



September 4, 2007

Mr. Mark Wassweiler, NCARB
MMW Architects, PC
912 E. Fairview Avenue
Montgomery, AL 36106

Re: LaQuinta/QRC

Dear Mr. Wassweiler,

As requested, our registered professional is Emmett G. "Pete" Dammon (985-649-5832) for the LaQuinta and Mr. Chuck Dammon is Dammon Engineering's representative for this project. We utilized the same design for QRC's paving/parking as that of the LaQuinta project, to assist QRC with the elevation errors they developed.

Mr. Wassweiler, we would imagine that since you are ultimately responsible for the QRC project, that back in early May 2007, you would have been informed, made aware of, and been involved in all the meetings, discussions, and correspondence between QRC representatives, QRC contractors and the LaQuinta team (owner, design engineer, owner's attorney and Lanco) regarding elevation deficiencies with the QRC design as it relates to the LaQuinta plans/elevations.

We can only assume it was you (or your agent) that reviewed and approved the site plan QRC utilized. Our review of your plans revealed a note requiring coordination with our previously established grades, this never occurred. We believe you also made a major mistake and/or were given erroneous information relating to the main entrance drive and adjacent parking. Please see the attached plans which depict QRC's property boundary and cross easement. The entrance drive, northside parallel parking spaces, rear western parking areas and drives belong to the hotel. QRC has a cross easement for these areas only, as illustrated.

The revisions and design requested/required for rear access and concrete parking spaces on QRC's property were discussed and approved by QRC's site representative in our May meeting, why you were not involved is beyond us. So, please refrain from accusing us of unauthorized, there were too many individuals present for the truth - not to be known.

Regarding QRC's refusal to pay our invoice, we find these primarily cosmetic related issues/excuses being employed by QRC, some 3+ months later, as a financial threat and smoke screen to escape their responsibility. Everyone involved knows the true sequence of events and timeless table; QRC's reluctance is verification of their project manager's unscrupulous nature. Your contentions of unsafe, public hazards, authorization and substandard are groundless with regards to the paving we performed for QRC. There may be some minor/punchlist items but one would think had any issues been so apparent, so unsafe, and so flagrant, that it would not

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take 3+ months for QRC or any educated individual to comment. Just think about it. There has been nothing said, nothing written, nothing emailed...until it's time to pay the piper, why?

We can only reiterate and request you review ALL relative documentation and visit the site. Mr. Dammon has been kept up to speed...you will be well served to reserve your comments until such time as you have the opportunity to discuss all pertinent issues with him.

The major question for you is "who is in charge" of reporting to you the discrepancies/concerns/inconsistencies with QRC's tie-in to our facilities? You're welcome to discuss this matter also with Chuck Dammon.

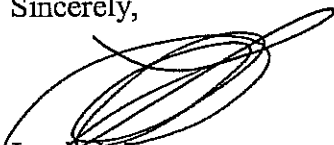
Our suggestion at this time is only for you to research the QRC's team records on this project's legal boundaries, modifications, visit the site, and construe your own conclusion. In the meantime, the LaQuinta project is still under construction and as such, QRC should be advised as to the legal ramifications of allowing public access to a construction area.

We realize your commitment and responsibilities are with QRC and can only hope that you perform your professional duty in a manner consistent with industry standards, and not blindly through the QRC site representative's deceptive directives.

When you have all the relevant facts and data we would think you could at least appreciate and comprehend our frustrations. We stand ready to review any design improvements, suggested methods or recommendations from your office to assist with QRC's problems. Please contact Chuck Dammon and arrange a site meeting (of the minds).

Thank you for your time and we would definitely appreciate your direct involvement with this project. If you have any questions or comments, please feel free to contact me at (504) 733-8141.

Sincerely,

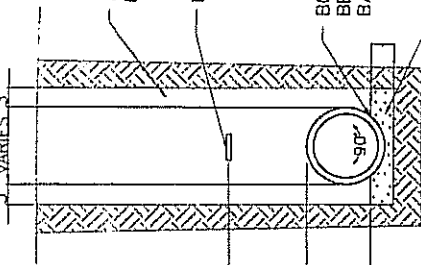


Louis J. Lanza

LJL/sl

Enclosures

CC: Dr. Hernandez
Ron Blair
Chuck Dammon



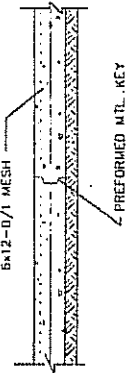
PIPE BEDDING
SCALE: 1" = 1'-0"

APPROVED BACK FILL COMPACT
IN MAX. 6" THICK LAYERS

IDENTIFICATION TAPE

BOTTOM QUADRANT OF PIPE SHALL
BE BEDDED FOR FULL LENGTH OF
BARREL

GRANULAR FILL



CONSTRUCTION JOINT
SCALE: 1" = 1'-0"

N. T. S.

ELEV. VARIES -
SEE PLAN

3-#5 BARS CONT.

6x12-0/1 MESH
ELEV. VARIES -
SEE PLAN

#3 TIES @ 24" O.C.

