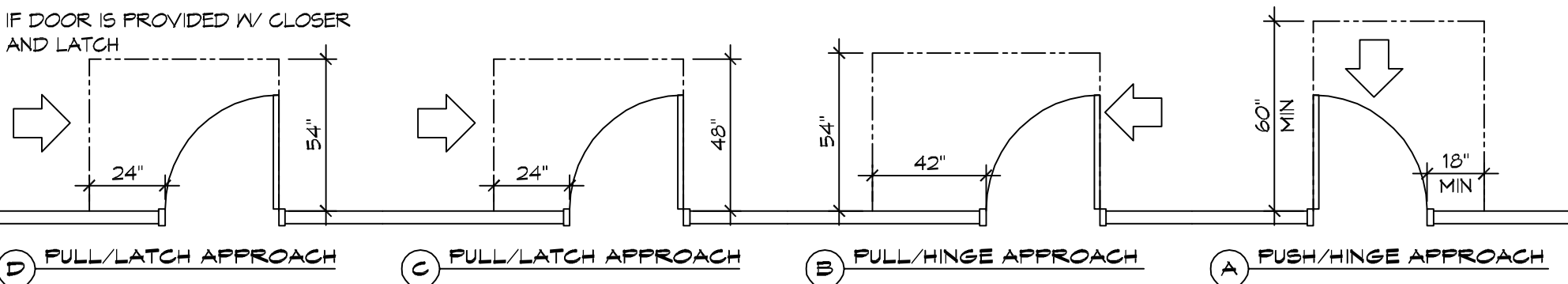
SCALE: NTS



L FRONT/FULL APPROACH K FRONT/PUSH APPROACH J FULL/HINGE APPROACH



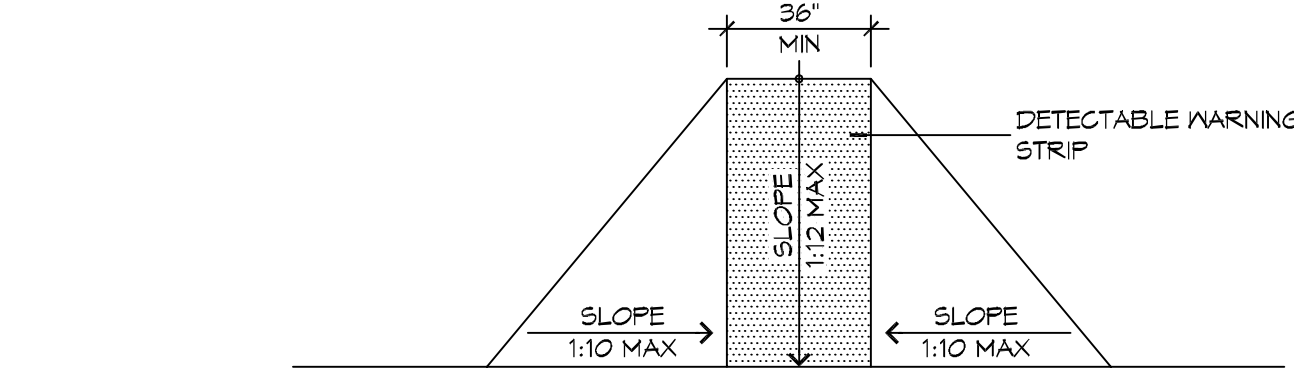
H PUSH/LATCH APPROACH G PUSH/HINGE APPROACH F PUSH/HINGE APPROACH E FRONT/PUSH APPROACH



