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COLUMBIA, MS 39429
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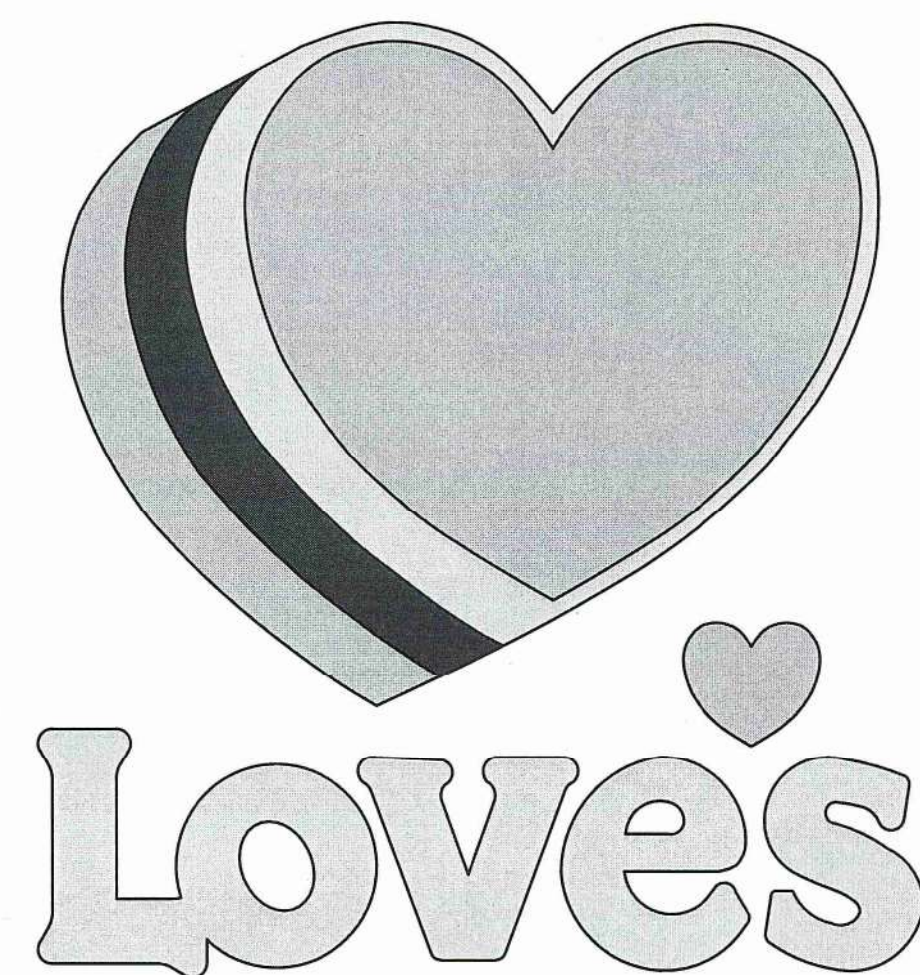
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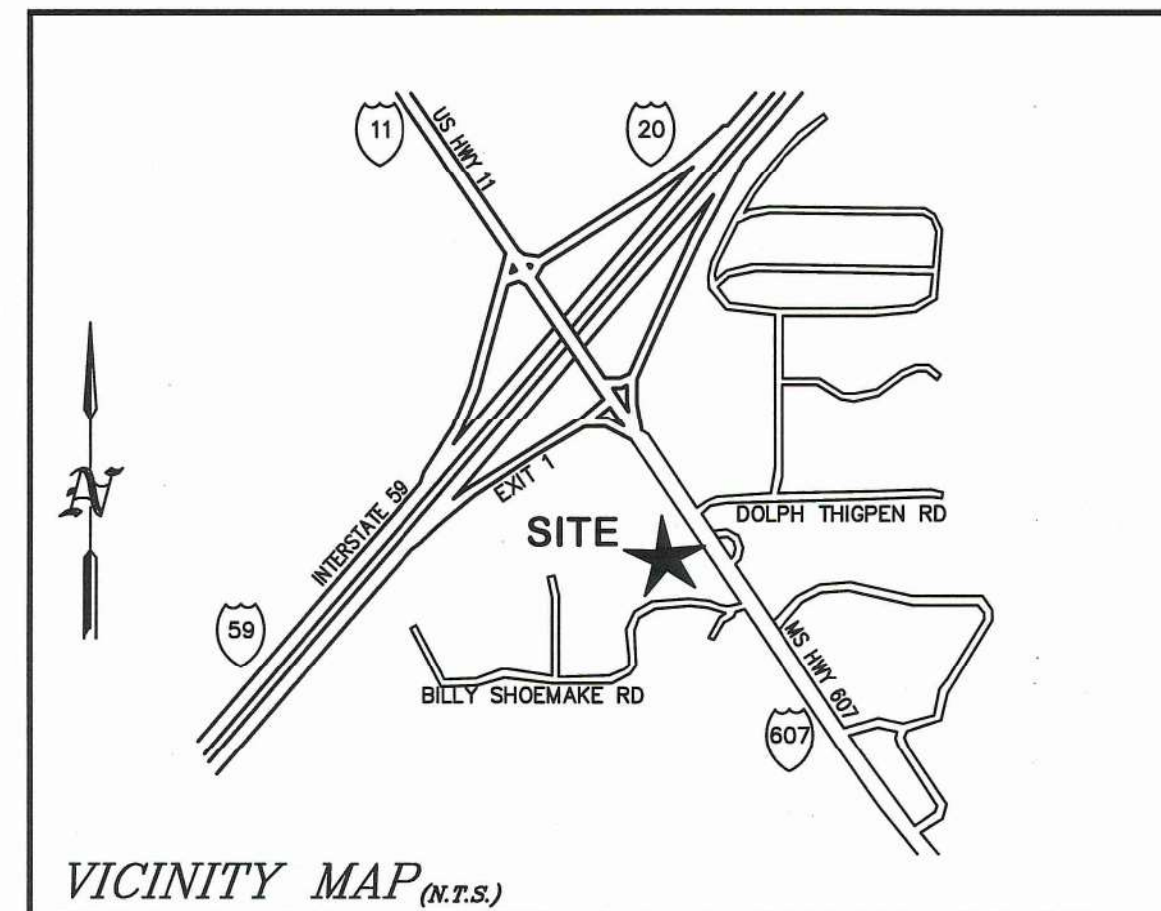
PROPOSED LOVE'S TRAVEL STOP

INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607 PEARL RIVER COUNTY, MISSISSIPPI

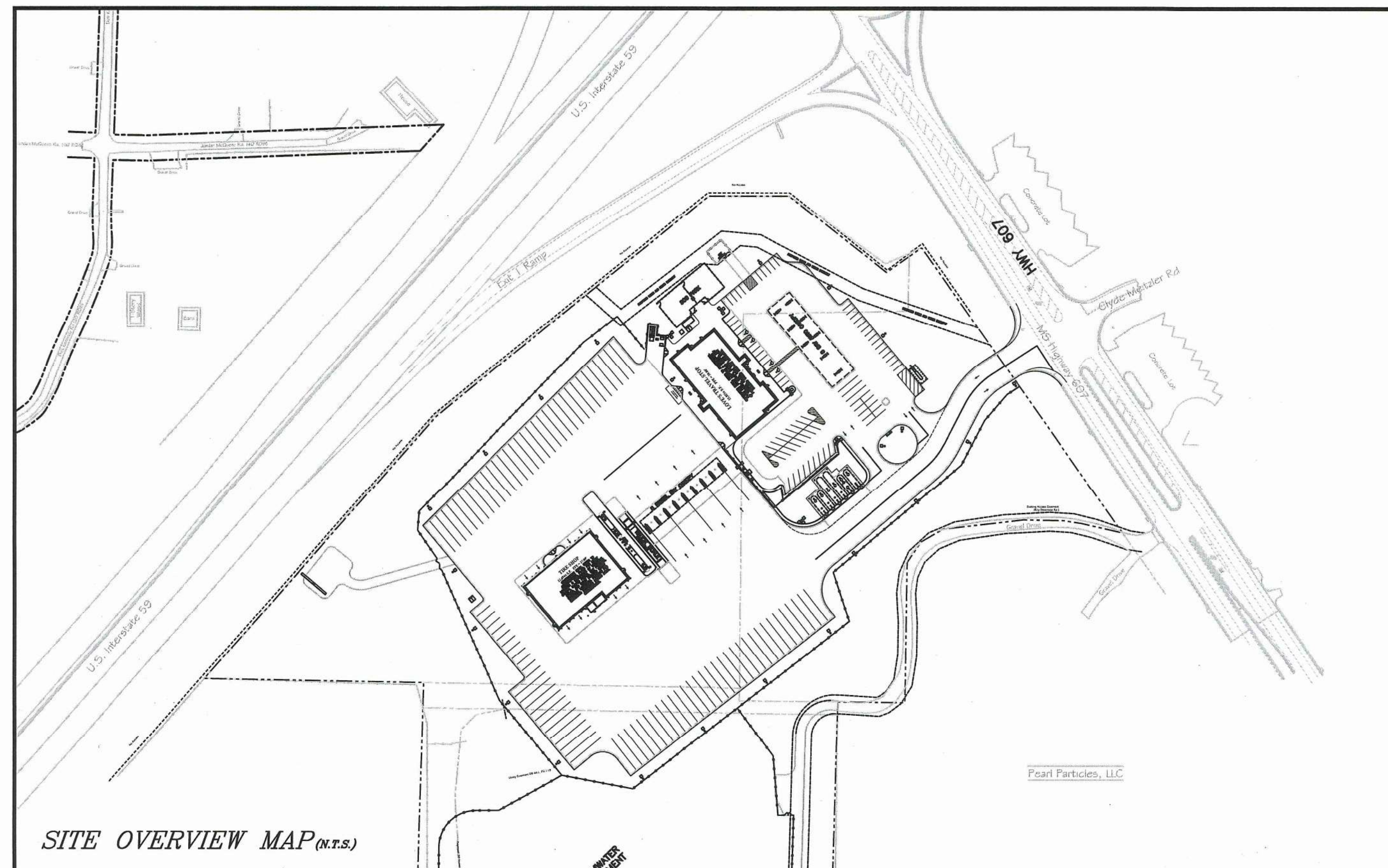


3/1/23
Jessie Marsh

developer:
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Oklahoma City, Ok 73126
(405) 749 1744 telephone
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VICINITY MAP (N.T.S.)



SITE OVERVIEW MAP (N.T.S.)



Know what's below.
Call before you dig.

- NOTES:
1. THE SITEWORK FOR THIS PROJECT SHALL MEET OR EXCEED THE SITEWORK SPECIFICATIONS.
 2. ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CODES AND/OR UTILITY SERVICE COMPANIES SHALL BE PERFORMED PRIOR TO ANNOUNCED BUILDING POSSESSION AND THE FINAL CONNECTION OF SERVICES.



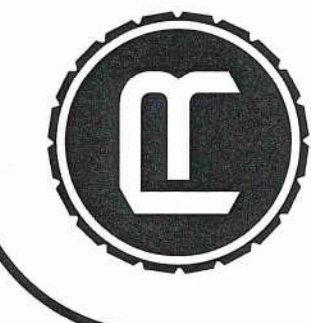
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CIVIL ENGINEERING
6700 Jefferson Highway - Resource Suite 4A
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Tel. (225) 761-9999, Fax (225) 766-6672



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F. BASED ON THE RESULTS OF THE INSPECTION, THE SITE DESCRIPTION IDENTIFIED IN THE SWPPP SHALL BE REVISED AS APPROPRIATE, BUT IN NO CASE LATER THAN 7 DAYS FOLLOWING THE INSPECTION. SUCH MODIFICATIONS SHALL PROVIDE FOR TIMELY IMPLEMENTATION OF ANY CHANGES TO THE SWPPP, BUT IN NO CASE LATER THAN 14 DAYS FOLLOWING THE INSPECTION.
G. INSPECTIONS SHALL BE DOCUMENTED AND INCLUDE THE SCOPE OF THE INSPECTION, NAME(S) AND TITLE OF PERSONNEL MAKING THE INSPECTION, THE DATE(S) OF THE INSPECTION, MAJOR OBSERVATIONS RELATING TO THE IMPLEMENTATION OF THE SWPPP (INCLUDING THE LOCATION(S) OF DISCHARGES OF SEDIMENT OR OTHER POLLUTANTS FROM THE SITE AND OF ANY CONTROL DEVICE THAT FAILED TO OPERATE AS DESIGNED OR PROVED INADEQUATE FOR A PARTICULAR LOCATION), AND ACTIONS. INSPECTION DOCUMENTATION WILL BE MAINTAINED ON SITE AND MADE AVAILABLE UPON REQUEST.
H. QUALITY ASSURANCE OF EROSION PREVENTION AND SEDIMENT CONTROLS SHALL BE DONE BY PERFORMING SITE ASSESSMENT AT THE SITE. THE SITE ASSESSMENT SHALL BE CONDUCTED AT EACH OUTFALL WITHIN A MONTH OF CONSTRUCTION COMMENCING AT EACH PORTION OF THE SITE THAT DRAINS THE QUALIFYING ACREAGE OF SUCH PORTION TO THE SITE. THE SITE ASSESSMENT SHALL BE PERFORMED BY THE ENGINEER.

PROJECT DESCRIPTION:
1. DEMOLITION, CLEARING, AND GRUBBING OF EXISTING SITE.
2. CONSTRUCTION OF THE NEW LOVE'S COUNTRY STORE WITH BUILDINGS, CANOPIES, DRAINAGE STRUCTURES, PARKING LOTS, UTILITIES AND OTHER APPURTENANCES. THE SITE AREA IS 22.40± ACRES. THE TOTAL AREA OF LAND DISTURBING ACTIVITIES FOR THE INFRASTRUCTURE WORK SHALL BE APPROXIMATELY 26.93± ACRES.
3. EXISTING SITE DESCRIPTION: THE SITE CONSISTS OF GRAVEL ROADS, MULTIPLE TRAILERS, A BRICK HOME AND A LARGE WAREHOUSE. THE PROPERTY IS ALSO USED AS A PARKING AREA FOR MULTIPLE FEMA TRAILERS PARKED IN A GRAVEL LOT. THE REMAINDER OF THE SITE IS FORESTED WITH A SINGLE LARGE DETENTION POND LOCATED AT THE SOUTHERN END OF THE SITE.
4. ADJACENT AREAS: THE SITE IS BOUND BY I-59 EXIT RAMP TO THE NORTH, MS HIGHWAY 607 TO THE EAST, FORESTED LAND TO THE WEST, AND A NEIGHBORING GRAVEL/CONCRETE PLANT TO THE SOUTH.
5. OFF-SITE AREAS: CONSTRUCTION ASSOCIATED WITH ROAD IMPROVEMENTS AND WATER AND FORCE MAIN EXTENSIONS ACROSS THE INTERSTATE AND TO THE NEAREST AVAILABLE TI-E-IN LOCATION TO THE NORTH. OTHERWISE, THE CONSTRUCTION IS CONTAINED ON-SITE.
6. SOILS: PER THE GEOTECHNICAL REPORT PREPARED BY TERRACON OF BLOOMINGDALE, MS, REVIEW OF SOIL BORINGS INDICATE THAT THE ONSITE SOILS IS CHARACTERIZED BY A CROSS-BEDEDDED MIXTURE OF SILT SAND, GRAVEL, AND CLAY. THE DEPOSITS MAY ALSO CONTAIN LIMESTONE AT DEPTH.
7. EROSION AND SEDIMENT CONTROL MEASURES AND SEQUENCE OF CONSTRUCTION:
A. THE CONTRACTOR SHALL GIVE THE CITY OFFICIALS AT LEAST 48 HOURS NOTICE TO SCHEDULE AN ON-SITE PRE-CONSTRUCTION MEETING PRIOR TO THE START OF WORK.
B. PERFORM SITE, DRAINAGE, EROSION AND SEDIMENT CONTROL AND UTILITY WORK. BACKFILL TRENCHES PER GEOTECHNICAL RECOMMENDATIONS, AND AS SPECIFIED IN THE SPECIFICATIONS. HAIL AND STOCKPILE WASTE MATERIAL AND DISPOSE ON-SITE. IT SHALL BE PROTECTED AGAINST EROSION AS PART OF THE LOVE'S PROJECT.
C. TOP DRESS ALL DISTURBED AREAS WITH TOSOOIL, FINE GRADE, TILL, AMEND, SEED AND MULCH ALL DISTURBED AREAS. ALL STEEP SLOPES SHALL BE COMPLETELY COVERED WITH EROSION MATTING SUCH AS "EXCELSIOR".
D. TEMPORARY OR PERMANENT GROUND COVER REQUIRED WITHIN 14 DAYS OF COMPLETION OF PROJECT OR INACTIVITY ON SLOPES 3:1 OR LESS, 7 DAYS ON SLOPES GREATER THAN 3:1.
E. SILT FENCE, AS APPLICABLE, SHALL REMAIN IN PLACE UNTIL LAWN AREAS ARE WELL ESTABLISHED.
F. PERFORM SITE, DRAINAGE, EROSION AND SEDIMENT CONTROL AND UTILITY WORK. BACKFILL TRENCHES PER GEOTECHNICAL RECOMMENDATIONS, AND AS SPECIFIED IN THE SPECIFICATIONS. HAIL AND STOCKPILE WASTE MATERIAL AND DISPOSE ON-SITE. IT SHALL BE PROTECTED AGAINST EROSION AS PART OF THE LOVE'S PROJECT.
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PLANNING APPROVAL ONLY. REVIEW IS REQUIRED BY A LICENSED NURSERYMAN AND TO INSTALLATION OF ANY PLANT MATERIALS.
2. REFERENCE SPECIFICATIONS SECTION 2900 FOR CONSTRUCTION OF LANDSCAPING MATERIALS AND PRODUCTS.
3. REFERENCE CONSTRUCTION DETAILS AND SPECIFICATIONS.
4. ALL LANDSCAPE BEDS SHALL BE RAISED A MINIMUM OF 6 INCHES ABOVE GRADE TO INSURE PROPER AND POSITIVE DRAINAGE.
5. ALL LANDSCAPE BEDS WHICH ARE BORDERED BY LAWN OR SIDEWALKS SHALL HAVE PVC EDGING BORDER AS PER THE SPECIFICATIONS.
6. AT THE DIRECTION OF THE OWNER, ALL LANDSCAPE AREAS NOT SHOWN AS A BED SHALL BE SOLID SODDED WITH CENTIPEDE SOD, OR SEEDED TO OBTAIN A HEALTHY STAND OF GRASS.
7. ALL BEDS AND TREES SHALL BE MULCHED WITH HARDWOOD MULCH AS PER THE SPECIFICATIONS FOR HARDWOOD MULCH.
8. ANY SITE CONDITIONS WHICH WOULD REQUIRE PLACEMENT OF PLANT MATERIALS IN A DIFFERENT LOCATION THAN SHOWN ON THESE PLANS, MUST BE APPROVED BY THE CITY.

10. CALCULATIONS: POST-DEVELOPMENT CURVE NUMBER; CN=88 FOR THE SITE.

3/1/23
THIS DRAWING IS NOT VALID WITHOUT AN ORIGINAL BLUE INK SIGNATURE AND ORIGINAL HANDWRITTEN DATE OF A LICENSED PROFESSIONAL ENGINEER.

EROSION CONTROL NOTES:
1. THE CONTRACTOR IS TO OBTAIN ANY NECESSARY LAND CLEARING AND/OR GRADING PERMITS FROM THE CITY.
2. THE CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH THE CURRENT NPDES PERMIT FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
3. THE SWPPP PLAN SHALL BE REVISED AS APPROPRIATE, BUT IN NO CASE LATER THAN 7 DAYS FOLLOWING THE INSPECTION. SUCH MODIFICATIONS SHALL PROVIDE FOR TIMELY IMPLEMENTATION OF ANY CHANGES TO THE SWPPP, BUT IN NO CASE LATER THAN 14 DAYS FOLLOWING THE INSPECTION.
4. THE CONTRACTOR SHALL INSTALL ALL PERIMETER SEDIMENT AND EROSION CONTROL STRUCTURES, INCLUDING MANHOLES, FLUMES, PIPES, PONDS, ETC., PRIOR TO COMMENCING ANY LAND CLEARING ACTIVITY. ALL MEASURES INDICATED ON THIS PLAN SHOULD BE CONSIDERED A MINIMUM GUIDELINE. ADDITIONAL MEASURES MAY BE NECESSARY.
5. THESE EROSION AND SEDIMENTATION CONTROL MEASURES AND BEST MANAGEMENT PRACTICES ARE DESIGNED AS A MINIMUM EFFORT TO CONTROL THE RELEASE OF SOILS AND POLLUTANTS VIA STORMWATER CONVEYANCE, AND ARE DEPENDENT ON THE PHASING OF THE CONTRACTOR'S WORK AND HIS DILIGENCE IN MAINTAINING THESE ELEMENTS.
6. THE SWPPP PLAN SHALL BE RETAINED ON-SITE AT THE FACILITY WHICH GENERATES THE STORM WATER DISCHARGE. THE CONTRACTOR SHALL POST NEAR THE ENTRANCE OF THE PROJECT A COPY OF THE NOTICE OF COVERAGE AND NPDES PERMIT NUMBER. THE POSTED NOTICE SHALL INCLUDE THE NAME AND TELEPHONE NUMBER OF A LOCAL CONTACT PERSON, A DESCRIPTION OF THE PROJECT, AND INDICATE WHERE THE SWPPP IS STORED.
7. THE SWPPP SHALL BE MADE AVAILABLE TO THE GOVERNING AUTHORITIES UPON REQUEST, ALONG WITH ALL INSPECTION REPORTS AND PAPERWORK IN THIS REGARD.
8. THE SWPPP PLAN IS SUBJECT TO REVISION BY THE GOVERNING AUTHORITY WHEN IT DOES NOT MEET THE MINIMUM REQUIREMENTS. THESE REVISIONS SHALL AMEND THE SWPPP AND BE NOTED ON THE SWPPP MAINTAINED ON-SITE.
9. THE CONTRACTOR IS REQUIRED TO MAINTAIN THE SITE IN COMPLIANCE WITH THE SWPPP AND TO MAKE NECESSARY AMENDMENTS TO KEEP THE SITE IN COMPLIANCE.
10. THE CONTRACTOR MAY BE REQUIRED TO PROVIDE ADDITIONAL EROSION CONTROL MEASURES AS DIRECTED BY THE GOVERNING AUTHORITY.
11. THE CONTRACTOR IS RESPONSIBLE FOR CLEANING OUT ALL STORM DRAINAGE STRUCTURES, INCLUDING MANHOLES, FLUMES, PIPES, PONDS, ETC., PRIOR TO COMPLETION OF THE PROJECT.
12. THE FINANCIALLY RESPONSIBLE PARTY IS RESPONSIBLE FOR ANY AND ALL FINES ASSOCIATED WITH THE DISCHARGE OF SEDIMENT, EROSION, OR POLLUTANTS CAUSED BY THIS WORK.
13. A PRECONSTRUCTION MEETING BETWEEN THE CITY, GENERAL CONTRACTOR, AND GRADING CONTRACTOR IS REQUIRED PRIOR TO ANY LAND DISTURBING ACTIVITY.