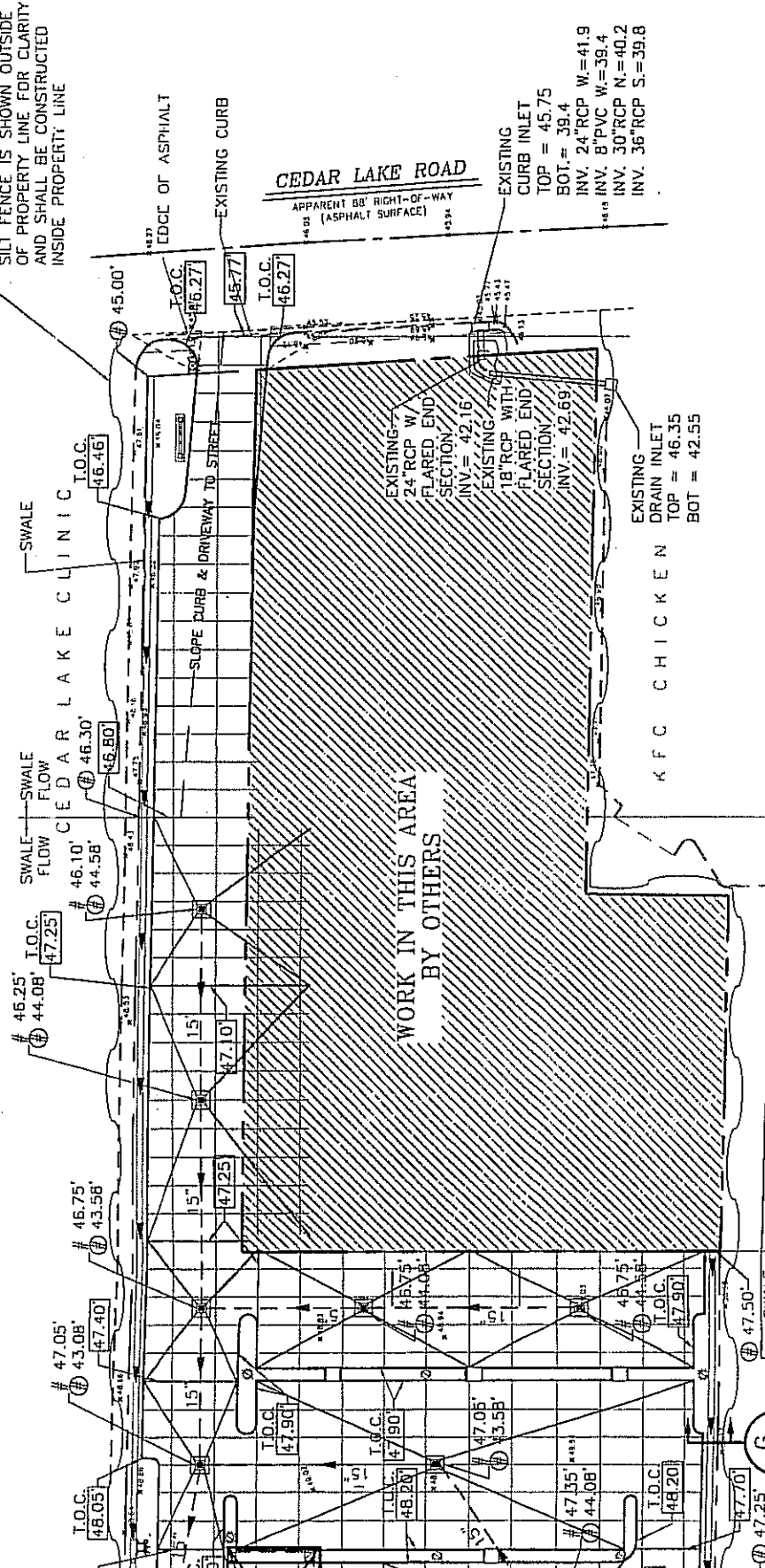
ELEV. VARIES -
SEE PLAN

TYPICAL CURB DETAIL
SCALE: 1" = 1'-0"

SCALE: 1" = 1'-0"

NOTE:

SILT FENCE IS SHOWN OUTSIDE
OF PROPERTY LINE FOR CLARITY
AND SHALL BE CONSTRUCTED
INSIDE PROPERTY LINE



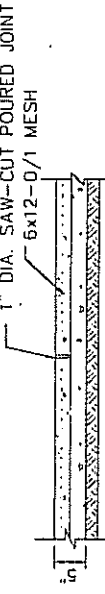
WORK IN THIS AREA
BY OTHERS

KFC CHICKEN

EXISTING
DRAIN INLET
TOP = 46.35
BOT = 42.55

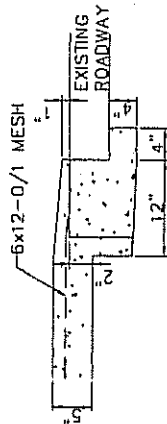
EXISTING
CURB INLET
TOP = 45.75
BOT = 39.4
INV. 24" RCP W. = 41.9
INV. 8" PVC W. = 39.4
INV. 30" RCP N. = 40.2
INV. 36" RCP S. = 39.8

CEDAR LAKE ROAD
APPARENT 88' RIGHT-OF-WAY
48.03 (ASPHALT SURFACE)



CONTRACTION JOINT
SCALE: 1" = 1'-0"

N. T. S.



PAVING & DRAINAGE PLAN
DR. EDUARDO HERNANDEZ
CEDAR LAKE ROAD
BILOXI, MISSISSIPPI

AMMON ENGINEERING, INC.
LA QUINTA INN & SUITES
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