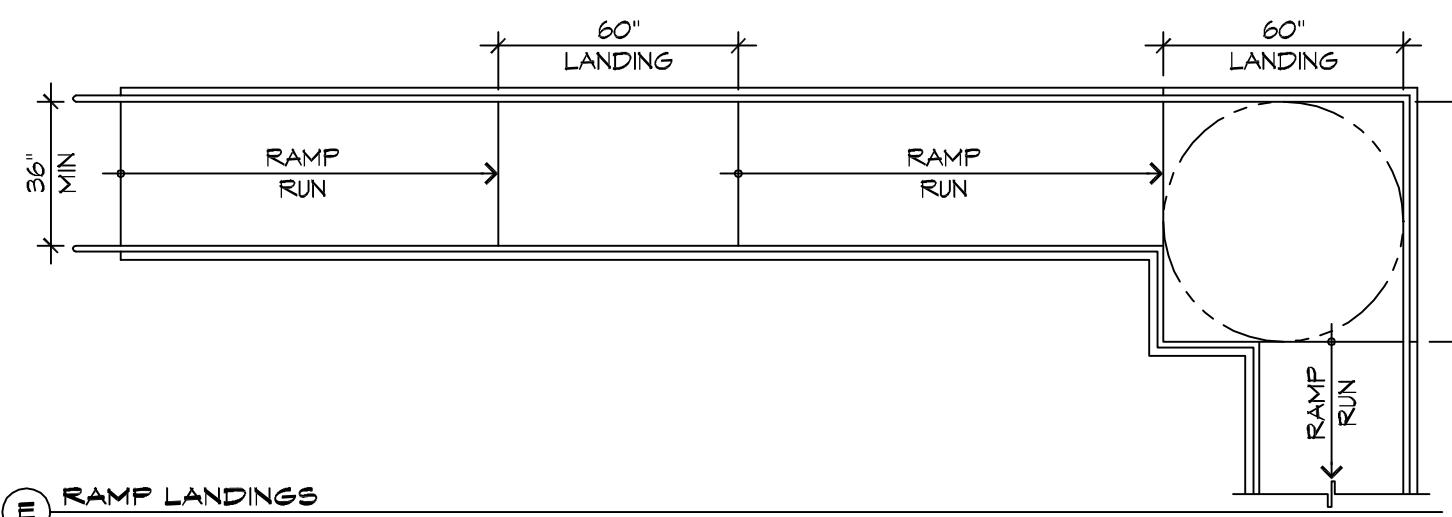
D FULL/LATCH APPROACH C FULL/LATCH APPROACH B FULL/HINGE APPROACH A PUSH/HINGE APPROACH

4 ADA DOOR CLEARANCES

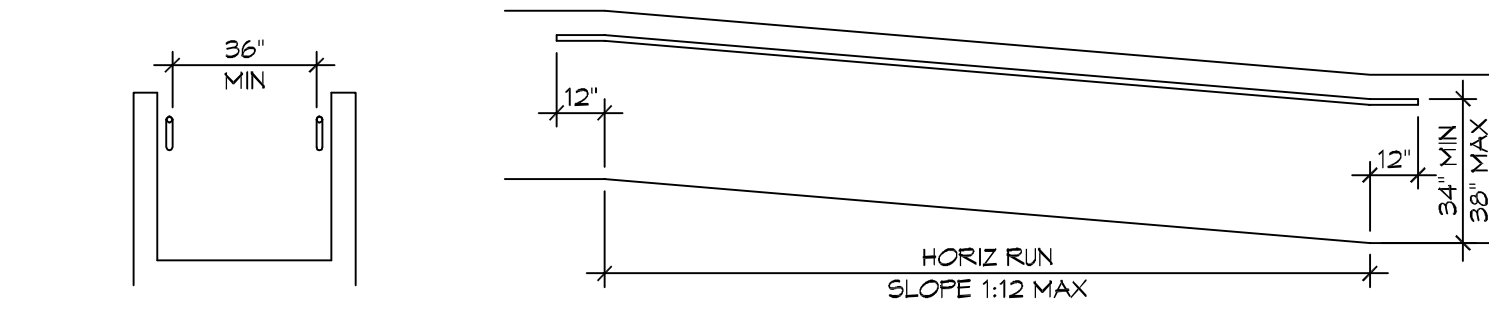
SCALE: 1/4" = 1'-0"