SWPPP NOTES:
1. STORMWATER RUNOFF CONTROLS:
A. ALL EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE PLACED PRIOR TO OR AS PART OF THE CONSTRUCTION PROCESS.
B. BEST MANAGEMENT PRACTICES (BMP'S) INCLUDE BOTH STRUCTURAL AND VEGETATIVE ELEMENTS INCLUDED IN THE SWPPP WHICH ARE INTENDED TO RETAIN SEDIMENT ON SITE.
C. PERIODIC INSPECTION INDICATED THAT A CONTROL HAS BEEN USED INAPPROPRIATELY, THE CONTRACTOR MUST REPLACE OR MODIFY THE CONTROL FOR CURRENT SITE SITUATIONS.
D. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFF-SITE ACCUMULATIONS OF SEDIMENT SHALL BE PICKED UP PRIOR TO ANTICIPATED STORM EVENTS.
E. SEDIMENT SHALL BE REMOVED FROM SEDIMENT TRASSO, SILT FENCES, SEDIMENTATION PONDS, AND OTHER SEDIMENT CONTROLS WHEN THE CAPACITY REACHES 50 PERCENT.
F. LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION CHEMICALS EXPOSED TO STORMWATER SHALL BE PICKED UP PRIOR TO ANTICIPATED STORM EVENTS.
G. OFFSITE MATERIAL STORAGE AREAS USED SOLELY FOR THIS PERMITTED PROJECT ARE CONSIDERED A PART OF THIS PROJECT. BMP'S SHALL ALSO BE IMPLEMENTED.
H. CONSTRUCTION MUST BE SEQUENCED TO MINIMIZE THE EXPOSURE TIME OF DENUDED AREAS. DISTURBING THE MINIMUM AREAS NECESSARY FOR GRADING AND EQUIPMENT OPERATION.
I. EROSION AND SEDIMENT CONTROL MEASURES (BMP'S) MUST BE IN PLACE AND FUNCTIONAL PRIOR TO EARTH MOVING OPERATIONS BEGIN, AND MUST BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. TEMPORARY MEASURES MAY BE REMOVED DAILY BUT REPLACED AT THE END OF THE WORKDAY.
J. THE FOLLOWING RECORDS SHALL BE MAINTAINED ON-SITE. THE DATES OF MAJOR GRADING ACTIVITIES OCCUR, THE DATES WHEN ACTIVITIES TEMPORARILY OR PERMANENTLY CEASE, THE DATE WHEN STABILIZATION MEASURES ARE IMPLEMENTED.
K. TEMPORARY OR PERMANENT GROUND COVER REQUIRED WITHIN 14 DAYS OF COMPLETION OF PROJECT OR INACTIVITY ON SLOPES 3:1 OR LESS, 7 DAYS ON SLOPES GREATER THAN 3:1.
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2. MAINTENANCE
A. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING SUITABLE EROSION AND SEDIMENT CONTROL DEVICES ON-SITE DURING CONSTRUCTION AS REQUIRED TO PREVENT SILT FROM LEAVING THE SITE. ALL SILTS ESCAPING BEYOND THE PROPERTY LINE RESULTANT FROM THIS CONSTRUCTION ACTIVITY ARE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE RECLAIMED AND UTILIZED ON-SITE OR HAULED OFF FOR DISPOSAL.
B. EROSION CONTROL MEASURES SHALL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
C. MAINTENANCE NEEDS THAT ARE IDENTIFIED DURING AN INSPECTION SHALL BE RECTIFIED AND ACCOMPLISHED BEFORE THE NEXT STORM EVENT, BUT WITHIN 7 DAYS.
3. INSPECTIONS:
A. INSPECTIONS DESCRIBED IN PARAGRAPHS B, C, AND D BELOW, SHALL BE PERFORMED AT LEAST TWICE EVERY CALENDAR WEEK. INSPECTIONS SHALL BE PERFORMED AT LEAST 72 HOURS APART, WHERE SITES OR PORTIONS OF CONSTRUCTION SITES HAVE BEEN TEMPORARILY STABILIZED, OR RUNOFF IS UNLIKELY DUE TO WINTER CONDITIONS (E.G., SITE COVERED WITH SNOW OR ICE), SUCH INSPECTION ONLY HAS TO BE CONDUCTED ONCE PER MONTH UNTIL THAWING RESULTS IN RUNOFF OR CONSTRUCTION ACTIVITY RESUMES. INSPECTIONS REQUIREMENTS DO NOT APPLY TO DEFINABLE AREAS THAT HAVE BEEN FINALLY STABILIZED. WRITTEN NOTIFICATION OF THE INTENT TO CONDUCT ONLY MONTHLY INSPECTIONS AND THE JUSTIFICATION FOR SUCH REQUEST MUST BE SUBMITTED TO THE LOCAL ENVIRONMENTAL FIELD OFFICE.
B. QUALIFIED PERSONNEL (PROVIDED BY THE PERMITTEE OR COOPERATIVELY BY MULTIPLE PERMITTEES) SHALL INSPECT DISTURBED AREAS OF THE CONSTRUCTION SITE THAT HAVE NOT BEEN FINALLY STABILIZED, AREAS USED FOR STORAGE OF MATERIALS THAT ARE EXPOSED TO PRECIPITATION, STRUCTURAL CONTROL MEASURES, LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE, AND EACH OUTFALL.
C. DISTURBED AREAS AND AREAS USED FOR STORAGE OF MATERIALS THAT ARE EXPOSED TO PRECIPITATION SHALL BE INSPECTED FOR EVIDENCE OF, OR THE POTENTIAL FOR, POLLUTANTS ENTERING THE DRAINAGE SYSTEM. EROSION PREVENTION AND SEDIMENT CONTROL MEASURES IDENTIFIED IN THE SWPPP SHALL BE OBSERVED TO ENSURE THAT THEY ARE OPERATING CORRECTLY.
D. OUTFALL POINTS (WHERE DISCHARGES LEAVE THE SITE OR ENTER WATERS OF THE STATE) SHALL BE INSPECTED TO DETERMINE WHETHER EROSION PREVENTION AND SEDIMENT CONTROL MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO RECEIVING WATERS, WHERE DISCHARGE LOCATIONS ARE INDEFINABLE. NEARBY DOWNSTREAM LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFFSITE SEDIMENT TRACKING.
E. BASED ON THE RESULTS OF THE INSPECTION, ANY INADEQUATE CONTROL MEASURES ONLY CONTROL MEASURES IN DISREPAIR SHALL BE REPLACED OR MODIFIED, OR REPAIRED AS NECESSARY, BEFORE THE NEXT RAIN EVENT IF POSSIBLE, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE NEED IS IDENTIFIED.

10. CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL UTILITIES AND NOTIFYING THE APPROPRIATE UTILITY COMPANY BEFORE BEGINNING CONSTRUCTION.
11. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF THE EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF THE UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
12. CONTRACTOR SHALL VERIFY HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING STORM SEWER STRUCTURES, PIPES, AND ALL UTILITIES BEFORE CONSTRUCTION.
13. CONTRACTOR TO REMOVE OR RELOCATE, WHEN APPLICABLE, ALL EXISTING BUILDINGS, FOUNDATIONS, EASEMENTS, AND CONNECTING IMPROVEMENTS, DRAIN PIPES, SANITARY SEWER PIPES, POWER POLES AND GUY WIRES, WATER METERS AND WATER LINES, WELLS SIDEWALKS SIGN POLES, UNDERGROUND GAS, SEPTIC TANKS, AND ASPHALT SHOWN AND NOT SHOWN WITHIN THE CONSTRUCTION LIMITS AND WHERE NEEDED, TO ALLOW FOR FILL MATERIAL, UNLESS OTHERWISE DENOTED, TO BE REMOVED AS UNCLASSIFIED EXCAVATION.
14. CONTRACTOR IS RESPONSIBLE FOR REPAIRS OF DAMAGE TO ANY EXISTING IMPROVEMENTS DURING CONSTRUCTION, SUCH AS, BUT NOT LIMITED TO, UTILITIES, PAVEMENT, STRIPING, CURBS, ETC. REPAIRS SHALL BE EQUAL TO OR BETTER THAN EXISTING CONDITIONS.
15. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS AND SPECIFICATIONS FOR ACTUAL LOCATION OF ALL UTILITY ENTRANCES TO INCLUDE SANITARY SEWER LATERALS, DOMESTIC AND FIRE PROTECTION WATER SERVICE, ELECTRICAL, TELEPHONE, CABLE TV, AND GAS SERVICE. CONTRACTOR SHALL COORDINATE THE INSTALLATION OF UTILITIES IN SUCH A MANNER AS TO AVOID CONFLICTS AND ASSURE PROPER DEPTHS ARE ACHIEVED, AS WELL AS COORDINATING WITH CITY AND UTILITY REQUIREMENTS AS TO LOCATIONS AND SCHEDULING FOR TI-INS AND CONNECTIONS PRIOR TO COMMENCING EXISTING FACILITIES.
16. CONTRACTOR SHALL COORDINATE WITH ARCHITECTURAL PLANS, POWER COMPANY, AND TELEPHONE COMPANY FOR ACTUAL ROUTING OF POWER AND TELEPHONE SERVICE TO BUILDING.
17. CONTRACTOR SHALL PAY ALL FEES AND CHARGES PERTINENT TO UTILITY CONSTRUCTION AND SHALL COORDINATE WITH ALL UTILITIES TO OBTAIN NECESSARY PERMITS AND LICENSES FOR THIS WORK.
18. SEE GEOTECHNICAL INVESTIGATION RECOMMENDATIONS FOR BACKFILLING AND COMPACTION REQUIREMENTS ON UTILITY TRENCHES.
19. CONTRACTOR SHALL COORDINATE WITH OTHER UTILITIES TO ASSURE PROPER DEPTH AND PREVENT ANY CONFLICT OF UTILITIES.
20. THE MINIMUM HORIZONTAL SEPARATION BETWEEN THE CLOSEST TWO POINTS OF THE WATER AND SEWER LINE IS TEN (10) FEET, OR MINIMUM VERTICAL SEPARATION BETWEEN THE CLOSEST TWO POINTS OF THE WATER AND SEWER LINE IS EIGHTEEN (18) INCHES.
21. CONTRACTOR SHALL COORDINATE INSPECTION ON ALL UTILITIES WITH THE APPROPRIATE AUTHORITIES BEFORE CLOSING TRENCHES AT INSTALLATION.
22. ALL EXCAVATIONS SHOULD BE SLOPED OR BRACED AS REQUIRED BY OSHA REGULATIONS TO PROVIDE STABILITY AND SAFE WORKING CONDITIONS. TEMPORARY EXCAVATIONS WILL PROBABLY BE REQUIRED DURING GRADING OPERATIONS. THE CONTRACTOR IS RESPONSIBLE FOR DESIGNING AND CONSTRUCTING STABLE, TEMPORARY EXCAVATIONS AND SHOULD SHORE, SLOPE OR BENCH THE SIDES OF THE EXCAVATIONS AS REQUIRED TO MAINTAIN STABILITY OF BOTH THE EXCAVATION SIDES AND BOTTOM. ALL EXCAVATIONS SHOULD COMPLY WITH APPLICABLE LOCAL, STATE, AND FEDERAL SAFETY REGULATIONS, INCLUDING THE CURRENT OCCUPATIONAL HEALTH AND SAFETY ADMINISTRATION (OSHA) EXCAVATION AND TRENCH SAFETY STANDARDS.
23. CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR WHO CONTROLS THE MEANS, METHODS, AND SEQUENCING OF CONSTRUCTION OPERATIONS. UNDER NO CIRCUMSTANCES SHALL THE INFORMATION PROVIDED HEREIN BE INTERPRETED TO MEAN THAT RESOURCE CONSULTING, LLC IS ASSUMING ANY RESPONSIBILITY FOR CONSTRUCTION SITE SAFETY OR THE CONTRACTOR'S ACTIVITIES; SUCH RESPONSIBILITY SHALL NEITHER BE IMPLIED NOR INFERRED.

WATER NOTES:
1. REFERENCE GENERAL NOTES
2. REFERENCE UTILITY NOTES
3. REFERENCE SITEMARK SPECIFICATIONS
4. REFERENCE DETAIL SHEETS FOR CONSTRUCTION DETAILS
5. ALL WATER TO CONFORM TO
6. REFERENCE ARCHITECTURAL PLANS FOR ALL BUILDING DOMESTIC SERVICE CONNECTION LOCATIONS.
7. CONTRACTOR SHALL COORDINATE THE REMOVAL OR RELOCATION OF ALL EXISTING WATER LINES IN CONFLICT WITH THE PROPOSED CONSTRUCTION.
8. THE CONTRACTOR SHALL MAKE WATER MAIN TAP AND CONSTRUCT METER BOX AND VAULT. CONTRACTOR IS TO PAY ALL FEES, EXTEND SERVICE AND INSTALL AN APPROVED REDUCED PRESSURE BACKFLOW PREVENTION DEVICE AS REQUIRED BY NICHOLSON WATER AND SEWER ASSOCIATION.
9. ALL SPRINKLER AND DOMESTIC LEADS TO THE BUILDING SHALL END 5' OUTSIDE THE FACE OF BUILDING WALL, UNLESS NOTED OTHERWISE, AND SHALL BE PROVIDED WITH A TEMPORARY PLUG (FOR OTHERS TO REMOVE AND EXTEND AS NECESSARY).
10. REFERENCE WATER COMPANY SPECIFICATIONS FOR ON-SITE PIPE TYPE, FITTINGS, VALVES, AND APPURTENANCES BASED ON PIPE SIZE REQUIREMENTS.
SANITARY SEWER NOTES:
1. REFERENCE GENERAL NOTES
2. REFERENCE UTILITY NOTES
3. REFERENCE DETAIL SHEETS FOR CONSTRUCTION DETAILS
4. REFERENCE ARCHITECTURAL PLANS FOR ALL BUILDING SERVICE AND DOMESTIC SERVICE CONNECTION LOCATIONS.
5. CORONATES AND DIMENSIONS ARE SHOWN TO CENTERLINE OF PIPE OR MANHOLE.
6. ALL SUB-OUTS AND WYE LATERALS SHALL BE PLUGGED WITH A STANDARD TYPE PLUG.
7. SANITARY SEWER PIPE OF DIFFERENT MATERIALS SHALL BE JOINED BY A RUBBER SLEEVE WITH STAINLESS STEEL COUPLING, MADE FOR TRANSITIONS FROM ONE PIPE MATERIAL TO ANOTHER.
8. SANITARY SEWER PICKUP POINTS SHALL TERMINATE 5 FEET OUTSIDE THE FACE OF BUILDING, UNLESS NOTED OTHERWISE.
9. THE TOP ELEVATION OF ALL MANHOLES IN GRASSED AREAS SHALL BE 6 INCHES ABOVE FINISHED GRADE.
10. THE SANITARY SEWER PIPE SHALL BE PVC, SDR35 SEWER PIPE UNLESS NOTED OTHERWISE ON PLAN.
11. CONTRACTOR SHALL GROUT AROUND ALL PIPE ENTRANCES TO SANITARY SEWER MANHOLES WITH NON-SHRINKING GROUT TO ASSURE ALL CONNECTIONS ARE WATERTIGHT.

POWER NOTES:
1. REFERENCE GENERAL NOTES
2. REFERENCE UTILITY NOTES
3. REFERENCE DETAIL SHEETS FOR CONSTRUCTION DETAILS
4. REFERENCE ARCHITECTURAL PLANS FOR ALL BUILDING SERVICE LOCATIONS.
5. ALL PRIMARY AND SECONDARY SERVICE LOCATIONS ARE APPROXIMATE AND ARE SHOWN FOR COORDINATION PURPOSES ONLY.
6. CONTRACTOR SHALL COORDINATE THE REMOVAL OF ALL EXISTING POWER FACILITIES IN CONFLICT WITH THE PROPOSED CONSTRUCTION.
7. CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF TRANSFORMER PAD PER POWER COMPANY SPECIFICATIONS.
8. CONTRACTOR IS RESPONSIBLE FOR PROVIDING PRIMARY PVC CONDUIT, TRENCHING, BEDDING, PULL WIRE, AND BACKFILLING. POWER COMPANY TO INSTALL PRIMARY SERVICE UNDERGROUND CONDUIT SHALL BE THREE (3) TWO INCH PVC SCH40 WITH A MINIMUM 48 INCH DEPTH OF COVER AND ALL BENDS SHALL BE LONG SWEEP BENDS.
9. CONTRACTOR IS RESPONSIBLE FOR SECONDARY INSTALLATION TO BUILDING PER THE ELECTRICAL CODE.
10. CONTRACTOR SHALL NOTIFY POWER COMPANY 48 HOURS BEFORE WORK SO THAT CONDUIT INSTALLATION INSPECTIONS MAY BE SCHEDULED.
11. BACKFILLING OF TRENCHES AND COMPACTION SHALL BE IN ACCORDANCE WITH THE GEOTECHNICAL INVESTIGATION RECOMMENDATIONS.

TELEPHONE NOTES:
1. REFERENCE GENERAL NOTES
2. REFERENCE UTILITY NOTES
3. ALL TELEPHONE LINE LOCATIONS ARE APPROXIMATE AND SHOWN FOR COORDINATION PURPOSES ONLY. REFERENCE ARCHITECTURAL PLANS FOR ALL BUILDING SERVICE CONNECTIONS.
4. CONTRACTOR SHALL COORDINATE THE REMOVAL OF ALL EXISTING TELEPHONE FACILITIES IN CONFLICT WITH THE PROPOSED CONSTRUCTION.
5. CONTRACTOR IS RESPONSIBLE FOR INSTALLING TWO (2) TWO INCH PVC SCH 40 CONDUITS FROM THE BUILDING TO THE PROPERTY LINE, OR AS REQUIRED BY THE PHONE COMPANY, INCLUDING TRENCHING, BEDDING, CONDUIT, PULL WIRE, BACKFILLING, AND INSTALLATION OF 4"x8" BACKBOARD WITH #6 AWG COPPER WIRE GROUNDED TO THE MAIN CIRCUIT BREAKER PANEL. ALL BENDS SHALL BE LONG SWEEP BENDS.
6. BACKFILLING OF TRENCHES AND COMPACTION SHALL BE IN ACCORDANCE WITH THE GEOTECHNICAL INVESTIGATION RECOMMENDATIONS.

PAVING NOTES:
1. PAVEMENT SECTION DESIGNS ARE TAKEN FROM THE GEOTECHNICAL INVESTIGATION REPORT AND ADDENDA AS PREPARED BY TERRACON; TELEPHONE (903) 693 7252. REFERENCE THIS REPORT FOR COMPLETE DESIGN SPECIFICATION AND DETAILED RECOMMENDATIONS.
2. ALL PAVING WORK SHALL CONFORM TO THE MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARDS AND SPECIFICATIONS, LATEST EDITION.
3. CONTRACTOR SHALL SUBMIT TO GEOTECHNICAL ENGINEER, SITE ENGINEER, AND CONSTRUCTION MANAGER ALL PAVING MIX DESIGNS PROPOSED FOR USE ON THIS PROJECT AND OBTAIN APPROVAL OF MIXES PRIOR TO PROCEEDING WITH PAVING OPERATIONS.
4. WITHIN 24 HOURS PRIOR TO ANY PAVING OPERATION, THE CONTRACTOR SHALL PERFORM SUBGRADE COMPACTION AND PROOF ROLL TESTS ON PREPARED SUBGRADE TO VERIFY THAT SUBGRADE HAS NOT DETERIORATED DURING PROJECT CONSTRUCTION.
5. ALL CONCRETE TO HAVE BROOM FINISH EXCEPT TRUCK FUELING BAYS TO HAVE HEAVY BROOM FINISH.
6. THE FOLLOWING AREAS ARE TO BE SEALED 30 DAYS AFTER CONCRETE IS POURED. REFERENCE ARCHITECTURAL SPECIFICATION:
- BUILDING SIDEWALKS
- AUTO CANOPY PAD
- TRUCK PARKING PAD
- BUILDING GARAGE SPACES AT FRONT OF BUILDING
- DUMPSTER PAD AREA
- LST AREA (PAD, PIT, DRIVEWAY, UNLOADING AREA, ETC.)
7. ALL ASPHALT THAT ABUTS CONCRETE IS TO BE FINISHED AT 1/8 INCH ABOVE FINISHED CONCRETE ELEVATION.
8. AFTER COMPLETING THE PAVING OPERATION, THE CONTRACTOR IS RESPONSIBLE FOR HAVING PAVEMENTS CORED AND SUBMITTING CORE RESULTS TO ENGINEER AND OWNER.
SITE PREPARATION NOTES:
1. SITE PREPARATION DESIGNS ARE TAKEN FROM THE GEOTECHNICAL INVESTIGATION REPORT AND ADDENDA AS PREPARED BY TERRACON; TELEPHONE (903) 693 7252. REFERENCE THIS REPORT FOR COMPLETE DESIGN SPECIFICATION AND DETAILED RECOMMENDATIONS.
2. THE CONTRACTOR IS TO PREPARE THE SITE IN STRICT ACCORDANCE WITH THE DESIGN SPECIFIED IN THE GEOTECHNICAL INVESTIGATION. REFERENCE THIS COMPLETE DESIGN SPECIFICATION FOR ALL NECESSARY PERMITS AND APPROVALS FROM AGENCIES GOVERNING THE CONSTRUCTION OF THIS WORK SHALL BE SECURED PRIOR TO BEGINNING CONSTRUCTION.
3. CONTRACTOR SHALL RESTRICT AND CONTROL CONSTRUCTION TRAFFIC ON AREAS OF STABILIZED SUBGRADE OR PARTIAL THICKNESS PAVEMENT. HEAVY LOADED EQUIPMENT OPERATION ON THESE SURFACES MAY CAUSE SIGNIFICANT DAMAGE RESULTING IN SUBGRADE DETERIORATION, AND WILL REQUIRE RETESTING AND REWORKING OF SUBGRADE WITHIN THE PROJECT SPECIFICATIONS.

GRADING NOTES:
1. REFERENCE GENERAL NOTES.
2. REFERENCE SITE PREPARATION NOTES.
3. REFERENCE SEDIMENT AND EROSION CONTROL NOTES.
4. REFERENCE DETAIL SHEETS FOR CONSTRUCTION DETAILS
5. CONSTRUCTION SHALL COMPLY WITH ALL GOVERNING CODES AND BE CONSTRUCTED TO THE SAME.
6. PROPERTY AND TOPOGRAPHIC SURVEY WAS PREPARED FOR AND PROVIDED BY THE DEVELOPER FOR USE AS THE BASE MAP IN THIS DESIGN. IT IS ASSUMED TO BE FINAL AND COMPLETE. IF CONTRACTOR DOES NOT DEEPT EXISTING TOPOGRAPHY AS SHOWN ON THE PLANS, WITHOUT EXCEPTION, HE SHALL MAKE A SWEEP AT HIS EXPENSE. A TOPOGRAPHIC SURVEY BY A REGISTERED LAND SURVEYOR AND SUBMIT IT TO THE OWNER AND SITE ENGINEER FOR REVIEW.
7. EXISTING GRADE CONTOURS ARE SHOWN AT ONE FOOT INTERVALS.
8. FINISHED GRADE CONTOUR ELEVATIONS ARE SHOWN AT ONE FOOT INTERVALS.
9. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THIS INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS PRIOR TO ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
10. CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL UTILITIES AND NOTIFYING THE APPROPRIATE UTILITY COMPANY PRIOR TO BEGINNING CONSTRUCTION.
11. CONTRACTOR SHALL VERIFY HORIZONTAL AND VERTICAL LOCATION FOR ALL EXISTING STORM SEWER STRUCTURES, PIPES, AND ALL UTILITIES PRIOR TO CONSTRUCTION.
12. CONTRACTOR SHALL COORDINATE WITH ARCHITECTURAL PLANS, POWER COMPANY, AND TELEPHONE COMPANY FOR ACTUAL ROUTING OF POWER AND TELEPHONE SERVICES TO THE BUILDING.
13. CONTRACTOR IS RESPONSIBLE FOR REPAIRS OF DAMAGE TO ANY EXISTING IMPROVEMENTS DURING CONSTRUCTION, SUCH AS, BUT NOT LIMITED TO, UTILITIES, PAVEMENT, STRIPING, CURBS, ETC. REPAIRS SHALL BE EQUAL TO OR BETTER THAN EXISTING CONDITION.
14. CONTRACTOR SHALL RESTRICT AND CONTROL CONSTRUCTION TRAFFIC ON AREAS OF STABILIZED SUBGRADE OR PARTIAL THICKNESS PAVEMENT. HEAVY LOADED EQUIPMENT OPERATION ON THESE SURFACES MAY CAUSE SIGNIFICANT DAMAGE RESULTING IN SUBGRADE DETERIORATION, AND WILL REQUIRE RETESTING AND REWORKING OF SUBGRADE WITHIN THE PROJECT SPECIFICATIONS.
15. ALL UNSURFACED AREAS DISTURBED BY GRADING OPERATIONS SHALL RECEIVE FOUR INCHES OF TOSOOIL, SEED, MULCH, AND WATER. CONTRACTOR SHALL APPLY STABILIZATION FABRIC TO ALL SLOPES 3:1V OR STEEPER. CONTRACTOR SHALL GRASS DISTURBED AREAS AS INDICATED ON THE LANDSCAPING PLAN AND SPECIFICATIONS AND WATER UNTIL A HEALTHY STAND OF GRASS IS OBTAINED.
16. ALL EXCAVATIONS SHOULD BE SLOPED OR BRACED AS REQUIRED BY OSHA REGULATIONS TO PROVIDE STABILITY AND SAFE WORKING CONDITIONS. TEMPORARY EXCAVATIONS WILL PROBABLY BE REQUIRED DURING GRADING OPERATIONS. THE CONTRACTOR IS RESPONSIBLE FOR DESIGNING AND CONSTRUCTING STABLE, TEMPORARY EXCAVATIONS AND SHOULD SHORE, SLOPE OR BENCH THE SIDES OF THE EXCAVATIONS AS REQUIRED TO MAINTAIN STABILITY OF BOTH THE EXCAVATION SIDES AND BOTTOM. ALL EXCAVATIONS SHOULD COMPLY WITH APPLICABLE LOCAL, STATE, AND FEDERAL SAFETY REGULATIONS, INCLUDING THE CURRENT OCCUPATIONAL HEALTH AND SAFETY ADMINISTRATION (OSHA) EXCAVATION AND TRENCH SAFETY STANDARDS.
17. CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR WHO CONTROLS THE MEANS, METHODS, AND SEQUENCING OF CONSTRUCTION OPERATIONS. UNDER NO CIRCUMSTANCES SHALL THE INFORMATION PROVIDED HEREIN BE INTERPRETED TO MEAN THAT RESOURCE CONSULTING, LLC IS ASSUMING ANY RESPONSIBILITY FOR CONSTRUCTION SITE SAFETY OR THE CONTRACTOR'S ACTIVITIES; SUCH RESPONSIBILITY SHALL NEITHER BE IMPLIED NOR INFERRED.
18. CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR WHO CONTROLS THE MEANS, METHODS, AND SEQUENCING OF CONSTRUCTION OPERATIONS. UNDER NO CIRCUMSTANCES SHALL THE INFORMATION PROVIDED HEREIN BE INTERPRETED TO MEAN THAT RESOURCE CONSULTING, LLC IS ASSUMING ANY RESPONSIBILITY FOR CONSTRUCTION SITE SAFETY OR THE CONTRACTOR'S ACTIVITIES; SUCH RESPONSIBILITY SHALL NEITHER BE IMPLIED NOR INFERRED.
19. CONSTRUCTION SHALL COMPLY WITH ALL GOVERNING CODES AND BE CONSTRUCTED TO THE SAME.
20. ANY WORK IN THE STATE'S RIGHT-OF-WAY SHALL BE PERFORMED IN ACCORDANCE WITH THE TEXAS DEPARTMENT OF TRANSPORTATION STANDARD DRAWINGS AND STANDARD SPECIFICATIONS.
21. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ADDRESS ANY WETLANDS ISSUES TO THE SATISFACTION OF THE U.S. ARMY CORPS OF ENGINEERS.
22. REFERENCE DETAIL SHEETS FOR CONSTRUCTION DETAILS

STORM DRAINAGE NOTES:
1. REFERENCE GENERAL NOTES
2. REFERENCE DETAIL SHEETS FOR CONSTRUCTION DETAILS
3. ALL PIPES ENTERING STORM SEWER STRUCTURES SHALL BE SEALED TO ASSURE CONNECTION AT STRUCTURE IS WATER TIGHT.
4. ALL PIPES AND STRUCTURES ON STREET RIGHT-OF-WAY SHALL BE PER LOCAL OR DEPARTMENT OF TRANSPORTATION STANDARDS, PER OWNERSHIP JURISDICTION.
5. TYPES OF PIPE ARE INDICATED ON THE PLANS AND SPECIFIED WITHIN THE SPECIFICATIONS.
6. ALL FLEXIBLE PIPE SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS. DEFORMED OR DEFLECTED PIPE MORE THAN 5 PERCENT SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE. TEST SHALL BE PERFORMED WITH THE USE OF A MANHOLE OR CONE BEING 95 PERCENT OF THE NOMINAL DIAMETER OF THE PIPE DIAMETER. TEST RESULTS SHALL BE SUBMITTED TO THE ENGINEER AND OWNER WITHIN 15 DAYS OF INSTALLATION.
7. CONTRACTOR SHALL NOT ALLOW WATER TO POND AT SUBGRADE OR BASE MATERIAL ADJACENT TO CURB INLETS AND CATCH BASINS PRIOR TO PLACEMENT OF PAVEMENT. TEMPORARY PROVISIONS SUCH AS DOWNSLOPE AND INSTALLATIONS OF SUBDRAINS SHALL BE TAKEN TO KEEP THE SUBGRADE DRY DURING CONSTRUCTION.

UTILITY NOTES:
1. REFERENCE GENERAL NOTES
2. REFERENCE CONTACTS LIST ON THE TITLE SHEET.
3. REFERENCE DETAIL SHEETS FOR CONSTRUCTION DETAILS
4. SITEMARK FOR THIS PROJECT SHALL MEET OR EXCEED THE PROJECT SITEMARK SPECIFICATIONS.
5. CONSTRUCTION TO COMPLY WITH ALL GOVERNING CODES AND BE CONSTRUCTED TO SAME.
6. CONSTRUCTION SHALL COMPLY WITH ALL GOVERNING CODES (I.E. CITY, COUNTY, POWER, TELEPHONE, ETC. STANDARDS AND SPECIFICATIONS) AND REQUIREMENTS. THE CONTRACTOR SHALL CONDUCT ALL REQUIRED TESTS TO THE SATISFACTION OF THE RESPECTIVE UTILITY COMPANIES AND OWNERS INSPECTING AUTHORITIES.
7. THE CONTRACTOR IS RESPONSIBLE FOR EXTENDING SERVICE LINES TO THE WATER AND SANITARY SEWER MAINS, AND FOR PICKING UP GAS SERVICE AT THE DOWNSTREAM SIDE OF THE METER.
8. CONTRACTOR SHALL COORDINATE PROPOSED RECONNECTION OF ALL UTILITIES WITH ARCHITECTURAL PLANS AS WELL AS UTILITY COMPANIES AND BUILDING CONTRACTOR.
9. CONTRACTOR SHALL COMPLY FULLY WITH THE LATEST STANDARD OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SHORING, BEDDING, AND OTHER MEANS OF PROTECTION. THIS INCLUDES BUT IS NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE TO COMPLY WITH PERFORMANCE CRITERIA FOR OSHA.

GENERAL NOTES:
1. ALL WORK SHALL COMPLY WITH PEARL RIVER COUNTY CODE OF ORDINANCES.
2. ALL WORK FOR UTILITY IMPROVEMENTS TO BE OWNED AND MAINTAINED BY NICHOLSON WATER AND SEWER ASSOCIATION SHALL COMPLY WITH THE ASSOCIATION'S SPECIFICATIONS.
3. SEE TITLE SHEET FOR CONTACT PERSONS LIST FOR GOVERNING AGENCIES AND UTILITIES. PROPERTY AND TOPOGRAPHIC SURVEY WAS PREPARED FOR AND PROVIDED BY THE DEVELOPER FOR USE AS THE BASE MAP IN THIS DESIGN. THE SURVEY IS ASSUMED TO BE FINAL AND COMPLETE. THIS SURVEY WAS PREPARED BY BRACEY LAND SURVEYING, TELEPHONE (601) 736 1221.
4. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULE, SLOPE PAVING, SIDEWALKS, EXIT PORCHES, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS, AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
5. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS AND SPECIFICATIONS FOR EXACT ACTUAL LOCATION OF ALL SERVICES INCLUDING ELECTRICAL, TELEPHONE, AND GAS SERVICE.
6. CONTRACTOR SHALL COORDINATE INSTALLATION OF UTILITIES IN SUCH A MANNER AS TO AVOID CONFLICTS AND ASSURE PROPER DEPTHS ARE ACHIEVED AS WELL AS COORDINATING WITH THE REGULATORY AGENCY AS TO THE LOCATION AND TI-INS/ CONNECTIONS TO THEIR FACILITIES. CONTRACTOR IS RESPONSIBLE FOR PROTECTING EXISTING BENCHMARK AND RELOCATING BENCHMARK IF NECESSARY.
7. EXISTING UTILITY LINES ARE APPROXIMATE LOCATION ONLY. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITY LINE LOCATIONS PRIOR TO ANY CONSTRUCTION. ANY DEVIATIONS FROM THE DESIGN LOCATION SHALL BE REPORTED TO THE OWNER AND THE ENGINEER PRIOR TO CONSTRUCTION.
8. ALL NECESSARY PERMITS AND APPROVALS FROM AGENCIES GOVERNING THE CONSTRUCTION OF THIS WORK SHALL BE SECURED PRIOR TO BEGINNING CONSTRUCTION.
9. CONTRACTOR IS RESPONSIBLE FOR OBTAINING APPROVAL FROM THE POWER AND TELEPHONE COMPANIES FOR LOCATION AND HEIGHT OF Pylon SIGN BEFORE CONDUIT AND WIRING IS INSTALLED.
10. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL RELOCATION, INCLUDING BUT NOT LIMITED TO ALL UTILITIES, STORM DRAINAGE, SIGNS, TRAFFIC SIGNALS AND POLES, ETC., AS REQUIRED FOR WIDENING OF ALL ROADWAYS. ALL WORK SHALL BE IN ACCORDANCE WITH GOVERNING AUTHORITIES SPECIFICATIONS AND SHALL BE APPROVED BY SUCH. ALL COSTS SHALL BE INCLUDED IN THE BASE BID.
11. THE CONTRACTOR IS RESPONSIBLE FOR THE PROTECTION FOR ALL AREAS INDICATED TO REMAIN UNDISTURBED TO REMAIN AS BUFFERS. ALL PROPERTY CORNERS, AND REPLACING ALL PINS ELIMINATED OR DAMAGED DURING CONSTRUCTION.
12. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRS OR DAMAGE TO ANY EXISTING IMPROVEMENTS DURING CONSTRUCTION, SUCH AS BUT NOT LIMITED TO, DRAINAGE, UTILITIES, PAVEMENT, STRIPING, CURB ETC. REPAIRS SHALL BE EQUAL TO OR BETTER THAN EXISTING CONDITIONS.
13. SITE CONTRACTOR SHALL COORDINATE THE INSTALLATION OF ALL UNDERGROUND UTILITIES WITH HIS WORK. ALL UNDERGROUND UTILITIES (WATER, SANITARY SEWER, STORM SEWER, ELECTRICAL CONDUIT, IRRIGATION SYSTEMS, AND ANY OTHER MISCELLAN

U.S. Interstate 59

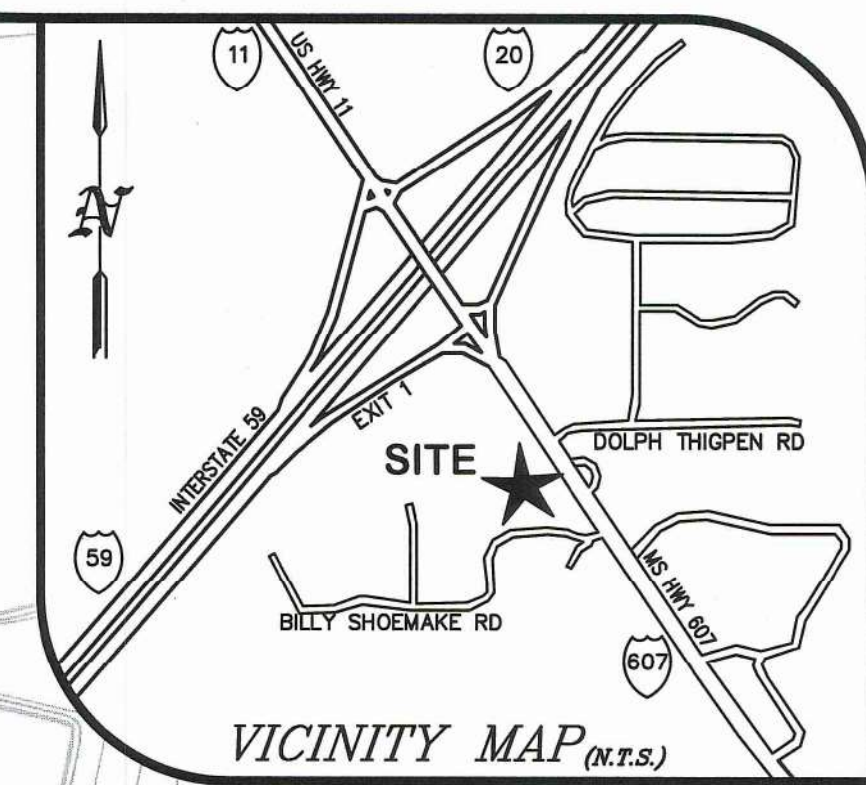
Exit 1 Ramp

LEGEND FOR PROPOSED IMPROVEMENTS

- PROPERTY LINE / R.O.W.
- PROPOSED BUILDING
- PROPOSED CURB AND GUTTER
- PROPOSED PARKING SPACES
- PROPOSED FENCING
- SYSL/4" - SINGLE YELLOW SOLID LINE / 4 INCH THICK
- DYSL/5" - DOUBLE YELLOW SOLID LINE / 5 INCH THICK EACH
- SWSL/24" - SINGLE WHITE SOLID LINE / 24 INCH THICK
- SWSL/8" - SINGLE WHITE SOLID LINE / 8 INCH THICK
- SWSL/4" - SINGLE WHITE SOLID LINE / 4 INCH THICK
- SWDL/4" - SINGLE WHITE DASHED LINE / 4 INCH THICK

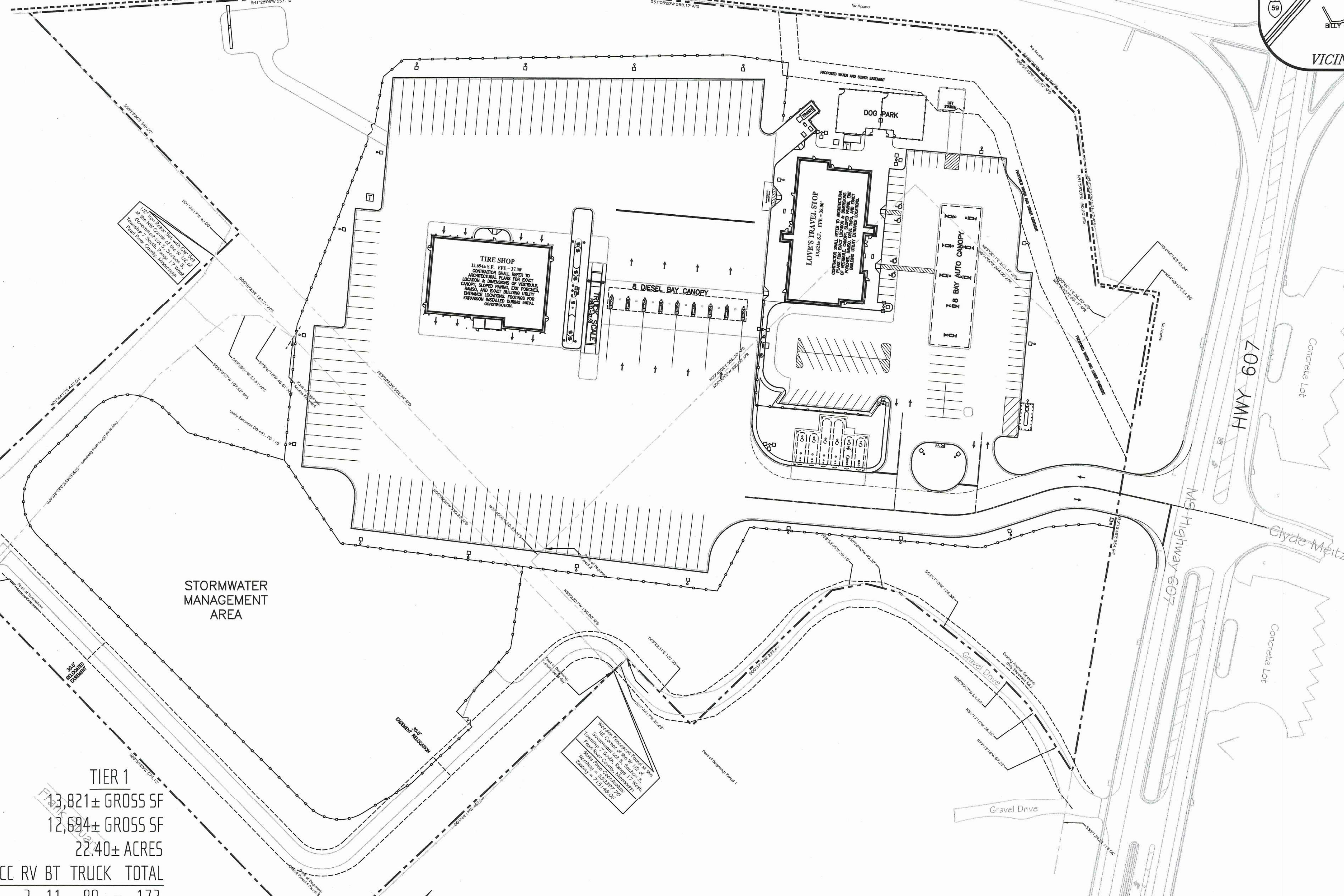
EXISTING LEGEND

- SUBJECT PROPERTY BOUNDARY LINE
- ADJACENT BOUNDARY LINES
- OLD WIRE FENCE
- *40" LINES
- LIMITS OF EASEMENT
- BURIED FIBER OPTIC LINE
- OVERHEAD UTILITY LINES
- MAJOR CONTOUR
- MINOR CONTOUR
- DRAINAGE PIPE
- SIGNIFICANT DRAINAGE FLOWLINE
- APPROXIMATE FLOOD ZONE LOCATION
- NO HIGHWAY ACCESS
- PAINT STRIPE
- 1/2" IRON REBAR SET
- IRON PIN FOUND
- FENCE CORNER FOUND
- UTILITY POLE
- APS AS PER SURVEY
- APR AS PER RECORD
- ROW RIGHT-OF-WAY
- G2 CONTOUR ELEVATION
- RCP REINFORCED CONCRETE PIPE
- CMP CORRUGATED METAL PIPE
- ROAD SIGN POST