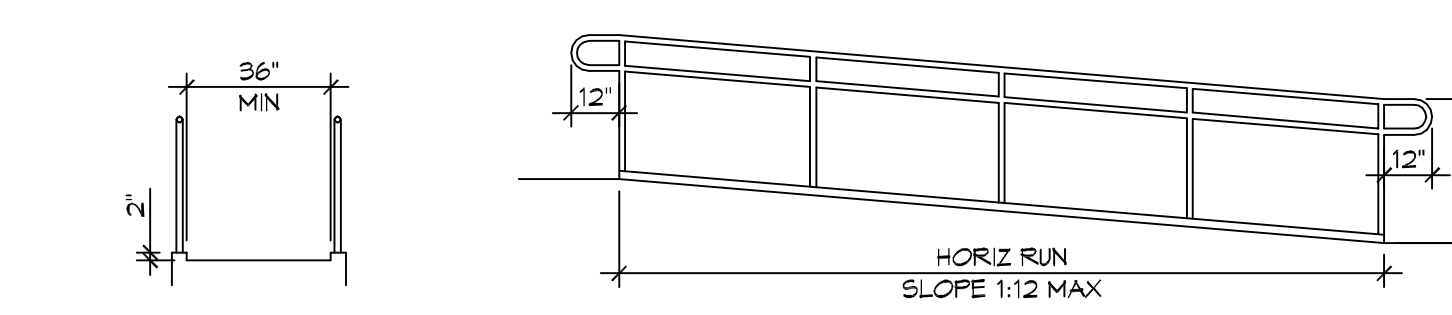
F FLARED RAMP



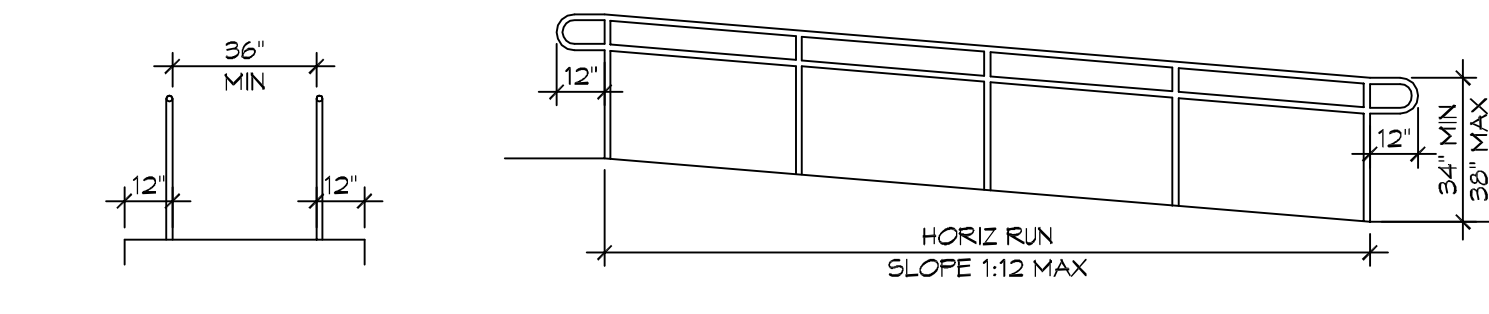
E RAMP LANDINGS



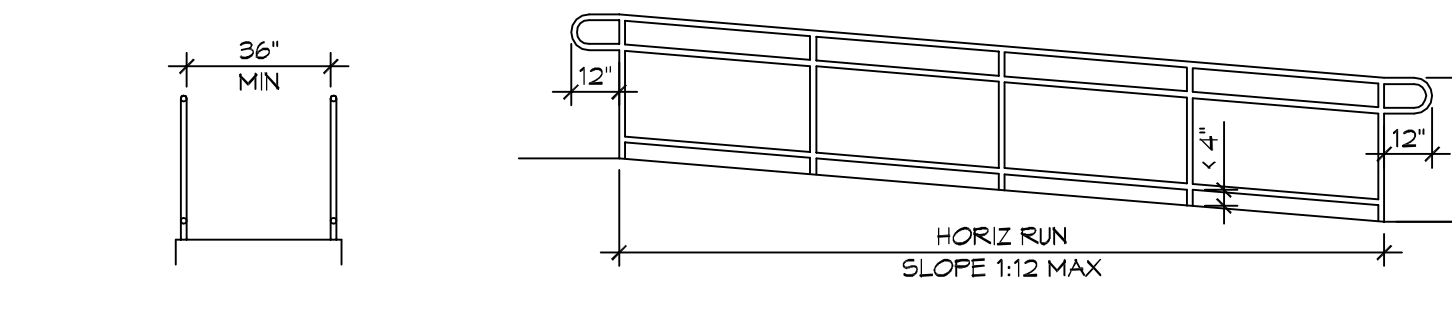
D WALL EDGE PROTECTION



C CURB EDGE PROTECTION