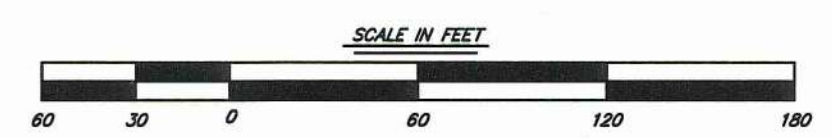
RC 200325
 file: 200325.rvt
 drawn: DMT
 date: 03-01-2023

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SITE ANALYSIS

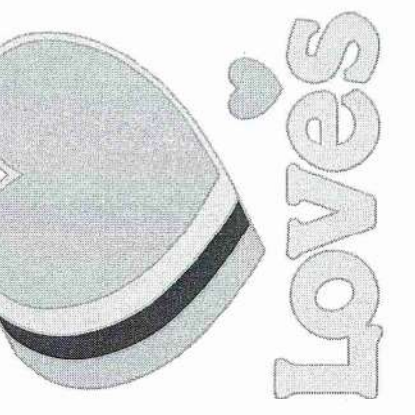
LOVE'S TRAVEL STOP	TIER 1					
LOVE'S TIRE SHOP	13,821± GROSS SF					
TOTAL SITE AREA	12,694± GROSS SF					
	22.40± ACRES					
PARKING SUMMARY	AUTO	ACC	RV	BT	TRUCK	TOTAL
SPACES PROVIDED	74	4	3	11	80	= 172



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OVERALL SITE PLAN

LOVE'S TRAVEL STOP
 INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
 PEARL RIVER COUNTY, MISSISSIPPI



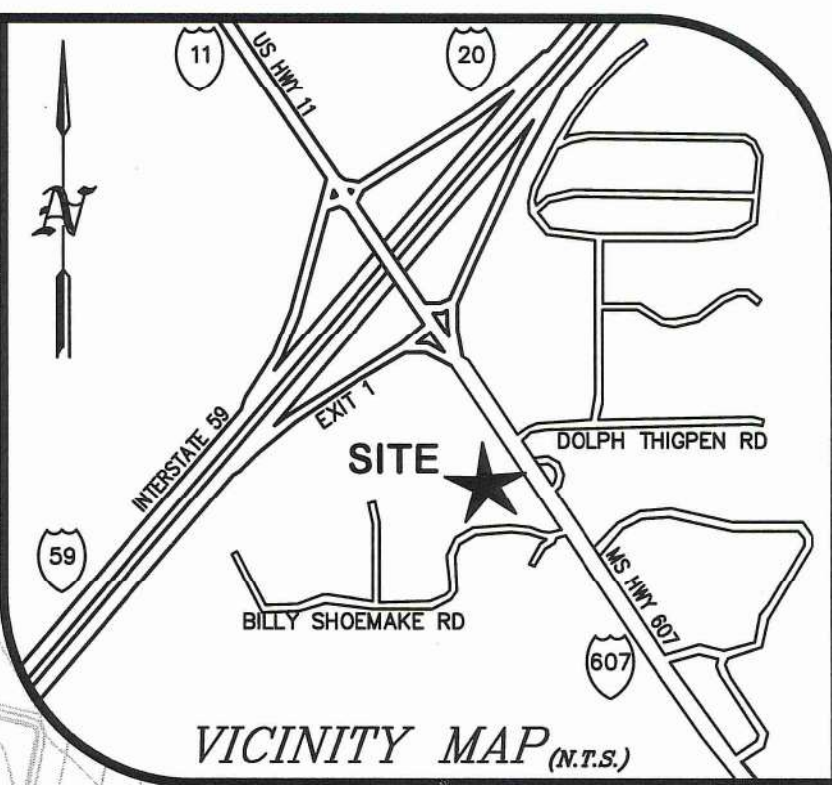
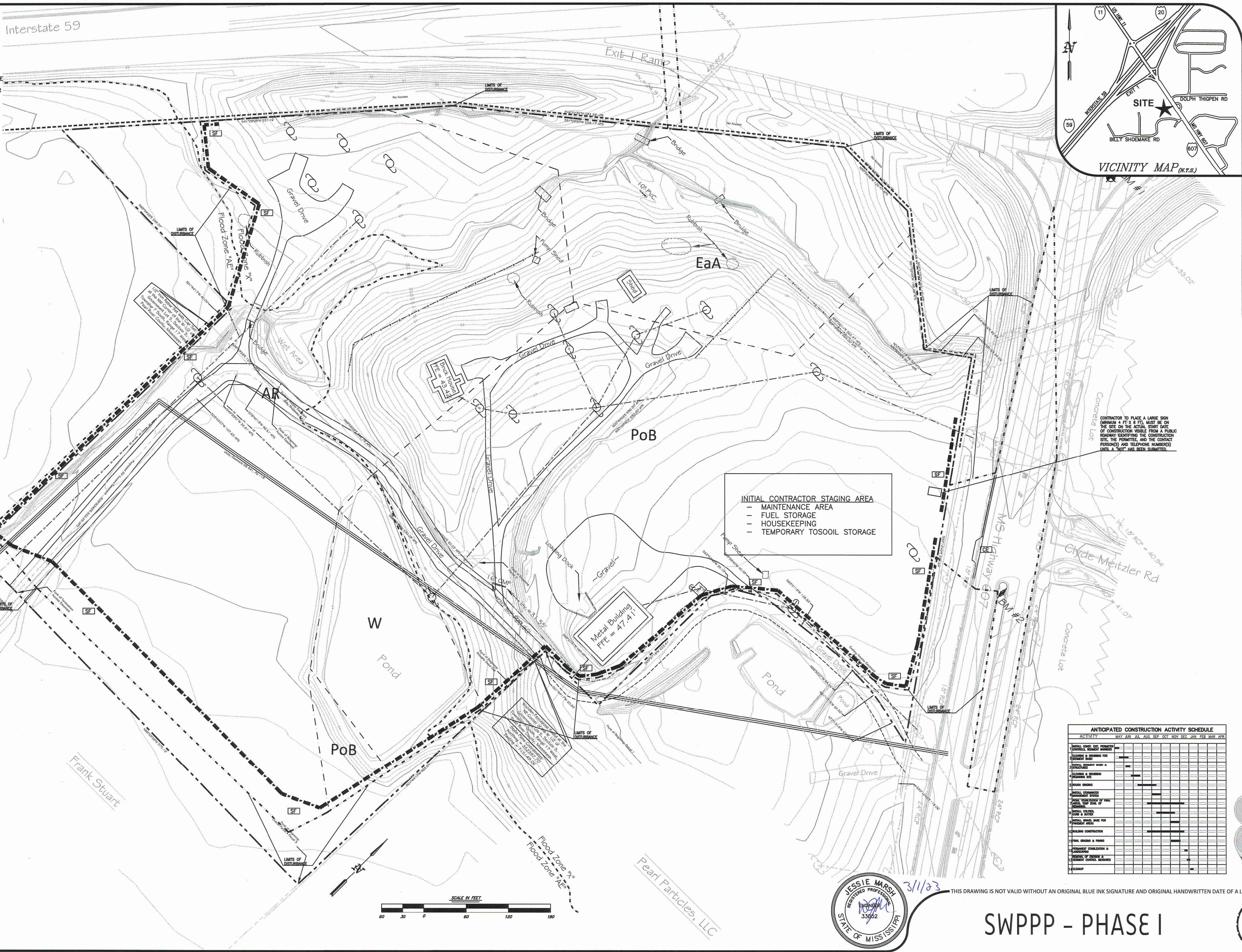
C3.0

K:\200325 - Pearl River County MS Love's I-59 @ MS Hwy 607 SEQ\Design\200325.rvt design.dwg, C3.0 OVERALL SITE PLAN, 3/1/2023 4:35:05 PM, Jessie, Resource Consulting, LLC copyright

LEGEND FOR PROPOSED IMPROVEMENTS

- PROPERTY LINE / R.O.W.
- PROPOSED BUILDING
- PROPOSED TYPE III SILT FENCE
- PROPOSED CURB AND GUTTER
- LIMITS OF DISTURBANCE
- PROPOSED INLET
- PROPOSED CURB INLET
- PROPOSED STORM SEWER
- IP-CG [Symbol] INLET PROTECTION
- RR [Symbol] STORM DRAIN OUTLET PROTECTION
- SO [Symbol] STABILIZATION - SOD
- SO [Symbol] STABILIZATION - PERMANENT VEGETATION
- TS [Symbol] STABILIZATION - TEMPORARY VEGETATION
- FS [Symbol] FAIRCLOTH SKIMMER
- SB [Symbol] SEDIMENT BASIN
- CE [Symbol] CONSTRUCTION ENTRANCE
- CD-R [Symbol] ROCK CHECK DAM
- AR ARKABUTLA-ROSEBLOOM ASSOCIATION
- EaA ESCAMBIA FINE SANDY LOAM
- PoB POARCH LOAM

- EXISTING LEGEND**
- SUBJECT PROPERTY BOUNDARY LINE
 - ADJACENT BOUNDARY LINES
 - OLD WIRE FENCE
 - "40" LINES
 - LIMITS OF EASEMENT
 - BURIED FIBER OPTIC LINE
 - OVERHEAD UTILITY LINES
 - MAJOR CONTOUR
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 - ROW RIGHT-OF-WAY
 - CONTOUR ELEVATION
 - RCP REINFORCED CONCRETE PIPE
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 - ROAD SIGN POST



INITIAL CONTRACTOR STAGING AREA

- MAINTENANCE AREA
- FUEL STORAGE
- HOUSEKEEPING
- TEMPORARY TOSOIL STORAGE

ANTICIPATED CONSTRUCTION ACTIVITY SCHEDULE

ACTIVITY	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC	JAN	FEB	MAR	APR
1. INITIAL CONSTRUCTION PERMITS												
2. CONTROL DISTURBED AREAS												
3. EROSION CONTROL												
4. STABILIZATION												
5. DRAINAGE & EROSION CONTROL												
6. ROADWORK												
7. METAL BUILDING CONSTRUCTION												
8. STABILIZATION OF FLOOD ZONE												
9. METAL BUILDING CONSTRUCTION												
10. METAL BUILDING CONSTRUCTION												
11. METAL BUILDING CONSTRUCTION												
12. METAL BUILDING CONSTRUCTION												
13. METAL BUILDING CONSTRUCTION												
14. METAL BUILDING CONSTRUCTION												
15. METAL BUILDING CONSTRUCTION												
16. METAL BUILDING CONSTRUCTION												
17. METAL BUILDING CONSTRUCTION												
18. METAL BUILDING CONSTRUCTION												
19. METAL BUILDING CONSTRUCTION												
20. METAL BUILDING CONSTRUCTION												

RC 200325
 File: 200325v7
 Drawn: DMT
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LOVE'S TRAVEL STOP
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 PEARL RIVER COUNTY, MISSISSIPPI



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SWPPP - PHASE I

C4.1





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LOVE'S TRAVEL STOP
INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
PEARL RIVER COUNTY, MISSISSIPPI



C4.4

**BMPs
Sediment Control**

Planning Considerations

- For temporary protection, materials such as straw bales or fabric barriers can be used; for permanent protection, block inlets or sodding can be placed around drain inlets.
- This practice should be put into place prior to grading.

Recommended Specifications

Method	Practice Specific Recommendations
Perimeter Excavation	<ul style="list-style-type: none"> Excavation depth: 1 ft (min.); 2 ft (max.) Excavation volume: 35 yd³ /acre (min.) Excavated slopes: 2:1 ratio (max.)
Fabric Barrier Inlet Protection	<ul style="list-style-type: none"> Stake placement: 3 ft apart (max.) Fabric extension: 1 ft (min.) below soil surface 1.5 ft (max.) above soil surface
Gravel/Concrete Barriers	<ul style="list-style-type: none"> Block height: 1 ft (min.); 2 ft (max.) Block extension: 2 inch (min.) below soil surface Mortar required 1/2" wire mesh should be used over block openings Gravel should be placed along outside of blocks
Sod Inlet Protection	<ul style="list-style-type: none"> Sod extension: 4 ft (min.) from inlet; all directions Slope: 4:1 (max.) for sodded area Sod should be placed in staggered arrangement

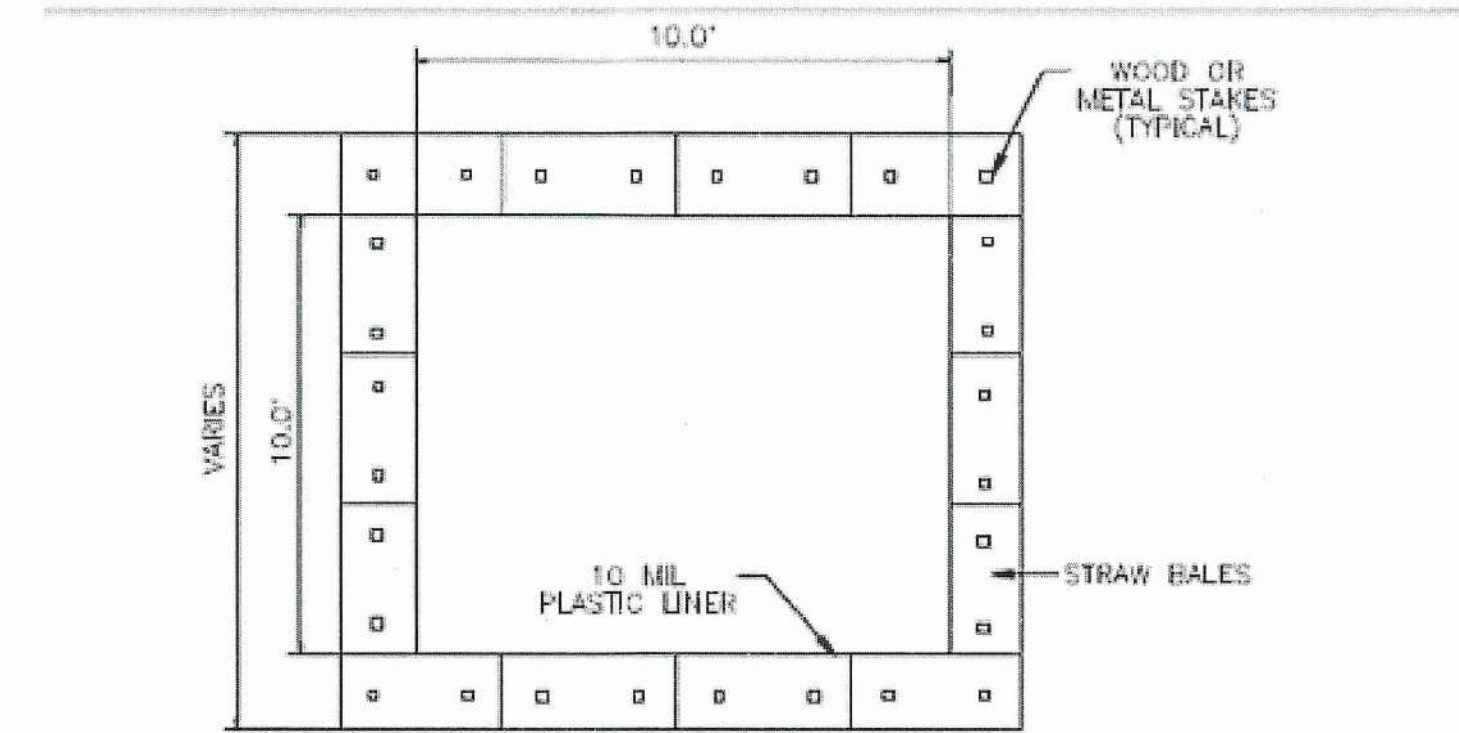
Maintenance

- Frequent cleaning to prevent clogging of the water intake and subsequent erosion of unprotected areas
- Repair and replacement of temporary material structures as necessary after storm events

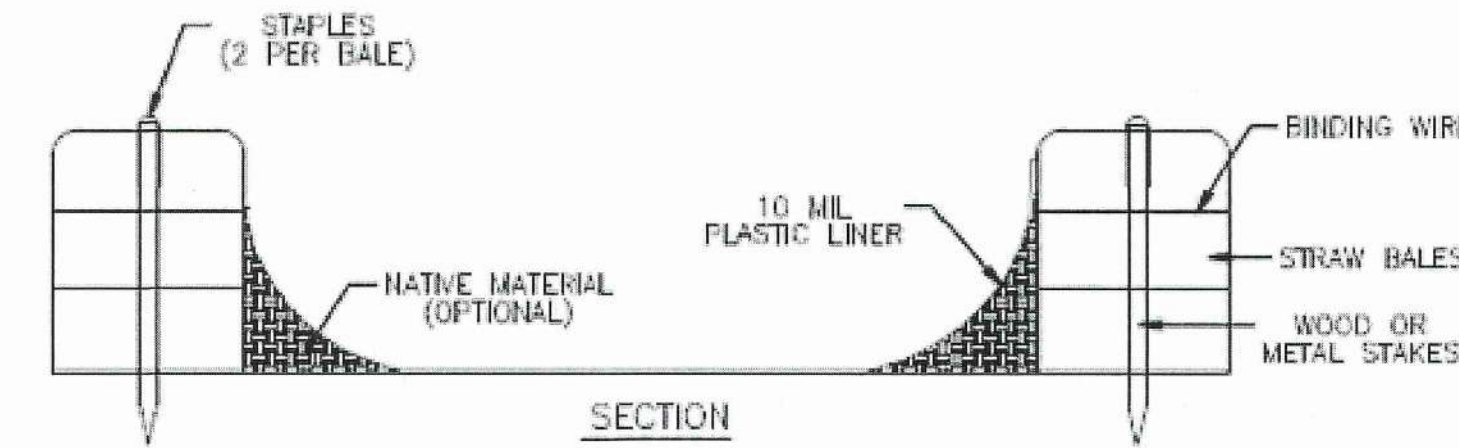
Louisiana Coastal Zone BMPs Urban Storm Water Runoff

74

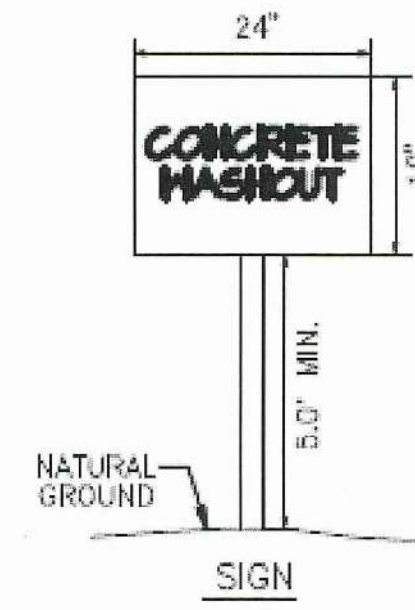
Sediment Control



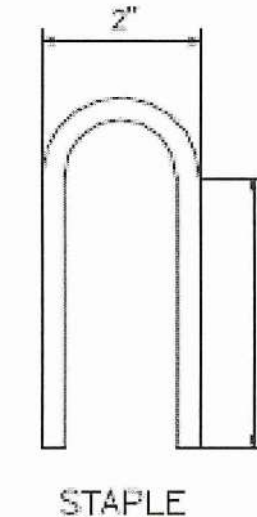
PLAN



SECTION



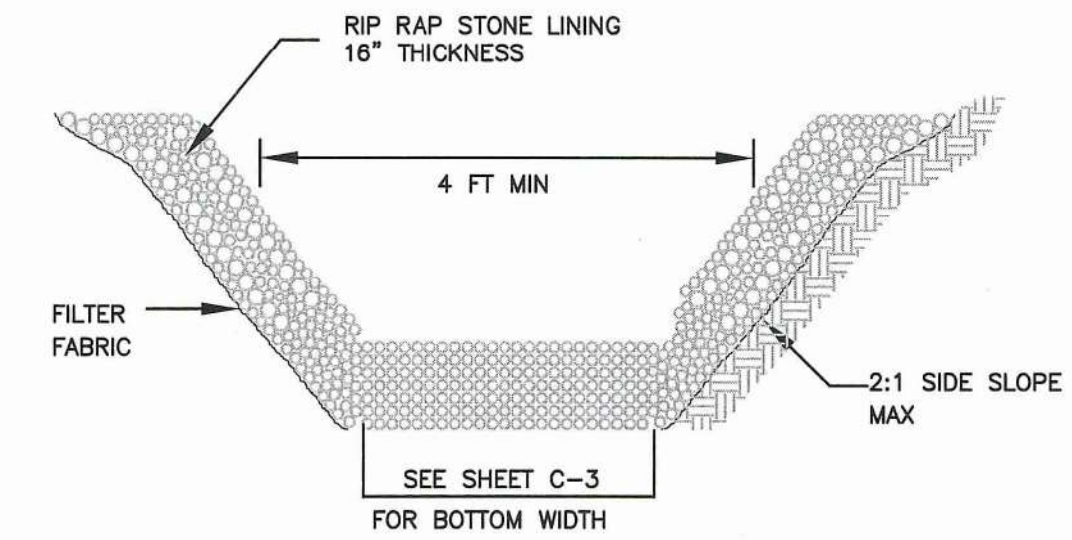
SIGN



STAPLE

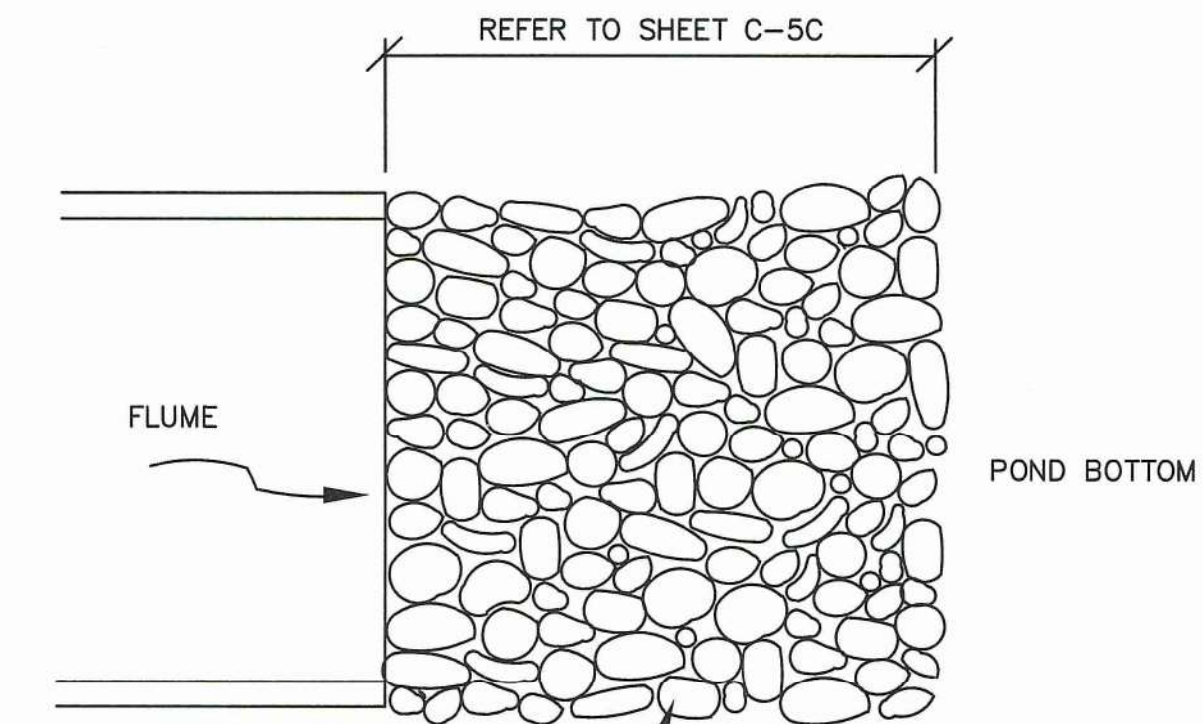
NOTES:

- NO CONCRETE WASHOUT SHALL BE PLACED WITHIN 50' OF STORM DRAIN, OPEN DITCHES, OR WATER BODIES.
- WASHOUT FACILITY SHOULD BE PLACED IN A LOCATION THAT IS CONVENIENT FOR CONCRETE TRUCKS.
- THE CONCRETE WASHOUT SIGN SHALL BE PLACED WITHIN 30 FEET OF THE WASHOUT FACILITY.
- THE CONCRETE WASHOUT LETTERS SHALL BE A MINIMUM OF 3 INCHES IN HEIGHT.



NOTE:
RIP RAP SIZE SHALL RANGE FROM 3" MIN. 12" MAX. WITH AVERAGE SIZE OF 6". AT LEAST 50% OF ROCK SHALL CONSIST OF PIECES LARGER THAN THE AVERAGE SIZE. PIECES SMALLER THAN THE SMALLER SIZE SHALL NOT EXCEED 15% OF THE TONNAGE SHIPPED.

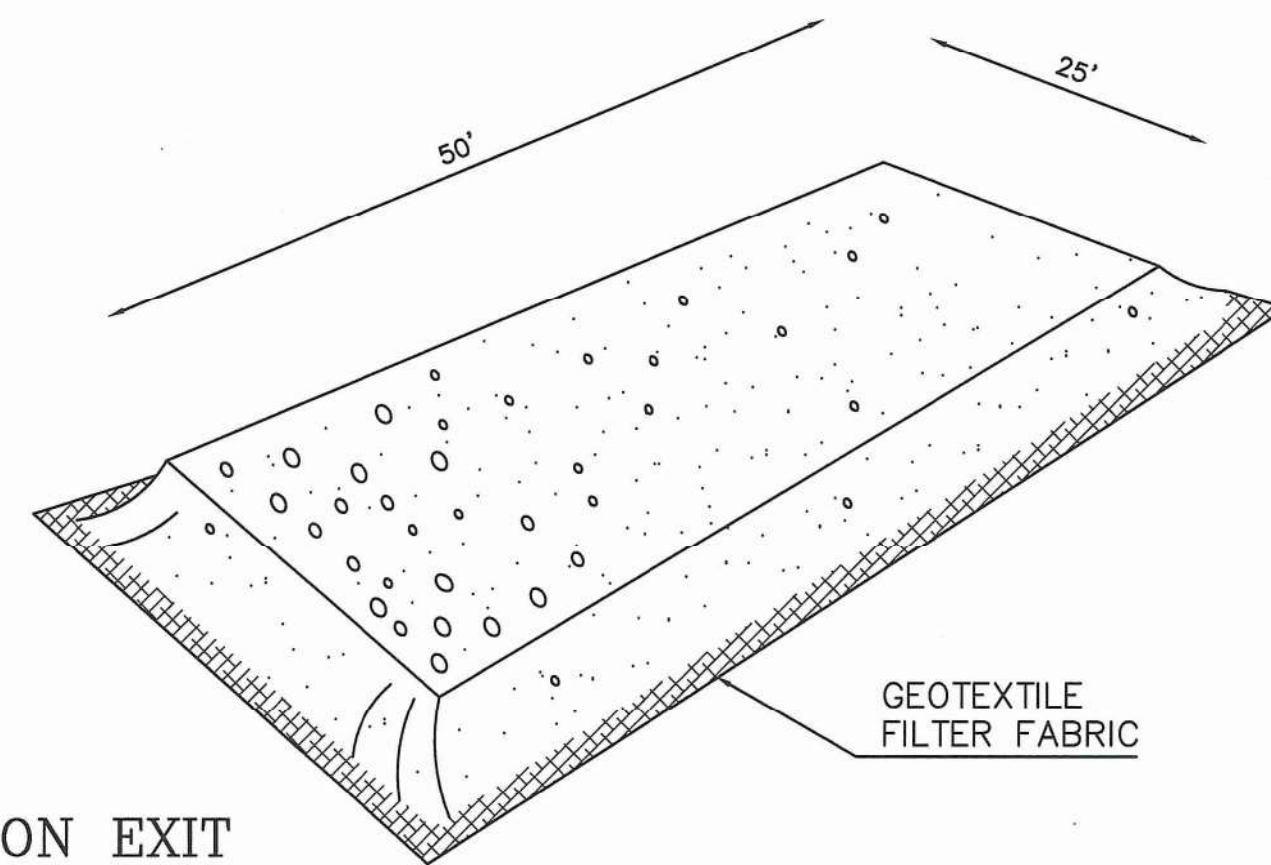
FLAT BOTTOM DITCH WITH RIP RAP LINING



RIP RAP DETAIL

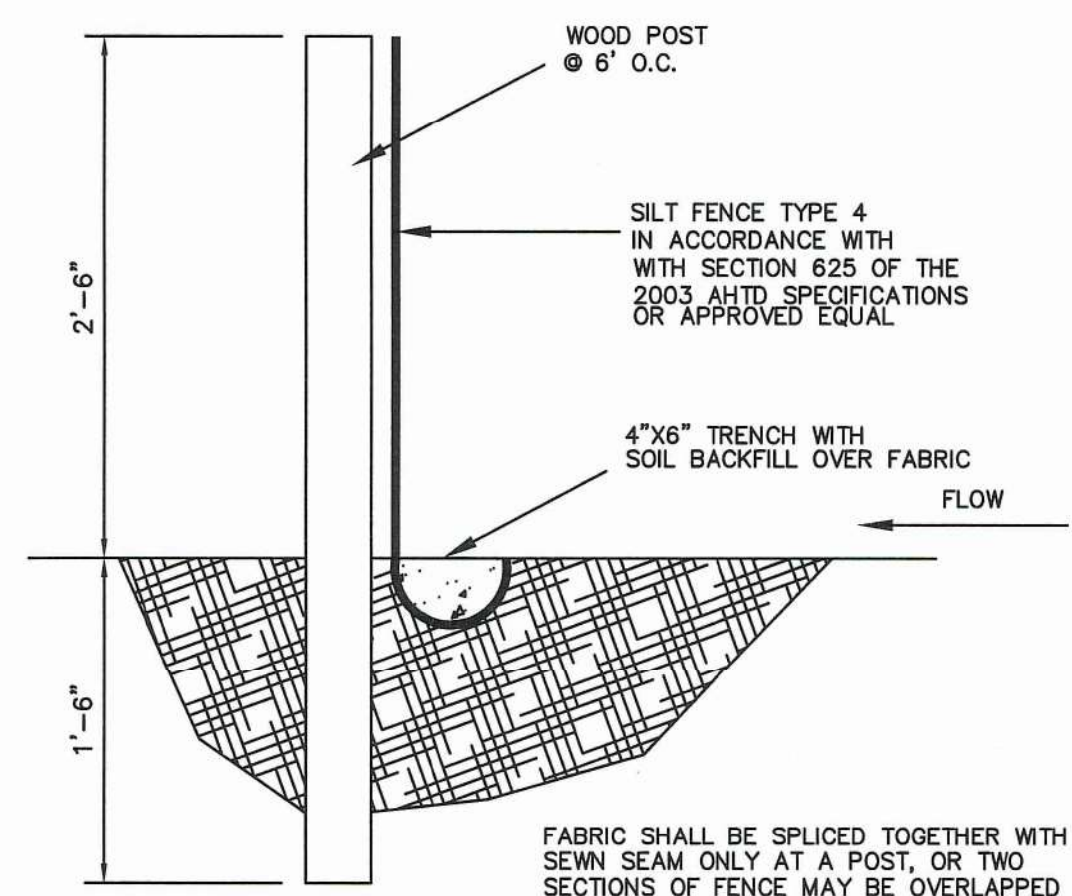
RR

- * STONE SIZE: ASTM D448 SIZE #1 #1(1 1/2" TO 3 1/2" DIAMETER)
- * PAD THICKNESS >= 6"
- * MAY REQUIRE PERIODIC TOP DRESSING WITH 2" STONE
- * PLACE LAYER OF GEOTEXTILE FILTER FABRIC UNDER STONE



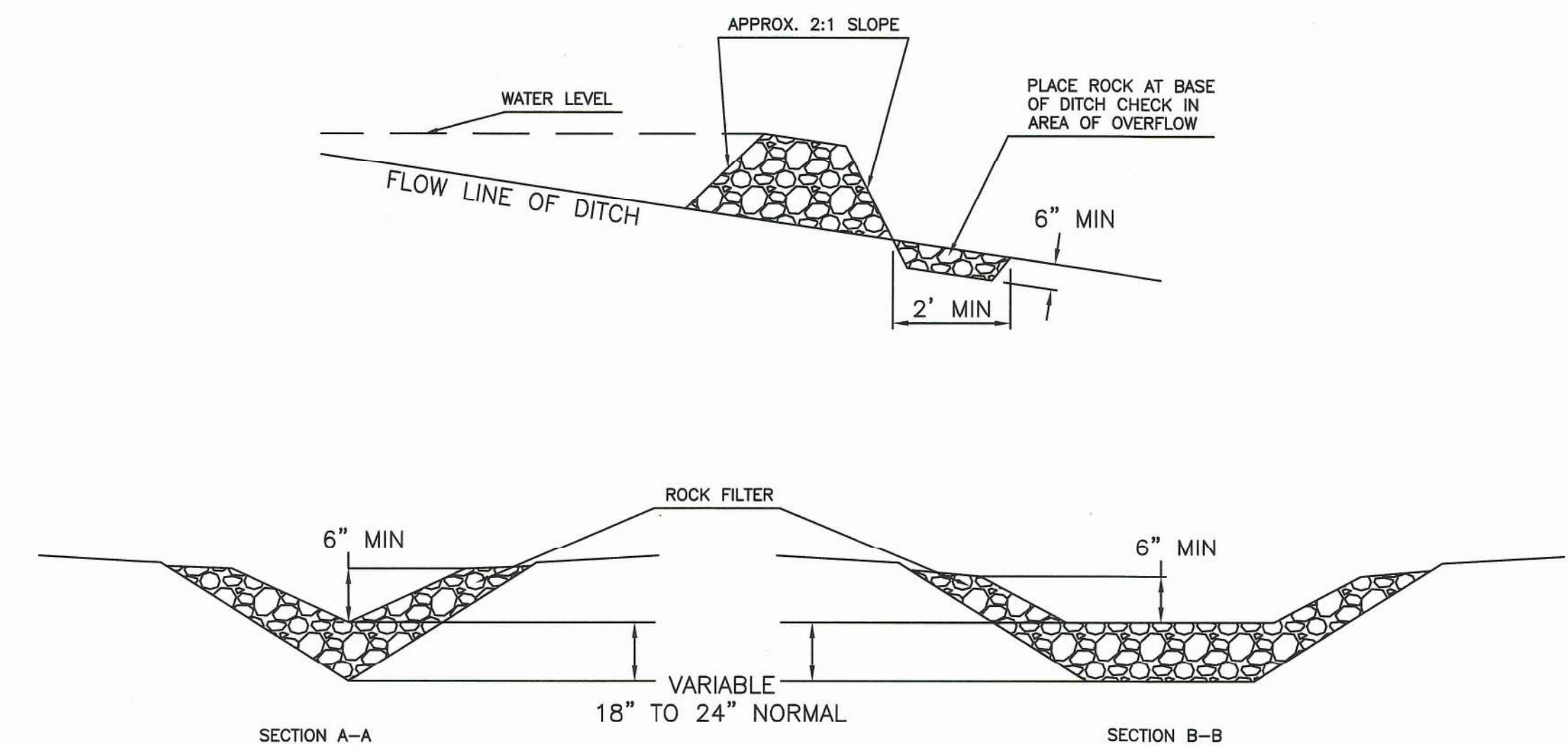
TEMPORARY CONSTRUCTION EXIT

CE



SILT FENCE DETAIL

SF



ROCK DITCH CHECK (E-6)

CD-R



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SWPPP DETAILS



Know what's below.
Call before you dig.



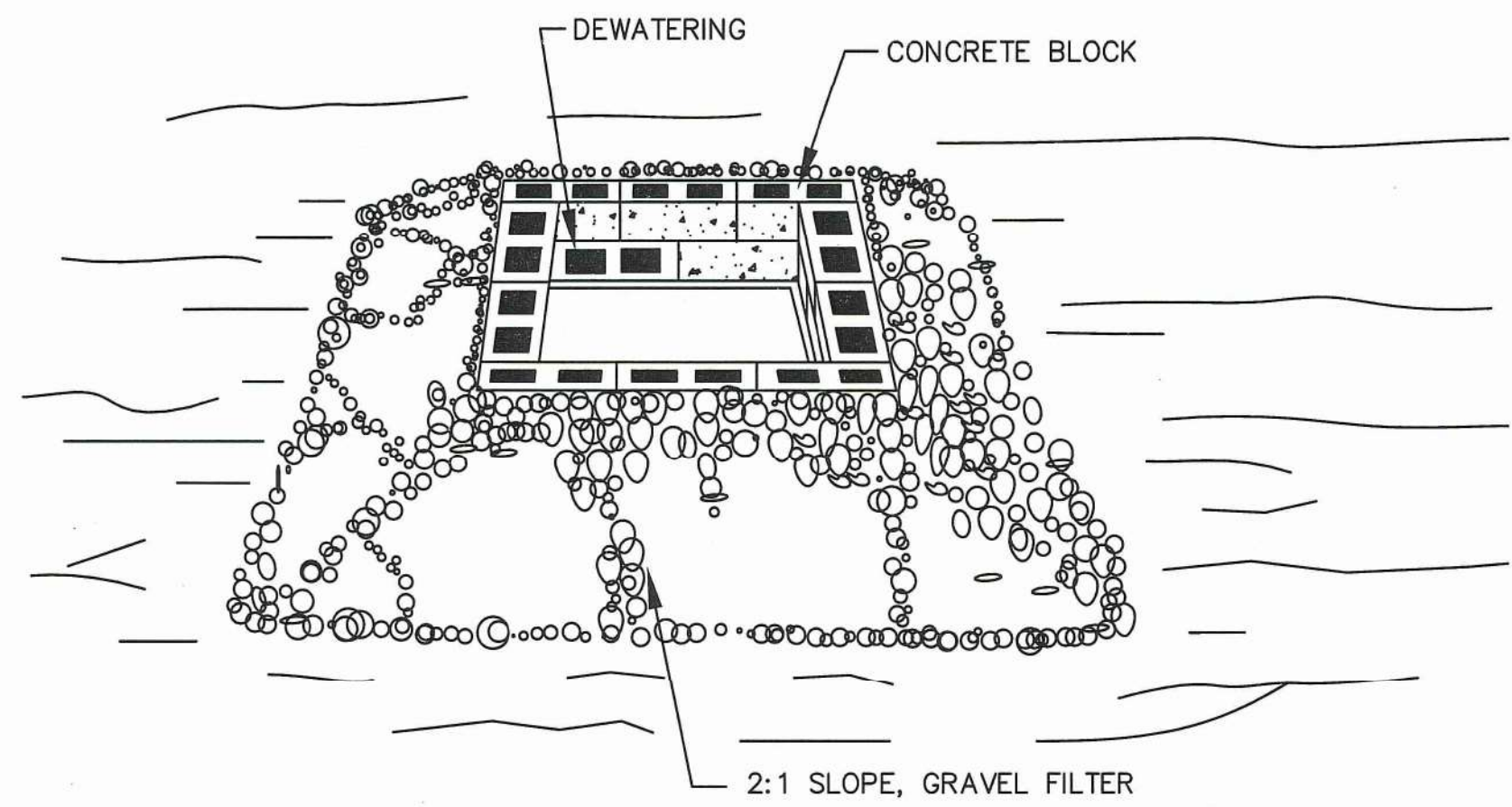
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LOVE'S TRAVEL STOP
INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
PEARL RIVER COUNTY, MISSISSIPPI