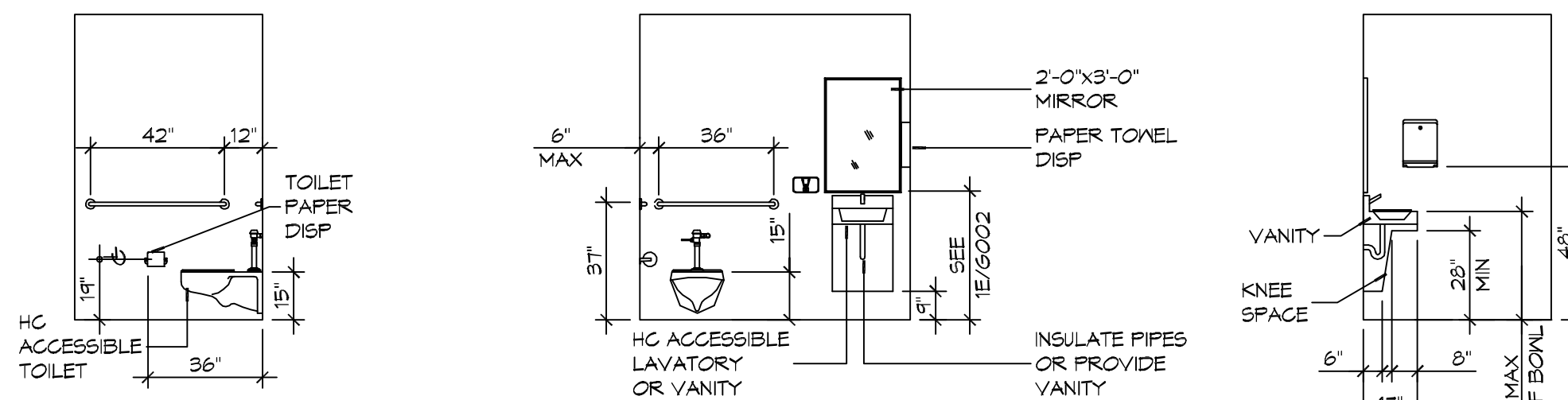
B EXTENDED SURFACE EDGE PROTECTION



A BARRIER EDGE PROTECTION

3 ACCESSIBLE RAMPS

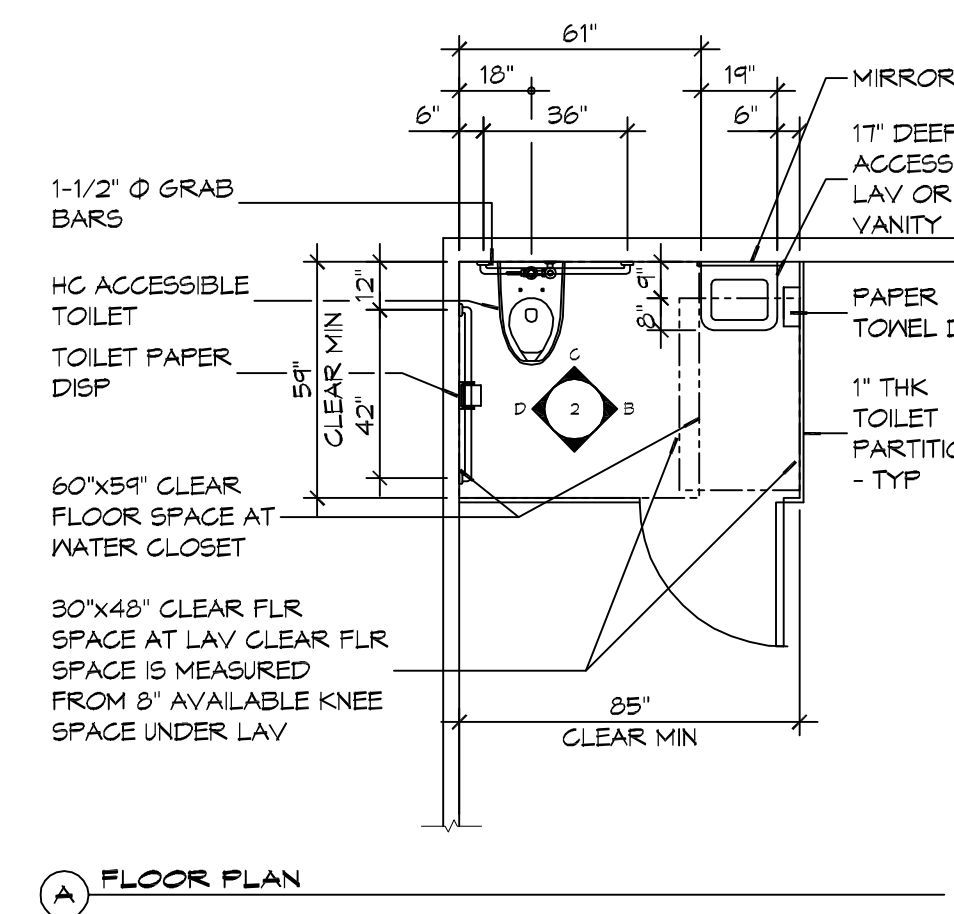
SCALE: 1/4" = 1'-0"