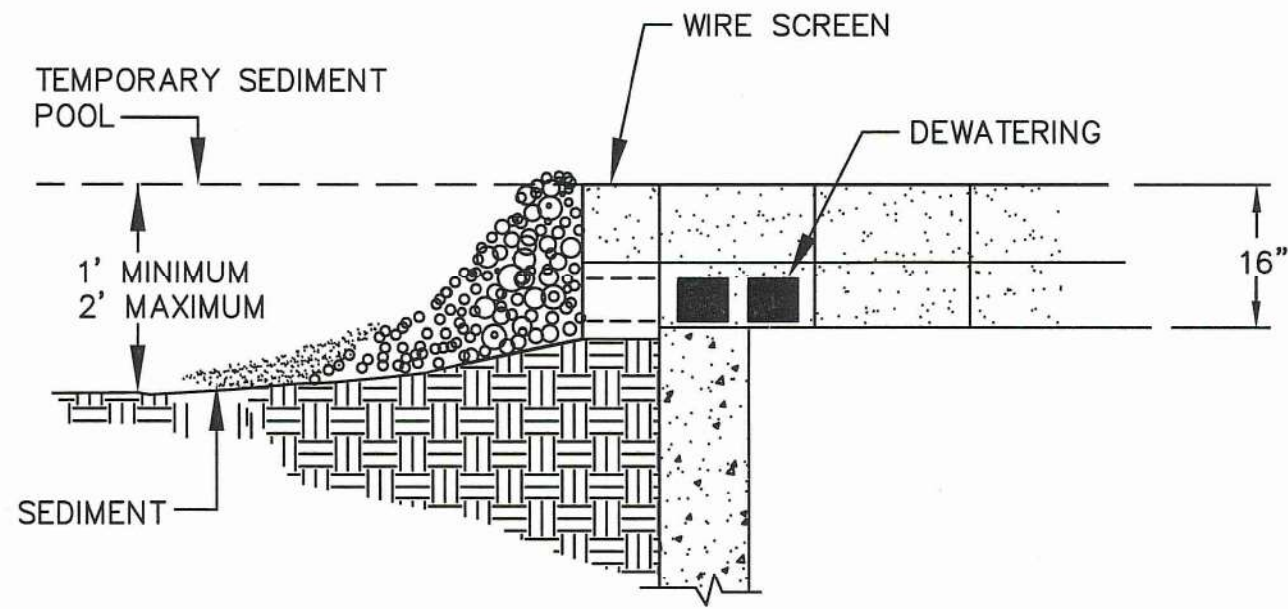
C4.5



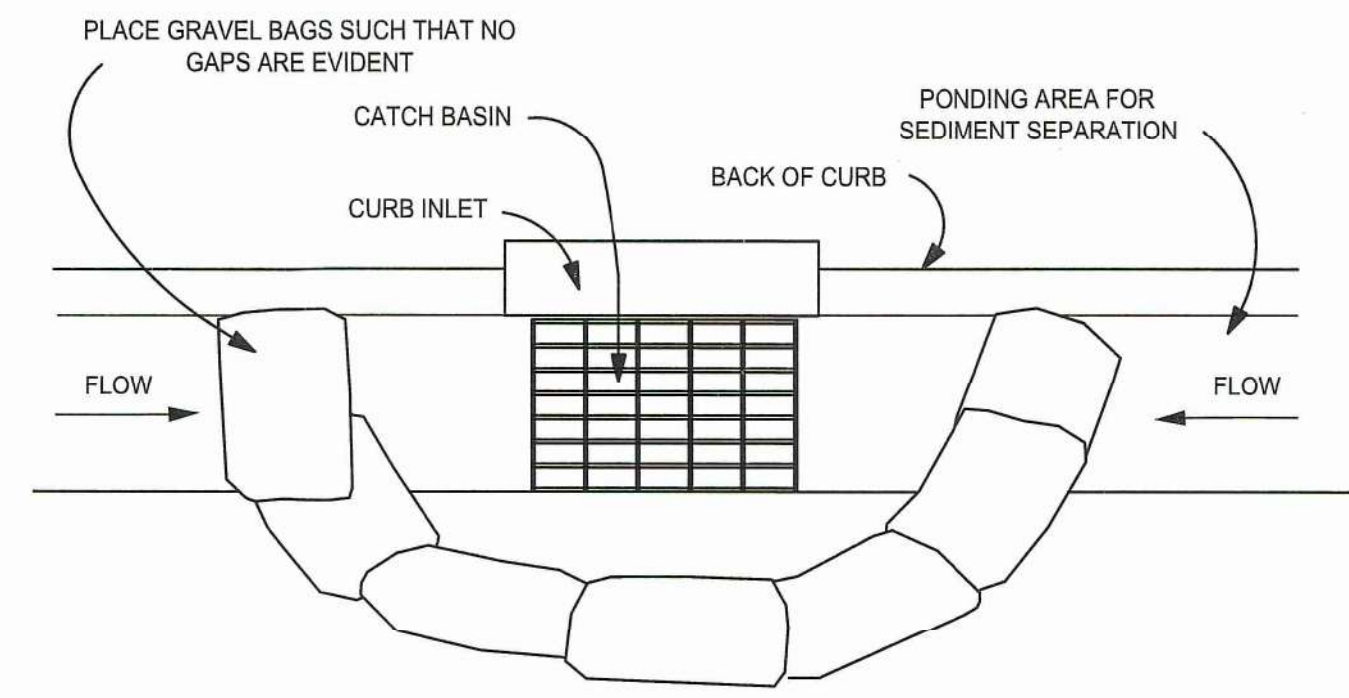
CONSTRUCTION SPECIFICATIONS

- LAY ONE BLOCK ON EACH SIDE OF THE STRUCTURE ON ITS SIDE IN THE BOTTOM ROW TO ALLOW POOL DRAINAGE. THE FOUNDATION SHOULD BE EXCAVATED AT LEAST 2 INCHES BELOW THE CREST OF THE STORM DRAIN. PLACE THE BOTTOM ROW OF BLOCKS AGAINST THE EDGE OF THE STORM DRAIN FOR LATERAL SUPPORT AND TO AVOID WASHOUTS WHEN OVERFLOW OCCURS. IF NEEDED, GIVE LATERAL SUPPORT TO SUBSEQUENT ROWS BY PLACING 2x4 WOOD STUDS THROUGH BLOCK OPENINGS.
- CAREFULLY FIT HARDWARE CLOTH OR COMPARABLE WIRE MESH WITH 1/2 INCH OPENINGS OVER ALL BLOCK OPENINGS TO HOLD GRAVEL IN PLACE.
- USE CLEAN GRAVEL, 3/4 TO 1/2 INCH IN DIAMETER, PLACED 2 INCHES BELOW THE TOP OF THE BLOCK ON A 2:1 SLOPE OR FLATTER AND SMOOTH IT TO AN EVEN GRADE. DOT #57 WASHED STONE IS RECOMMENDED.

MAINTENANCE: INSPECT INLET PROTECTION AND REMOVE SEDIMENT AFTER EACH RAIN EVENT. GRAVEL SHOULD BE REPLACED AND REPAIRS MADE AS NEEDED.



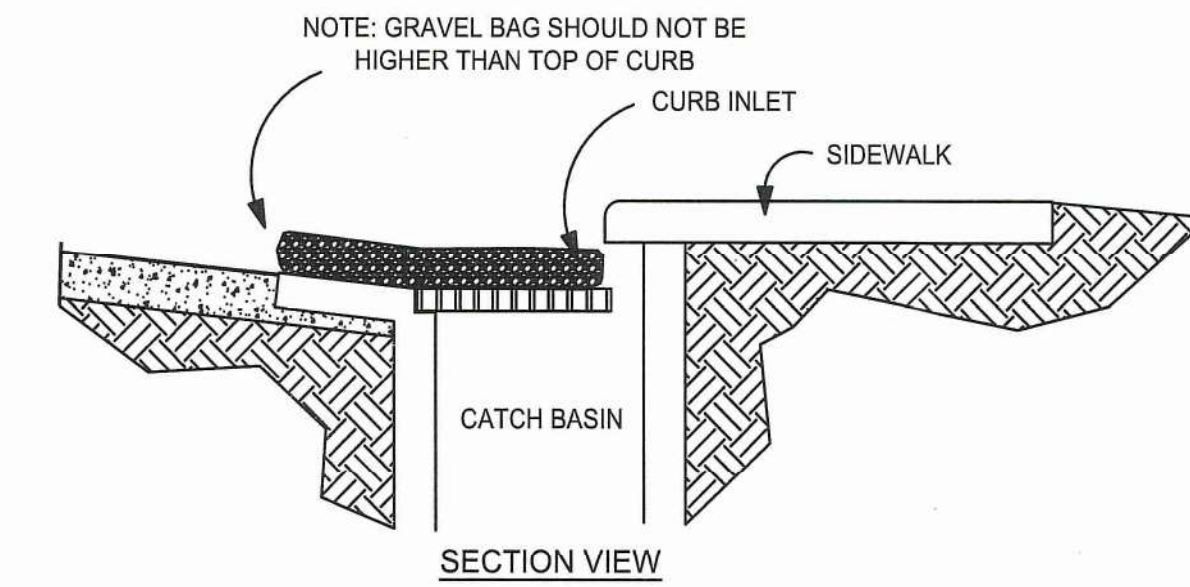
STANDARD BLOCK & GRAVEL DROP INLET PROTECTION **IP-BG**



PLAN VIEW

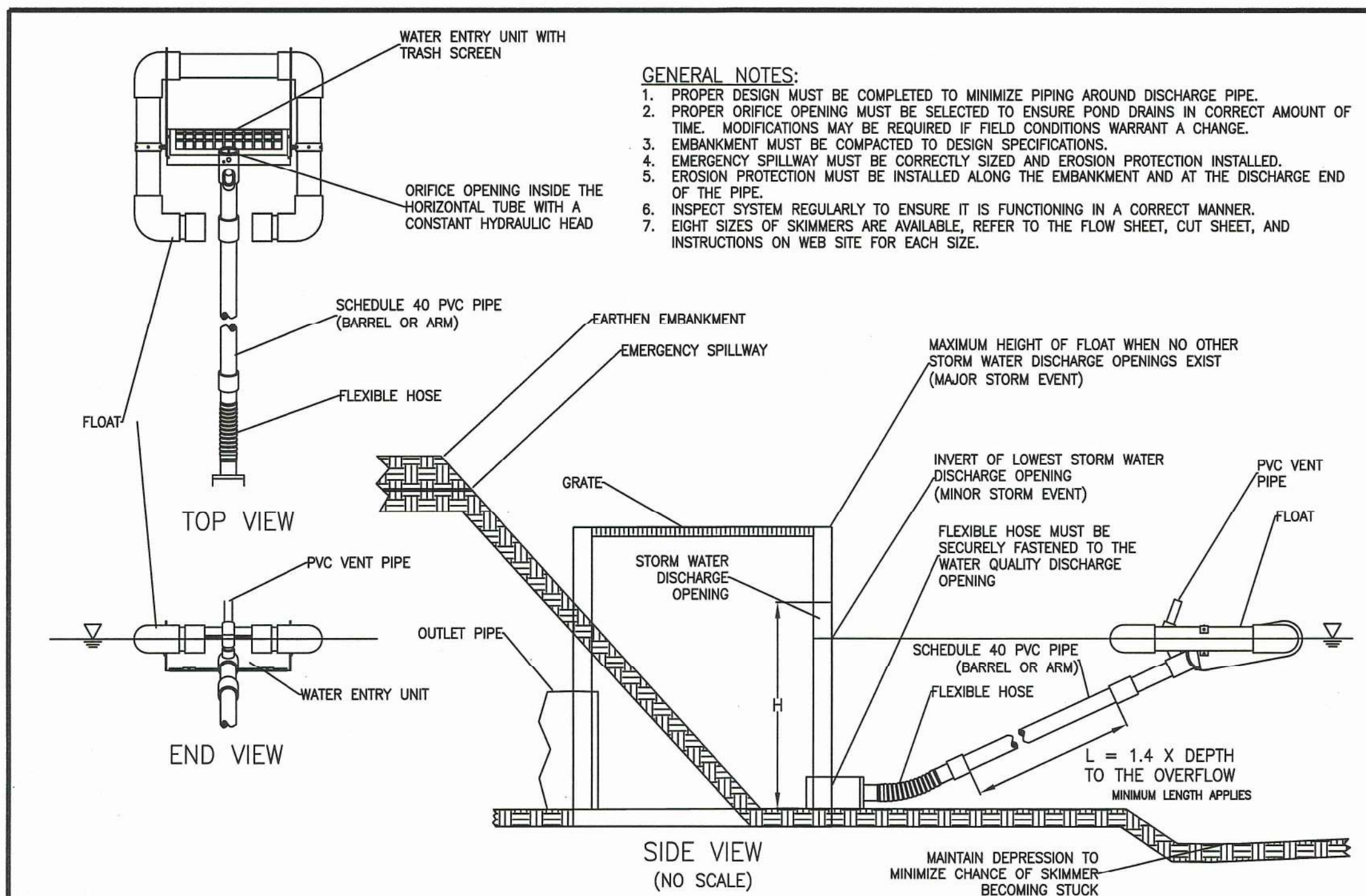
NOTES:

- PLACE GRAVEL BAG BARRIER ON GENTLY SLOPING STREET, WHERE WATER CAN POND AND ALLOW SEDIMENT TO SEPARATE FROM RUNOFF.
- USE SAND BAGS OF WOVEN GEOTEXTILE FABRIC (NOT BURLAP) AND FILL WITH 1/2 INCH (OR SMALLER) GRAVEL BAGS MUST BE LAYERED SUCH THAT NO GAPS ARE EVIDENT.
- INSPECT BARRIERS AND REMOVE SEDIMENT AFTER EACH STORM EVENT, SEDIMENT AND GRAVEL MUST BE REMOVED FROM THE TRAVELED WAY IMMEDIATELY.
- WHEN INSTALLING CURB INLET PROTECTION DEVICES, NEVER BLOCK THE CURB INLET.



SECTION VIEW

STANDARD GRAVEL BAG CURB INLET PROTECTION **IP**



GENERAL NOTES:

- PROPER DESIGN MUST BE COMPLETED TO MINIMIZE PIPING AROUND DISCHARGE PIPE.
- PROPER ORIFICE OPENING MUST BE SELECTED TO ENSURE POND DRAINS IN CORRECT AMOUNT OF TIME. MODIFICATIONS MAY BE REQUIRED IF FIELD CONDITIONS WARRANT A CHANGE.
- EMBANKMENT MUST BE COMPACTED TO DESIGN SPECIFICATIONS.
- EMERGENCY SPILLWAY MUST BE CORRECTLY SIZED AND EROSION PROTECTION INSTALLED.
- EROSION PROTECTION MUST BE INSTALLED ALONG THE EMBANKMENT AND AT THE DISCHARGE END OF THE PIPE.
- INSPECT SYSTEM REGULARLY TO ENSURE IT IS FUNCTIONING IN A CORRECT MANNER.
- EIGHT SIZES OF SKIMMERS ARE AVAILABLE, REFER TO THE FLOW SHEET, CUT SHEET, AND INSTRUCTIONS ON WEB SITE FOR EACH SIZE.

FAIRCLOTH SKIMMER DISCHARGE SYSTEM WITH OUTLET STRUCTURE

J. W. FAIRCLOTH & SON INC.
WWW.FAIRCLOTHSKIMMER.COM
TELEPHONE: (910) 732-1244
FAX: (910) 732-1266
EMAIL: WARREN@FAIRCLOTHSKIMMER.COM

SOIL PREPARATION

PREPARATION FOR PRIMARY/PERMANENT STABILIZATION SHALL NOT BEGIN UNTIL ALL CONSTRUCTION AND UTILITY WORK WITHIN THE PREPARATION AREA IS COMPLETE. HOWEVER, IT MAY BE NECESSARY TO PREPARE FOR NURSE CROPS PRIOR TO COMPLETION OF CONSTRUCTION AND INSTALLATION OF UTILITIES.

A SOILS TEST SHALL BE OBTAINED FOR ALL AREAS TO BE SEEDED, SPRIGGED, SODDED OR PLANTED. RECOMMENDED FERTILIZER AND PH ADJUSTING PRODUCTS SHALL BE INCORPORATED INTO THE PREPARED AREAS AND BACKFILL MATERIAL PER THE TEST. ALL AREAS TO BE SEEDED OR PLANTED SHALL BE TILLED OR RIPPED TO A DEPTH SPECIFIED ON THE APPROVED PLANS, CONSTRUCTION SEQUENCE AND/OR CONSTRUCTION BID LIST. RIPPING CONSISTS OF CREATING FISSURES IN A CRISS-CROSS PATTERN OVER THE ENTIRE SURFACE AREA, UTILIZING AN IMPLEMENT THAT WILL NOT GLAZE THE SIDE WALLS OF THE FISSURES. SITE PREPARATION THAT DOES NOT COMPLY WITH THESE DOCUMENTS SHALL NOT BE ACCEPTABLE. THE DEPTH OF SOIL PREPARATION MAY BE ESTABLISHED AS A RANGE BASED ON THE APPROVAL OF THE REVIEWING STATE OR LOCAL AGENCY. ONCE TILLED OR RIPPED ACCORDING TO THE APPROVED PLAN, ALL AREAS ARE TO BE RETURNED TO THE APPROVED FINAL GRADE. PH MODIFIERS AND/OR OTHER SOIL AMENDMENTS SPECIFIED IN THE SOIL TESTS CAN BE ADDED DURING THE SOIL PREPARATION PROCEDURE OR AS DESCRIBED BELOW.

ALL STONES LARGER THAN (3) INCHES ON ANY SIDE, STICKS, ROOTS, AND OTHER EXTRANEOUS MATERIALS THAT SURFACE DURING THE BED PREPARATION SHALL BE REMOVED.

AREAS TO BE SEEDED:

TILL OR DISC THE PREPARED AREAS TO BE SEEDED TO A MINIMUM DEPTH OF FOUR (4) INCHES. REMOVE STONES LARGER THAN THREE (3) INCHES ON ANY SIDE, STICKS, ROOTS AND OTHER EXTRANEOUS MATERIAL THAT SURFACE. IF NOT INCORPORATED DURING THE SOIL PREPARATION PROCESS, ADD PH MODIFIER AND FERTILIZERS AT THE RATE SPECIFIED IN THE SOIL TEST REPORT.

RE-COMPACT THE AREA UTILIZING A CULTIPACKER ROLLER. THE FINISHED GRADE SHALL BE A SMOOTH EVEN SOIL SURFACE WITH A LOOSE, UNIFORMLY FINE TEXTURE. ALL RIDGES AND DEPRESSIONS SHALL BE REMOVED AND FILLED TO PROVIDE THE APPROVED SURFACE DRAINAGE. SEEDING OF GRADED AREAS IS TO BE DONE IMMEDIATELY AFTER FINISHED GRADES ARE OBTAINED AND SEEDBED PREPARATION IS COMPLETED.

AREAS TO BE SPRIGGED, SODDED, AND/OR PLANTED:

AT THE TIME OF PLANTING TILL OR DISC THE PREPARED AREAS TO A DEPTH OF FOUR (4) TO SIX (6) INCHES BELOW THE APPROVED FINISHED GRADE. REMOVE ALL STONES LARGER THAN THREE (3) INCHES ON ANY SIDE, STICKS, ROOTS AND OTHER EXTRANEOUS MATERIALS THAT SURFACE. IF NOT INCORPORATED IN THE RIPPING PROCESS, ADD PH MODIFIER, FERTILIZER AND OTHER RECOMMENDED SOIL AMENDMENTS.

RE-COMPACT THE AREA UTILIZING A CULTIPACKER ROLLER AND PREPARE FINAL GRADES AS DESCRIBED ABOVE. INSTALL SPRIGS, SOD AND PLANTS AS DIRECTED IMMEDIATELY AFTER FINE GRADING IS COMPLETE. MULCH, MAT AND/OR TACK AS SPECIFIED.

TEMPORARY SEEDING SPECIFICATIONS

TEMPORARY SEEDING IS THE USE OF RAPID GROWING ANNUAL GRASSES, SMALL GRAINS OR LEGUMES TO PROVIDE INITIAL TEMPORARY COVER FOR EROSION CONTROL ON DISTURBED AREA FOR LESS THAN 12 MONTHS. SEED BED PREPARATIONS AND SOIL AMENDMENTS SHALL BE IN ACCORDANCE WITH THE METHOD DESCRIBED UNDER "SEEDING AND MULCHING" **TS**

SEEDING AND MULCHING SHALL BE DONE IMMEDIATELY FOLLOWING CONSTRUCTION. ALL DISTURBED AREAS SHALL BE DRESSED TO A DEPTH OF 8 INCHES. THE TOP 3 INCHES SHALL BE PULVERIZED TO PROVIDE A UNIFORM SEEDBED. AGRICULTURAL LIM SHALL BE APPLIED AT THE RATE OF 95LB/1000SF IMMEDIATELY BEFORE PLOWING. GRASS SEED SHALL BE APPLIED AT THE RATES OUTLINED IN TABLES 4100 AND 4200. 5-10-10 FERTILIZER SHALL BE APPLIED TO ALL DISTURBED AREAS AT A RATE OF 21LB/1000SF AND MULCHING SHALL CONSIST OF SMALL GRAIN STRAW APPLIED AT A RATE OF 70LB/1000SF. MULCHED AREAS SHALL BE TACKED WITH ASPHALT AT A RATE OF 200 TO 400 GALLONS PER ACRE, OR OTHER APPROVED METHOD SUFFICIENT TO HOLD THE STRAW IN PLACE.

IF ACTIVE CONSTRUCTION CEASES, MEANING NO SUBSTANTIAL OF SIGNIFICANT PROGRESS IS MADE IN ANY AREA FOR MORE THAN 15 DAYS, ALL DISTURBED AREAS MUST BE SEEDED, MULCHED, AND TACKED UNLESS WRITTEN APPROVAL IS GRANTED. INCIDENTAL GRADING SHALL NOT CONSTITUTE SUBSTANTIAL OR SIGNIFICANT PROGRESS IN CONSTRUCTION ACTIVITY.

PERMANENT GROUND COVER IS THE ESTABLISHMENT OF PERENNIAL VEGETATION COVER FOR PERIODS LONGER THAN 12 MONTHS. ALL DISTURBED AREAS SHALL RECEIVE A PERMANENT GROUND COVER. PERMANENT SEEDING AND TEMPORARY SEEDING DIFFER ONLY IN THE TYPE OF SEED TO BE USED (I.E. ANNUAL VERSUS PERENNIAL). SEED BED PREPARATIONS AND SOIL AMENDMENTS SHALL BE IN ACCORDANCE WITH "SEEDING AND MULCHING", AS A PART OF PERMANENT SEEDING. MAINTENANCE MAY BE REQUIRED TO MAINTAIN VEGETATION FOR 12 MONTHS (INCLUDING MOWING AND WATERING). THIS MAINTENANCE SHALL BE CONSIDERED A PART OF ESTABLISHING PERMANENT GROUND COVER. **PS**
SO



3/1/23

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SWPPP DETAILS



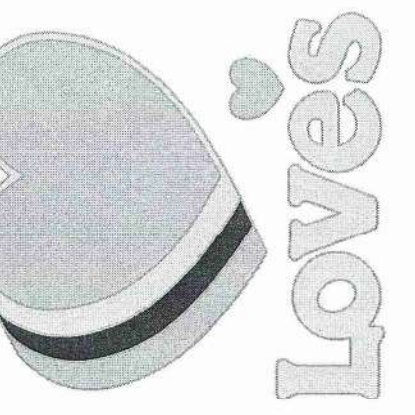
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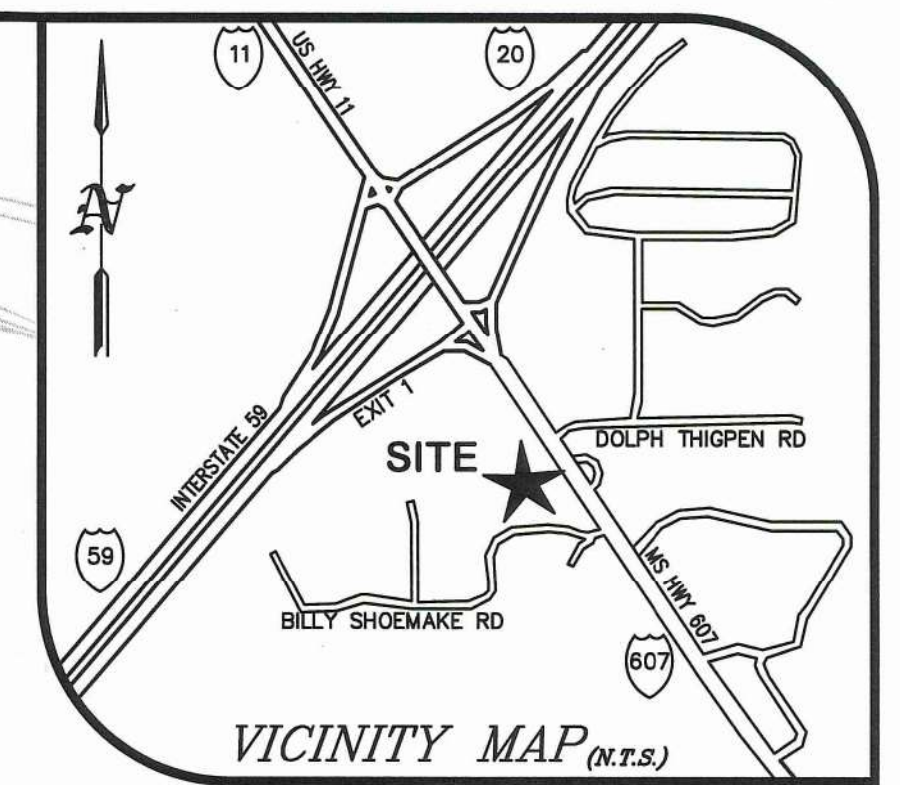
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LOVE'S TRAVEL STOP
INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
PEARL RIVER COUNTY, MISSISSIPPI



C5.0



LEGEND FOR PROPOSED IMPROVEMENTS

- PROPERTY LINE / R.O.W.
- PROPOSED BUILDING
- PROPOSED CURB AND GUTTER
- PROPOSED PARKING SPACES
- PROPOSED FENCING
- SYSL/4" - SINGLE YELLOW SOLID LINE / 4 INCH THICK
- DYSL/5" - DOUBLE YELLOW SOLID LINE / 5 INCH THICK EACH
- SWSL/24" - SINGLE WHITE SOLID LINE / 24 INCH THICK
- SWSL/8" - SINGLE WHITE SOLID LINE / 8 INCH THICK
- SWSL/4" - SINGLE WHITE SOLID LINE / 4 INCH THICK
- SWDL/4" - SINGLE WHITE DASHED LINE / 4 INCH THICK

EXISTING LEGEND

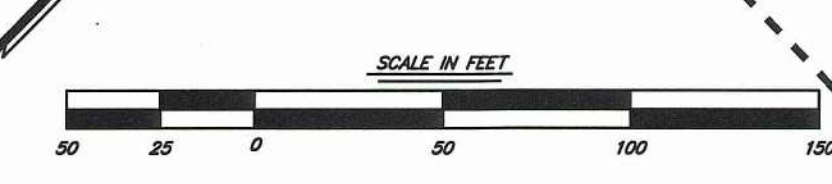
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- ADJACENT BOUNDARY LINES
- OLD WIRE FENCE
- "40" LINES
- LIMITS OF EASEMENT
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- MINOR CONTOUR
- DRAINAGE PIPE
- SIGNIFICANT DRAINAGE FLOWLINE
- APPROXIMATE FLOOD ZONE LOCATION
- NO HIGHWAY ACCESS
- PAINT STRIPE
- 1/2" IRON REBAR SET
- IRON PIN FOUND
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- UTILITY POLE
- AS PER SURVEY
- AS PER RECORD
- RIGHT-OF-WAY
- CONTOUR ELEVATION
- REINFORCED CONCRETE PIPE
- CORRUGATED METAL PIPE
- ROAD SIGN POST

SITE ANALYSIS

LOVE'S TRAVEL STOP	12,106± GROSS SF
LOVE'S TIRE SHOP	11,982± GROSS SF
TOTAL SITE AREA	22.40± ACRES
PARKING SUMMARY	AUTO ACC RV BT TRUCK TOTAL
SPACES PROVIDED	58 5 6 11 81 = 161

TIER 1

Frank Stuart



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SITE PLAN

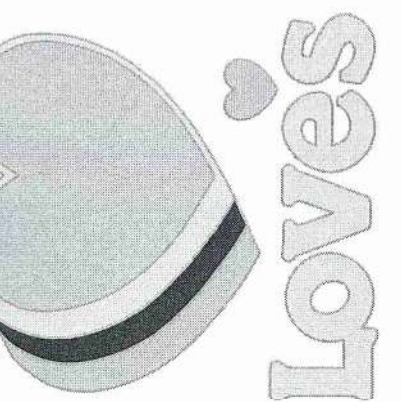
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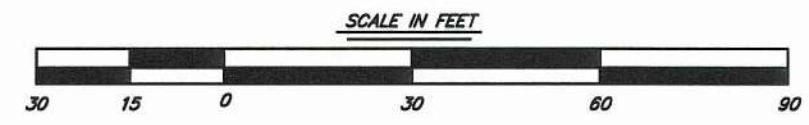
LOVE'S TRAVEL STOP
INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
PEARL RIVER COUNTY, MISSISSIPPI



C5.1

LEGEND FOR PROPOSED IMPROVEMENTS

- PROPERTY LINE / R.O.W.
- ▬ PROPOSED BUILDING
- ▬ PROPOSED CURB AND GUTTER
- ⑫ PROPOSED PARKING SPACES
- PROPOSED FENCING
- SYSL/4" - SINGLE YELLOW SOLID LINE / 4 INCH THICK
- DYSL/5" - DOUBLE YELLOW SOLID LINE / 5 INCH THICK EACH
- SWSL/24" - SINGLE WHITE SOLID LINE / 24 INCH THICK
- SWSL/8" - SINGLE WHITE SOLID LINE / 8 INCH THICK
- SWSL/4" - SINGLE WHITE SOLID LINE / 4 INCH THICK
- SWDL/4" - SINGLE WHITE DASHED LINE / 4 INCH THICK



MS Highway 607

HWY 607

Concrete Lot

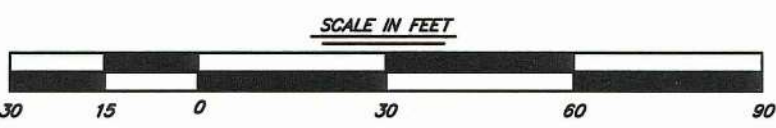
Concrete Lot

STRIPING & SIGNAGE

GRADING & DRAINAGE

LEGEND FOR PROPOSED IMPROVEMENTS

- PROPERTY LINE / R.O.W.
- ▬ PROPOSED CURB AND GUTTER
- ▬ PROPOSED INLET
- ▬ PROPOSED CURB INLET
- ▬ PROPOSED STORM SEWER
- x12.10 PROPOSED ELEVATION



Know what's below.
Call before you dig.



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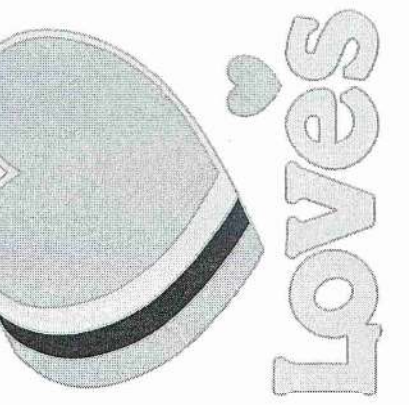
DRIVEWAY PLAN



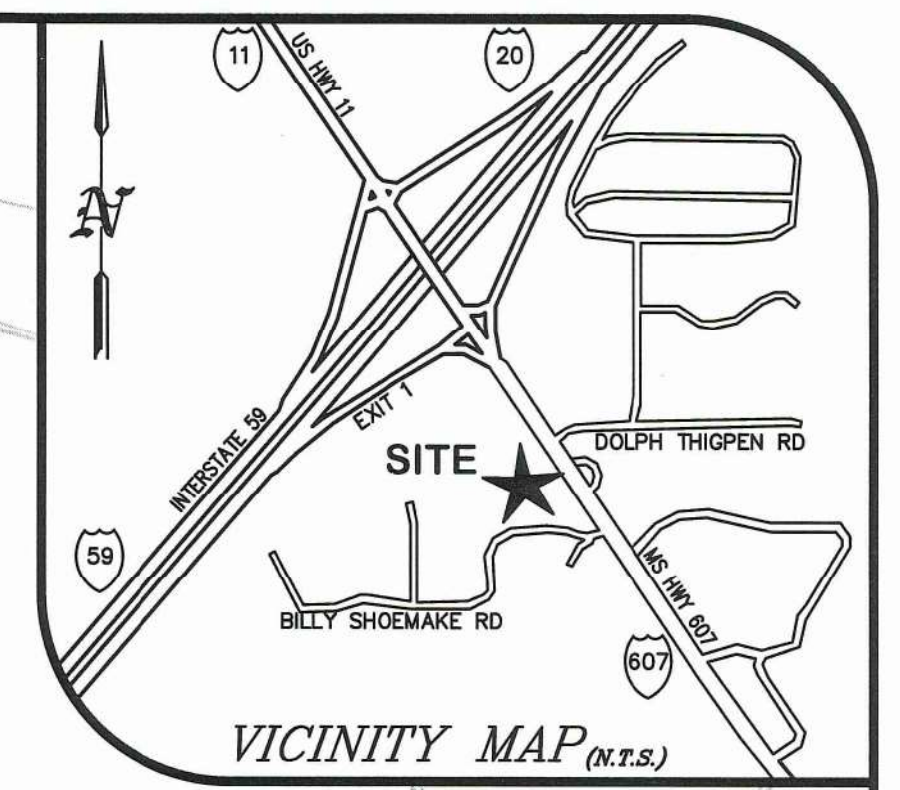
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LOVE'S TRAVEL STOP
INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
PEARL RIVER COUNTY, MISSISSIPPI



C6.0



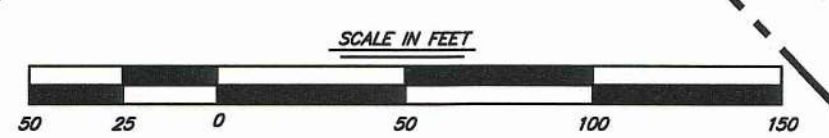
LEGEND FOR PROPOSED IMPROVEMENTS

- PROPERTY LINE / R.O.W.
- PROPOSED BUILDING
- PROPOSED CURB AND GUTTER
- PROPOSED SAW CUT JOINT
- PROPOSED SIDEWALK SAWED CONTRACTION JOINT
- PROPOSED EXTRA HEAVY DUTY ASPHALT PAVEMENT
- PROPOSED HEAVY DUTY ASPHALT PAVEMENT
- PROPOSED MEDIUM DUTY ASPHALT PAVEMENT
- PROPOSED LIGHT DUTY ASPHALT PAVEMENT
- PROPOSED EXTRA HEAVY DUTY CONCRETE PAVEMENT
- PROPOSED HEAVY DUTY CONCRETE PAVEMENT
- PROPOSED LIGHT DUTY CONCRETE PAVEMENT
- PROPOSED ROADWAY WIDENING ASPHALT PAVEMENT
- PROPOSED GRAVEL AREAS

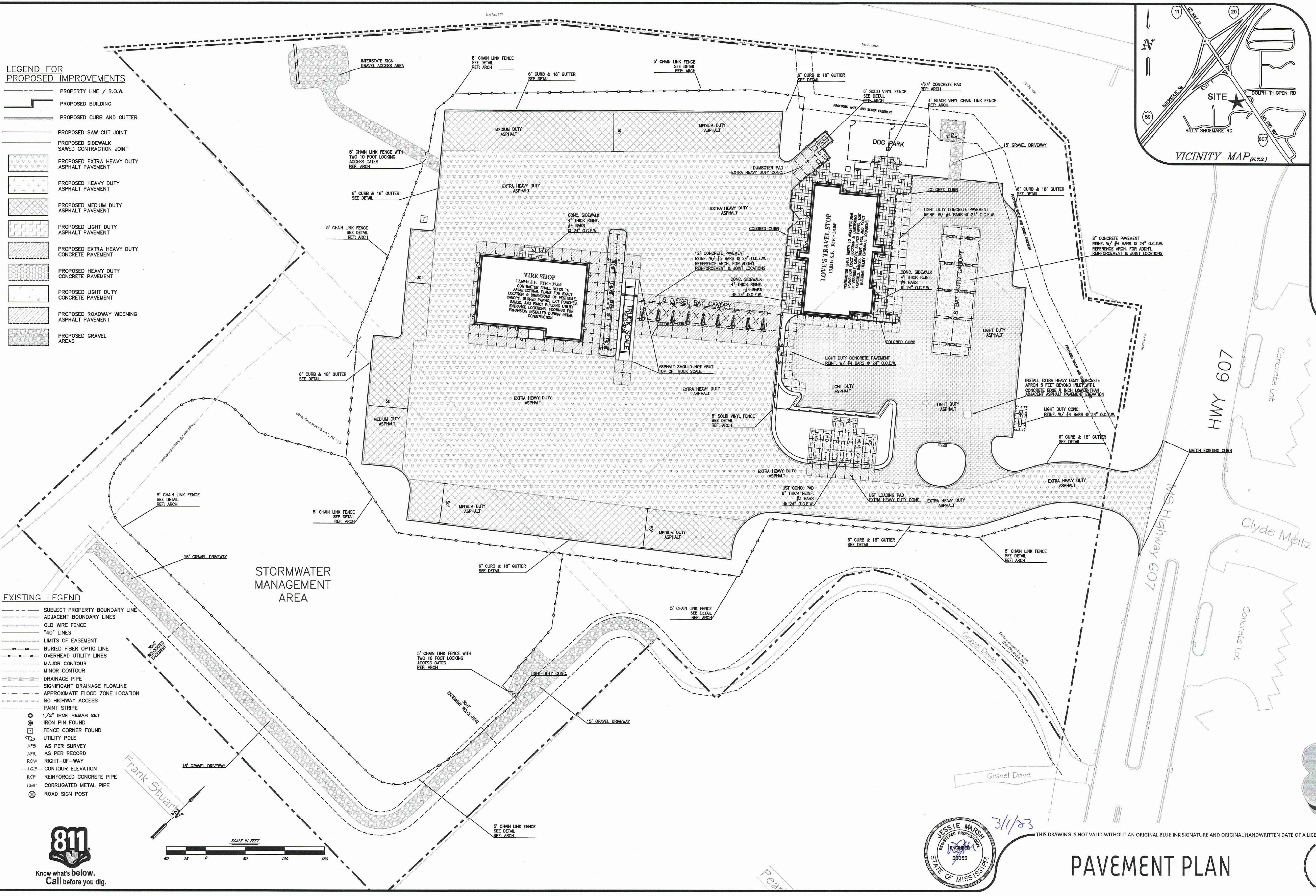
EXISTING LEGEND

- SUBJECT PROPERTY BOUNDARY LINE
- ADJACENT BOUNDARY LINES
- OLD WIRE FENCE
- *40" LINES
- LIMITS OF EASEMENT
- BURIED FIBER OPTIC LINE
- OVERHEAD UTILITY LINES
- MAJOR CONTOUR
- MINOR CONTOUR
- DRAINAGE PIPE
- SIGNIFICANT DRAINAGE FLOWLINE
- APPROXIMATE FLOOD ZONE LOCATION
- NO HIGHWAY ACCESS
- PAINT STRIPE
- 1/2" IRON REBAR SET
- IRON PIN FOUND
- FENCE CORNER FOUND
- UTILITY POLE
- AS PER SURVEY
- AS PER RECORD
- RIGHT-OF-WAY
- CONTOUR ELEVATION
- REINFORCED CONCRETE PIPE
- CORRUGATED METAL PIPE
- ROAD SIGN POST

STORMWATER MANAGEMENT AREA



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PAVEMENT PLAN





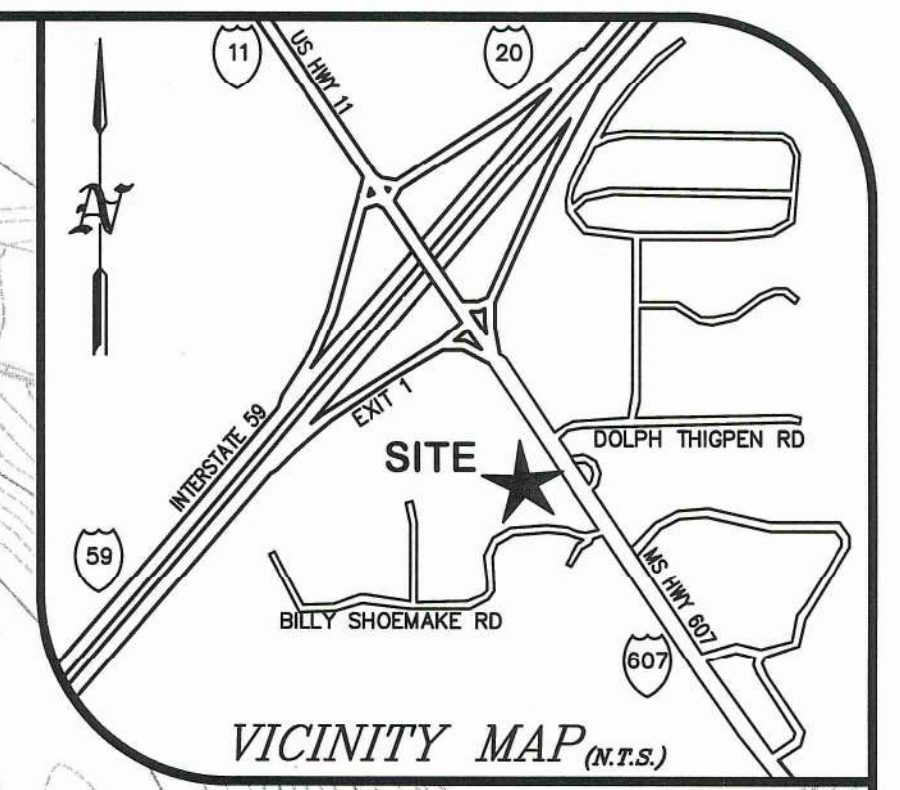
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LOVE'S TRAVEL STOP
INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
PEARL RIVER COUNTY, MISSISSIPPI



C7.0



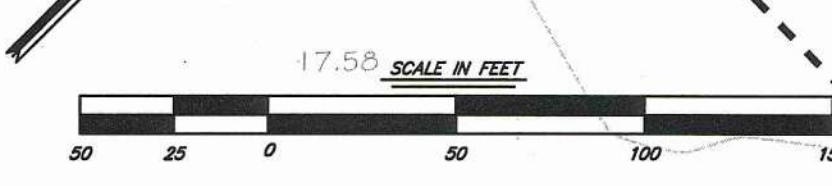
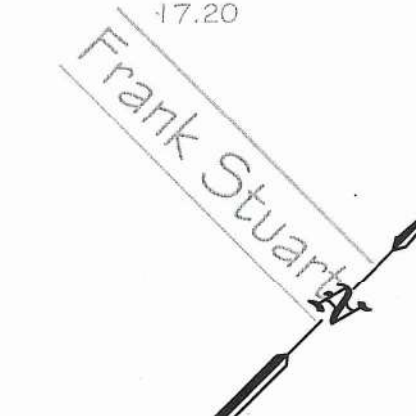
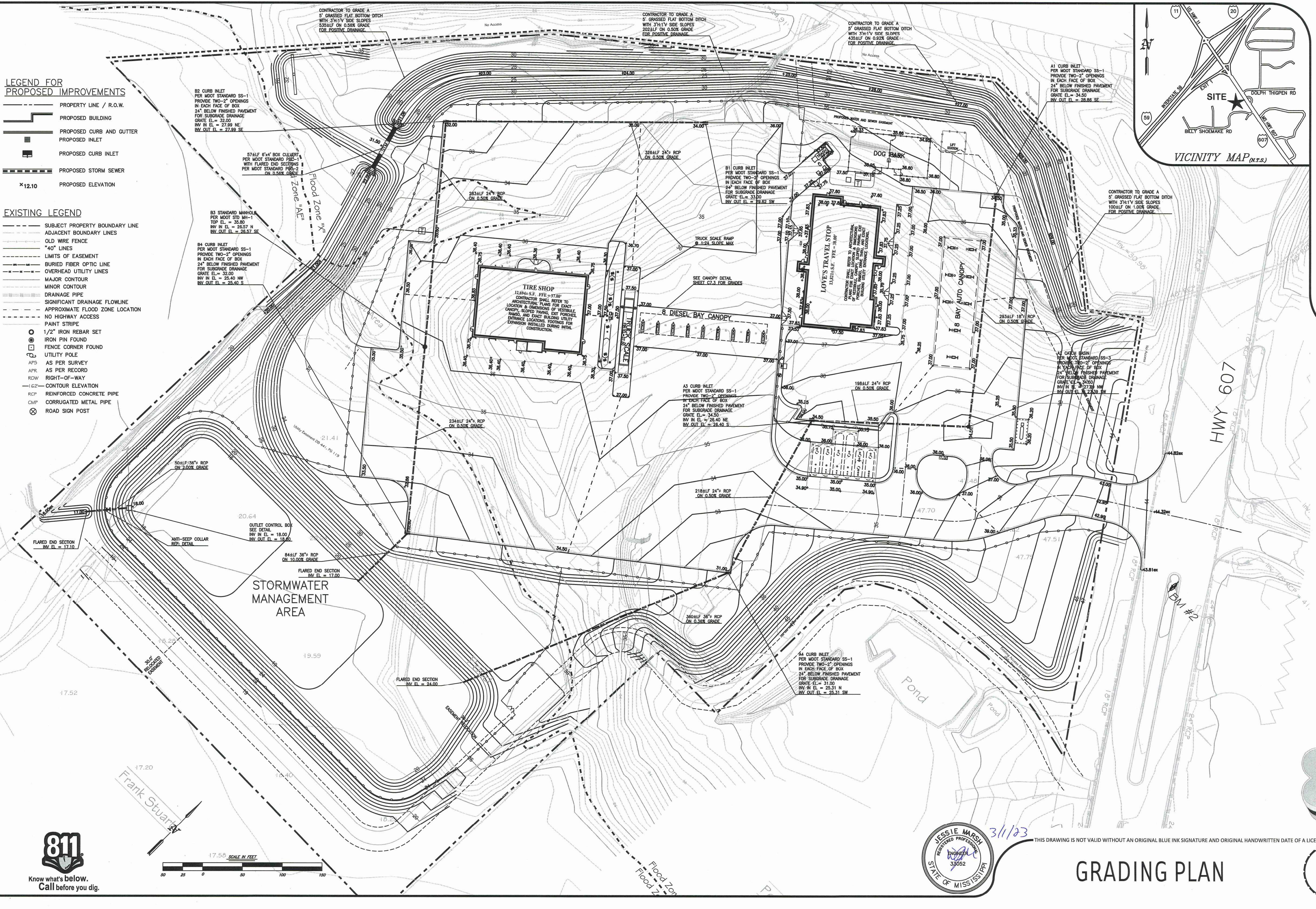
LEGEND FOR PROPOSED IMPROVEMENTS