D INTERIOR ELEVATION

C INTERIOR ELEVATION

B INTERIOR ELEVATION



A FLOOR PLAN

5 RESTROOM CLEARANCES

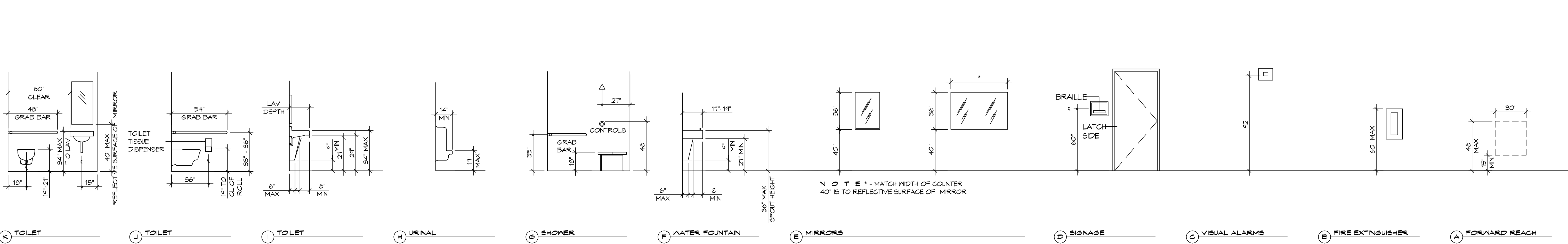
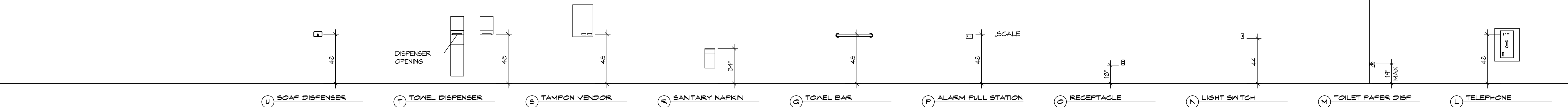
SCALE: 1/4" = 1'-0"

ACCESSIBILITY NOTES

DOOR CLEARANCE NOTES
 ALCOVES SHALL COMPLY WITH THE CLEARANCES FOR FRONT APPROACHES, 31/3002 - 31/3002.
 DOOR HARDWARE SHALL BE LEVER TYPE.
 MAX DOOR OPENING FORCE:
 INTERIOR HINGED DOORS: 5 LBF
 EXTERIOR HINGED DOORS: 8.5 LBF
 SLIDING OR FOLDING DOORS: 5 LBF
 FIRE DOORS SHALL HAVE THE MINIMUM OPENING FORCE ALLOWABLE BY THE APPROPRIATE ADMINISTRATIVE AUTHORITY.
 HARDWARE REQUIRED FOR ACCESSIBLE DOOR PASSAGE SHALL BE MOUNTED NO HIGHER THAN 48" AND NOT LESS THAN 34" ABOVE FINISHED FLOOR.
 THE FLOOR OR GROUND AREA WITHIN THE REQUIRED CLEARANCES SHALL BE LEVEL AND CLEAR.
 THRESHOLDS AT DOORWAYS SHALL NOT EXCEED 3/4" IN HEIGHT FOR EXTERIOR SLIDING DOORS OR 1/2" FOR OTHER TYPES OF DOORS. RAISED THRESHOLDS AND FLOOR LEVEL CHANGES AT ACCESSIBLE DOORWAYS SHALL BE BEVELED WITH A SLOPE NO GREATER THAN 1:2.
 DOORWAYS SHALL HAVE A MINIMUM CLEAR OPENING OF 32" WITH THE DOOR OPEN 90°, MEASURED BETWEEN THE FACE OF THE DOOR AND THE OPPOSITE STOP. OPENINGS MORE THAN 24" IN DEPTH SHALL MAINTAIN 32" MIN CLEARANCE.
RAMP NOTES
 THE CLEAR SPACE BETWEEN THE HANDRAIL AND THE WALL SHALL BE MIN 1-1/2" CLEAR.
 GRIPPING SURFACES SHALL BE CONTINUOUS AND UNOBSTRUCTED.
 ENDS OF HANDRAILS SHALL BE EITHER ROUNDED OR RETURNED SMOOTHLY TO FLOOR, WALL, OR POST.
 HANDRAILS SHALL NOT ROTATE WITHIN THEIR FITTINGS.
 THE CROSS SLOPE OF RAMP SURFACES SHALL BE NO GREATER THAN 1:50.
 OUTDOOR RAMPS AND THEIR APPROACHES SHALL BE DESIGNED SO THAT WATER WILL NOT ACCUMULATE ON WALKING SURFACES.
 RAMPS AND LANDINGS WITH DROP-OFFS SHALL HAVE CURBS, WALLS, RAILINGS, OR PROJECTING SURFACES THAT PREVENT PEOPLE FROM SLIPPING OFF THE RAMP. CURBS SHALL BE A MINIMUM OF 2" HIGH.
 HANDRAILS SHALL BE PROVIDED ALONG BOTH SIDES OF RAMP SEGMENTS. THE INSIDE HANDRAIL ON SWITCHBACK OR DOGLEG RAMPS SHALL ALWAYS BE CONTINUOUS.
 RAMP LANDINGS SHALL BE AT LEAST AS WIDE AS THE RAMP RUN LEADING TO IT.

GENERAL SITE ACCESSIBILITY NOTES

1. ACCESSIBILITY SIGNAGE SHALL COMPLY WITH ADAAG 2010 GUIDELINES SECTION 303.7.
2. ACCESSIBLE RAMP AND HANDRAIL DESIGNS WHERE THEY OCCUR.
3. ALL ACCESSIBLE PARKING SPACES AND ASILES THAT SERVE THEM SHALL COMPLY WITH ADAAG 2010 GUIDELINES SECTIONS 502.4 AND 502.5.
4. OPENINGS IN GROUND SURFACES SHALL COMPLY WITH ADAAG 2010 GUIDELINES SECTION 302.3.
5. VERTICAL CHANGES IN ELEVATION ALONG ALL ACCESSIBLE ROUTES SHALL COMPLY WITH ADAAG 2010 GUIDELINES SECTIONS 303.2, 303.3, AND 303.4.
6. PARKING SPACES DESIGNATED AS ACCESSIBLE SHALL INCLUDE THE INTERNATIONAL SYMBOL OF ACCESSIBILITY COMPLYING WITH ADAAG 2010 GUIDELINES SECTIONS 303.2.1 AND 502.6.
7. ALL ACCESSIBLE PARKING SPACES AND ROUTES SERVING THEM SHALL HAVE A ROUGH, SLIP-RESISTANT SURFACE OR LIGHT BROOM FINISH IN COMPLIANCE WITH ADAAG 2010 GUIDELINES SECTION 302.1.



6 MOUNTING HEIGHTS

SCALE: 1/4" = 1'-0"

DAMMON ENGINEERING, INC.
 LOUISIANA & MISSISSIPPI
 Chief Engineer: Brian Metch, PE
 554 Old Spanish Trail
 Slidell, LA 70488
 www.dammonengineering.com
 info@dammonengineering.com
 PH: 985.649.5832

#	DESCRIPTION	DATE

SEAL:

MERKLE CENTER
 123 EAST I-10 SERVICE ROAD
 SLIDELL, LA 70469
 JOB No: 2442 DATE: 07/07/2021
 DRAWN BY: CKZ CHECKED BY: CKZ

SHEET TITLE:
 ACCESSIBILITY INFORMATION

DRAWING NUMBER:
G102

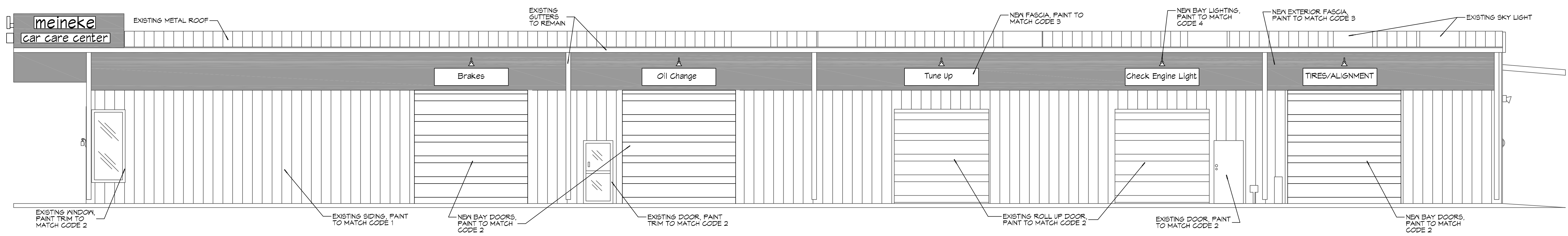
SHEET No: 2 of 2

FILE NAME: J:_Comm\10242 - Meineke Automotive Retail Service - 10242\10242.dwg - Date: 07/07/2021 10:45:00 AM
 PLOT DATE: 07/07/2021 10:45:00 AM
 PLOT BY: JLD

PAINT CODES		
CODE	COLOR CODE	COLOR
1	SN 1014	SHERWIN WILLIAMS ELDER WHITE (FLAT FINISH)
2	SN 6258	SHERWIN WILLIAMS TRICORN BLACK (FLAT FINISH)
3	SN 7064	SHERWIN WILLIAMS IRON ORE (FLAT FINISH)
4	104 U	PANTONE 104 YELLOW

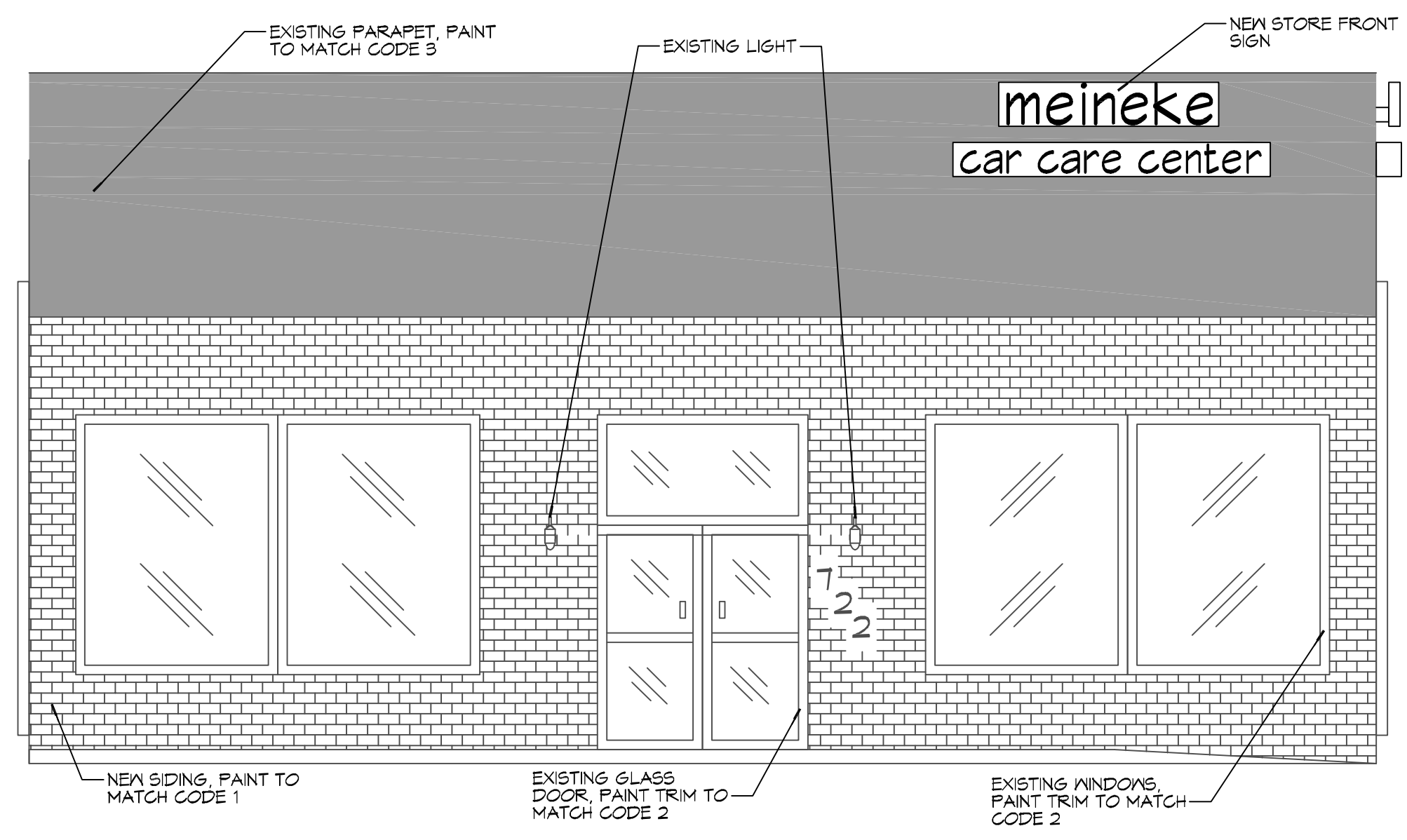
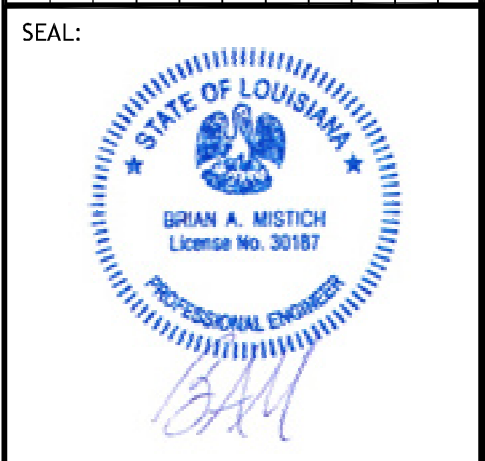
*SEE MEINEKE CENTER GUIDELINES

DAMMON ENGINEERING, INC.
 LOUISIANA & MISSISSIPPI
 Chief Engineer: Brian Mistich, PE
 554 Old Spanish Trail
 Slidell, LA 70488
 www.dammonengineering.com
 info@dammonengineering.com
 PH: 985.649.5832

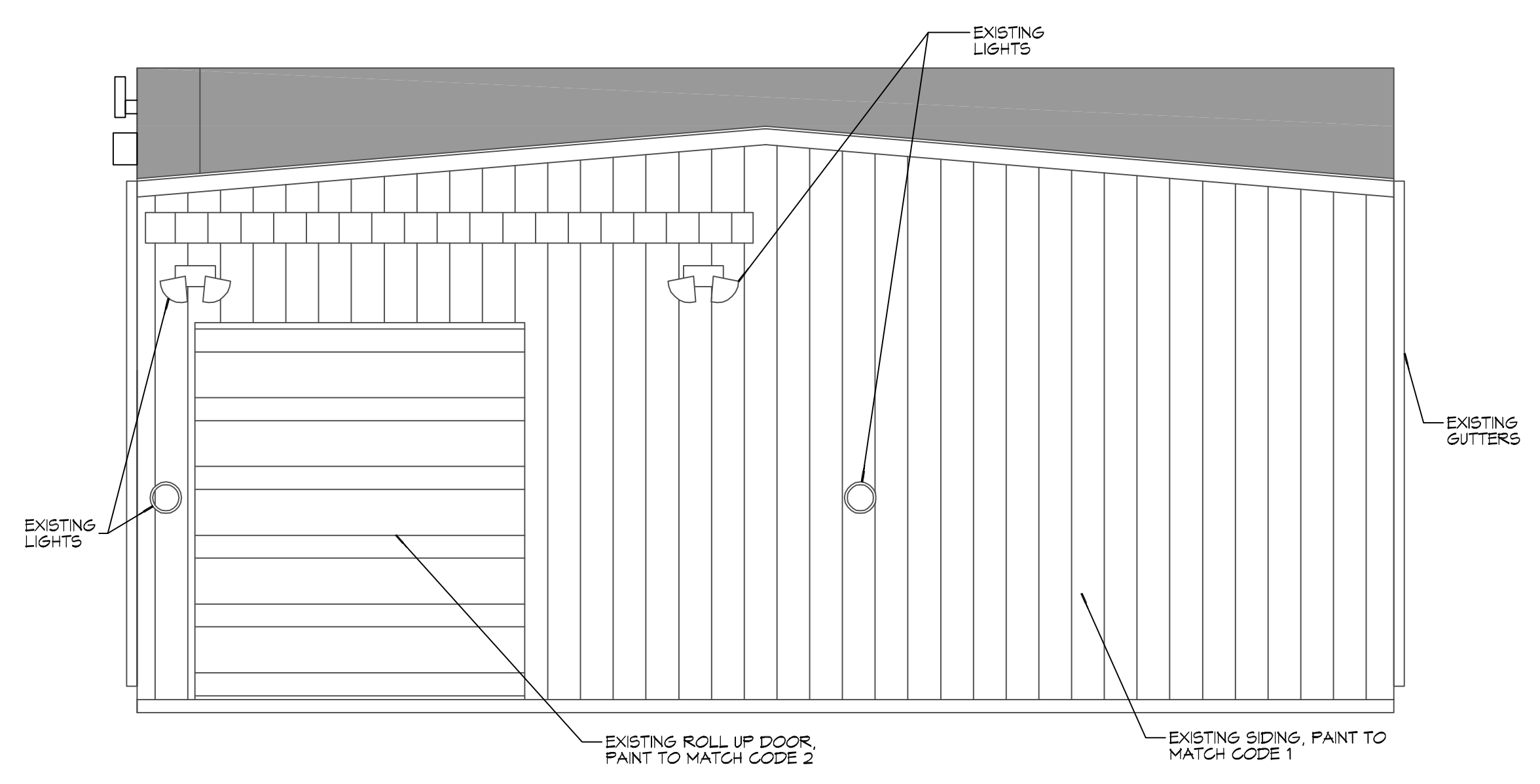


9 RIGHT ELEVATION
SCALE: 3/16"=1'-0"

#	DESCRIPTION	DATE



10 FRONT ELEVATION
SCALE: 1/4"=1'-0"



11 REAR ELEVATION
SCALE: 1/4"=1'-0"

MEINEKE
MEINEKE CENTER
 122 EAST I-10 SERVICE ROAD
 SLIDELL, LA 70488
 JOB No: 2442 DATE: 07/07/2021
 DRAWN BY: JLD CHECKED BY: CSD

SHEET TITLE:
NEW ELEVATIONS

DRAWING NUMBER:
A102