- PROPERTY LINE / R.O.W.
- PROPOSED BUILDING
- PROPOSED CURB AND GUTTER
- PROPOSED INLET
- PROPOSED CURB INLET
- PROPOSED STORM SEWER
- *12.10 PROPOSED ELEVATION

EXISTING LEGEND

- SUBJECT PROPERTY BOUNDARY LINE
- ADJACENT BOUNDARY LINES
- OLD WIRE FENCE
- "40' LINES
- LIMITS OF EASEMENT
- BURIED FIBER OPTIC LINE
- OVERHEAD UTILITY LINES
- MAJOR CONTOUR
- MINOR CONTOUR
- DRAINAGE PIPE
- SIGNIFICANT DRAINAGE FLOWLINE
- APPROXIMATE FLOOD ZONE LOCATION
- NO HIGHWAY ACCESS
- PAINT STRIPE
-
- IRON PIN FOUND
- FENCE CORNER FOUND
- UTILITY POLE
- APS AS PER SURVEY
- APR AS PER RECORD
- ROW RIGHT-OF-WAY
- 1G2 CONTOUR ELEVATION
- RCP REINFORCED CONCRETE PIPE
- CMP CORRUGATED METAL PIPE
- ROAD SIGN POST

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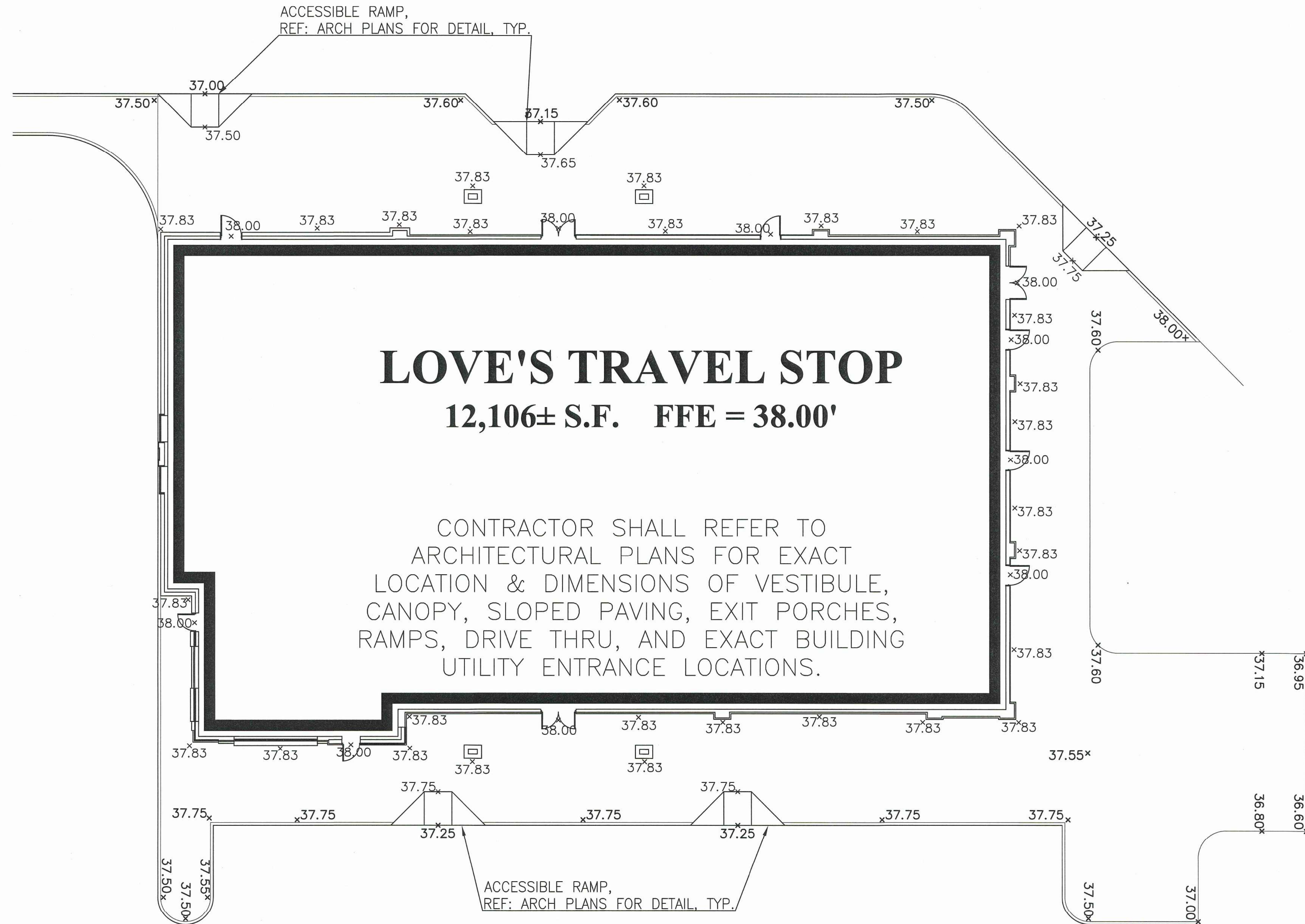
GRADING PLAN



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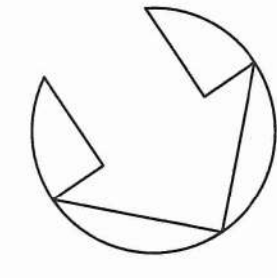


LOVE'S TRAVEL STOP
INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
PEARL RIVER COUNTY, MISSISSIPPI



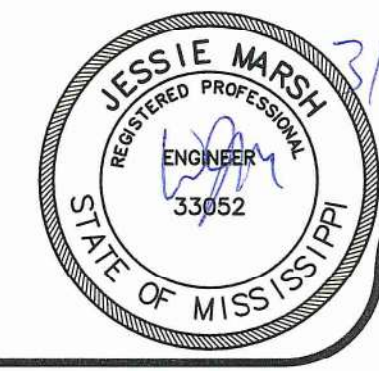
LOVE'S TRAVEL STOP
12,106± S.F. FFE = 38.00'

CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATION & DIMENSIONS OF VESTIBULE, CANOPY, SLOPED PAVING, EXIT PORCHES, RAMPS, DRIVE THRU, AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.



SPOT ELEVATIONS FOR LOVE'S TRAVEL STOP

- SCALE 1"=10'
- NOTES:
1. THE EXTERNAL GRADING DESIGN FOR FINISHED PAVEMENT GRADES IS SHOWN HEREIN, AND IS BASED ON BUILDING FINISHED GRADE TEMPLATE FROM ARCHITECT.
 2. REFERENCE PAVEMENT DESIGNS SHOWN ON SHEET C-2.
 3. REFERENCE GRADING PLAN FOR CORRESPONDING FINISHED PERIMETER GRADES.
 4. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATION & DIMENSIONS OF VESTIBULES, CANOPIES, INTERNAL SLOPED PAVING, EXIT PORCHES, RAMPS, AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.



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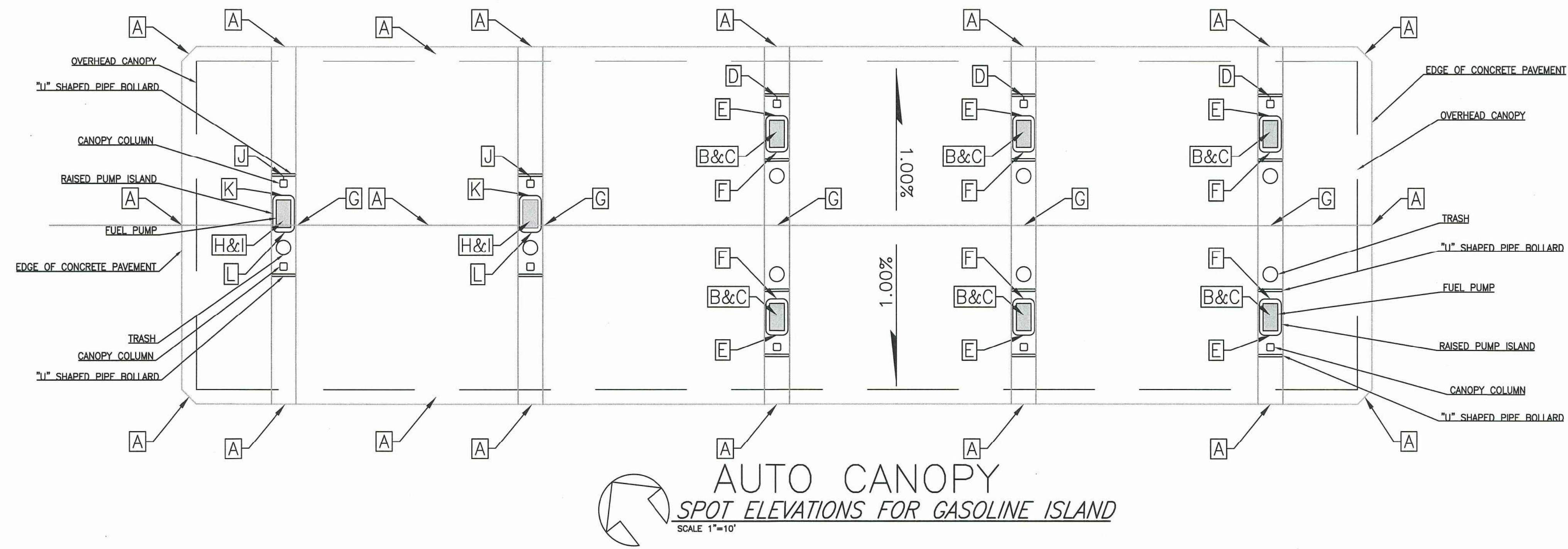
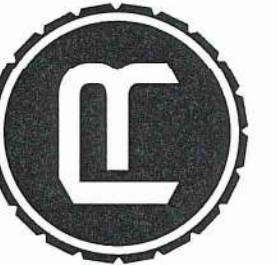
LOVE'S GRADING DETAILS C7.1



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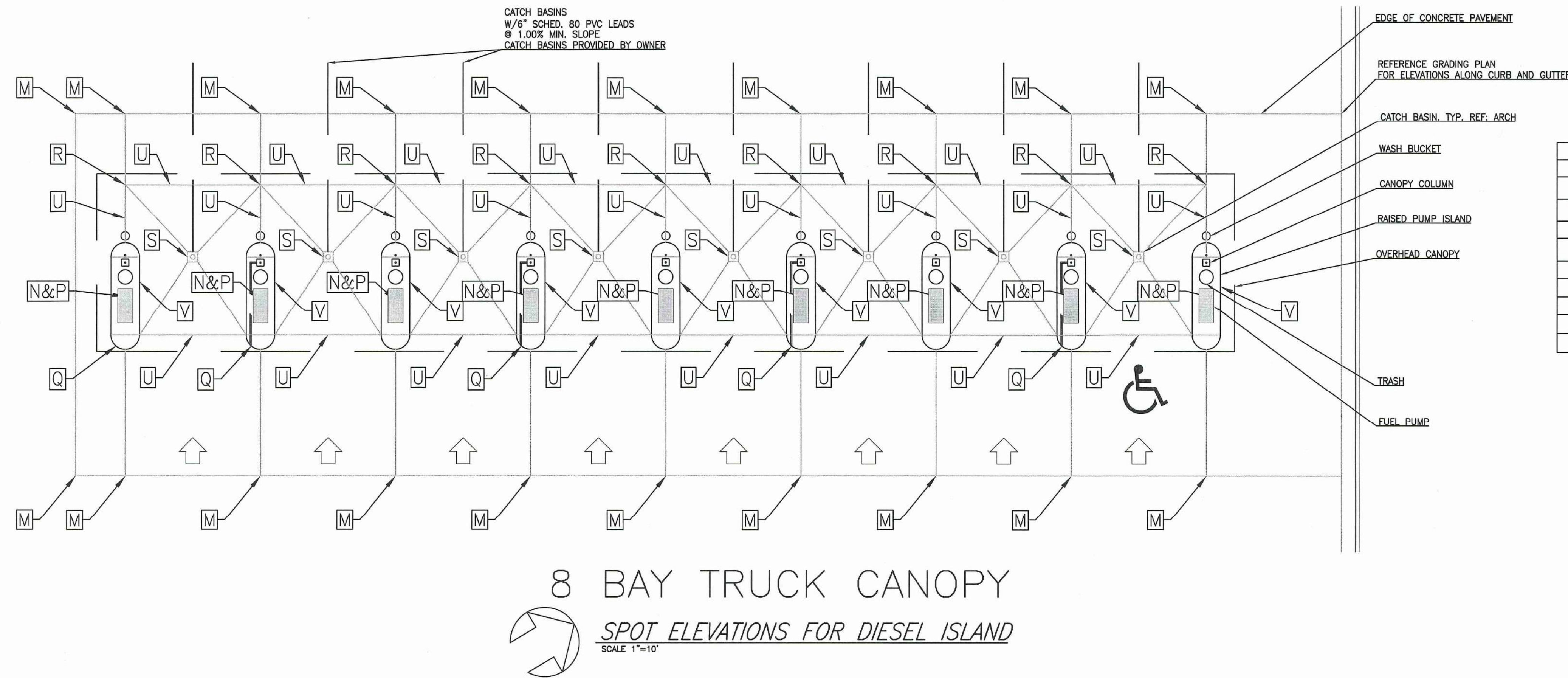


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LEGEND	DESCRIPTION	ELEVATION
A	PERIMETER ELEVATION	37.00
B	TOP OF PUMP ISLAND	37.65
C	TOP OF FOOTING	34.15
D	FLOW LINE OUT OF CANOPY DOWNSPOUT	37.15
E	FINISHED PAVEMENT	37.10
F	FINISHED PAVEMENT	37.15
G	CREST ELEVATION	37.25
H	TOP OF RV PUMP ISLAND	37.25
I	TOP OF RV PUMP FOOTING	33.75
J	FLOW LINE OUT OF RV DOWNSPOUT	37.15
K	FINISHED PAVEMENT	37.20
L	FINISHED PAVEMENT	37.24

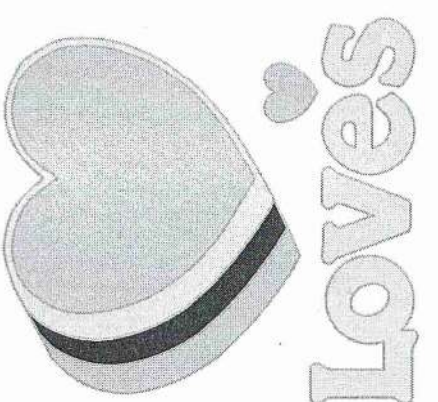
AUTO CANOPY
SPOT ELEVATIONS FOR GASOLINE ISLAND
SCALE 1"=10'



LEGEND	DESCRIPTION	ELEVATION
M	PERIMETER ELEVATION	37.00
N	TOP OF PUMP ISLAND	37.65
P	TOP OF FOOTING	34.15
Q	FLOW LINE OUT OF DOWNSPOUT	37.15
R	FINISHED PAVEMENT	37.15
S	TOP OF GRATE INLET	37.00
T	HANDICAP TOP OF GRATE INLET	37.00
U	CREST ELEVATION	37.15
V	FINISHED PAVEMENT AT PUMP ISLAND	37.15

8 BAY TRUCK CANOPY
SPOT ELEVATIONS FOR DIESEL ISLAND
SCALE 1"=10'

LOVE'S TRAVEL STOP
INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
PEARL RIVER COUNTY, MISSISSIPPI



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CANOPY GRADING DETAILS C7.3



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LOVE'S TRAVEL STOP
 INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
 PEARL RIVER COUNTY, MISSISSIPPI



C7.4

200325v6a
 Prepared by Resource Consulting
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Summary for Pond 3P: Proposed SW Management Pond

#1	Invert	Avail. Storage	Storage Description
18.00'	18.00'	8.446 af	Custom Stage Data (Prismatic) Listed below (Recalc)
Elevation (feet)	Surf. Area (acres)	Inc. Store (acre-feet)	Cum. Store (acre-feet)
18.00	0.484	0.000	0.000
19.00	0.995	0.740	0.740
20.00	1.425	1.210	1.950
21.00	1.523	1.474	3.424
22.00	1.622	1.572	4.997
23.00	1.724	1.673	6.670
24.00	1.828	1.776	8.446

Device	Routing	Invert	Outlet Devices	Notes
#1	Primary	18.00'	36.0" Vert. Orifice/Grate	C=0.600 Limited to weir flow at low heads
#2	Device 1	18.00'	6.0" Vert. Orifice/Grate	C=0.600 Limited to weir flow at low heads
#3	Device 1	19.50'	36.0" Vert. Orifice/Grate	C=0.600 Limited to weir flow at low heads
#4	Device 1	23.00'	4.0' long Sharp-Crested Rectangular Weir	2 End Contractions
#5	Device 1	23.00'	4.0' long Sharp-Crested Rectangular Weir	2 End Contractions

Primary Outflow
 1=Orifice/Grate
 2=Orifice/Grate
 3=Orifice/Grate
 4=Sharp-Crested Rectangular Weir
 5=Sharp-Crested Rectangular Weir

Post-Scenario Pond Regulated Stormwater Analysis

Event	Inflow (cfs)	Outflow (cfs)	Elevation (ft)	Storage (ac-ft)
2-Year	25.10	7.34	20.44	2.590
5-Year	37.71	16.21	21.00	3.426
10-Year	45.23	21.51	21.28	3.858
25-year	52.71	26.67	21.54	4.266
50-year	59.54	31.17	21.77	4.623
100-year	66.97	35.80	22.00	5.004

Pre/Post Development Comparison

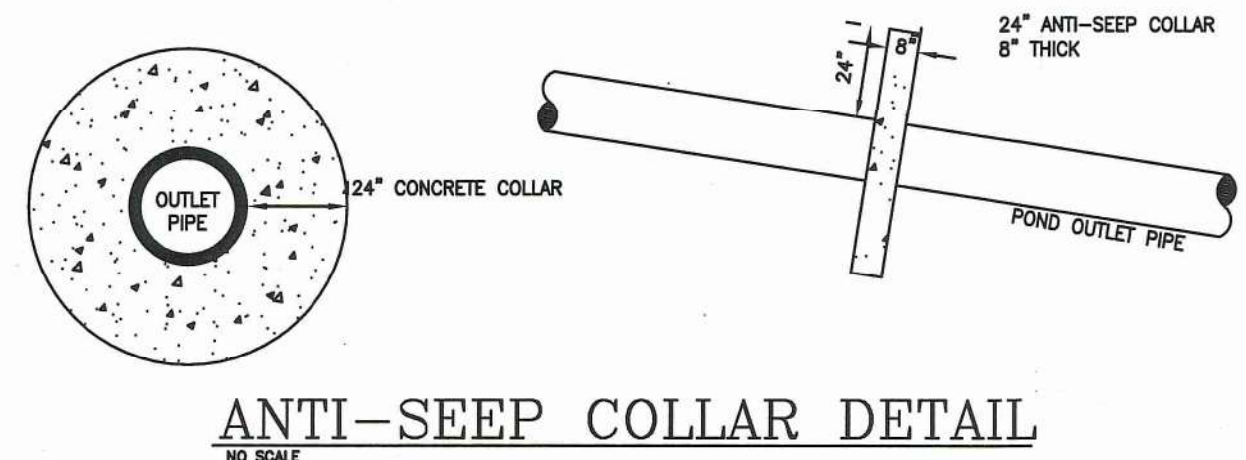
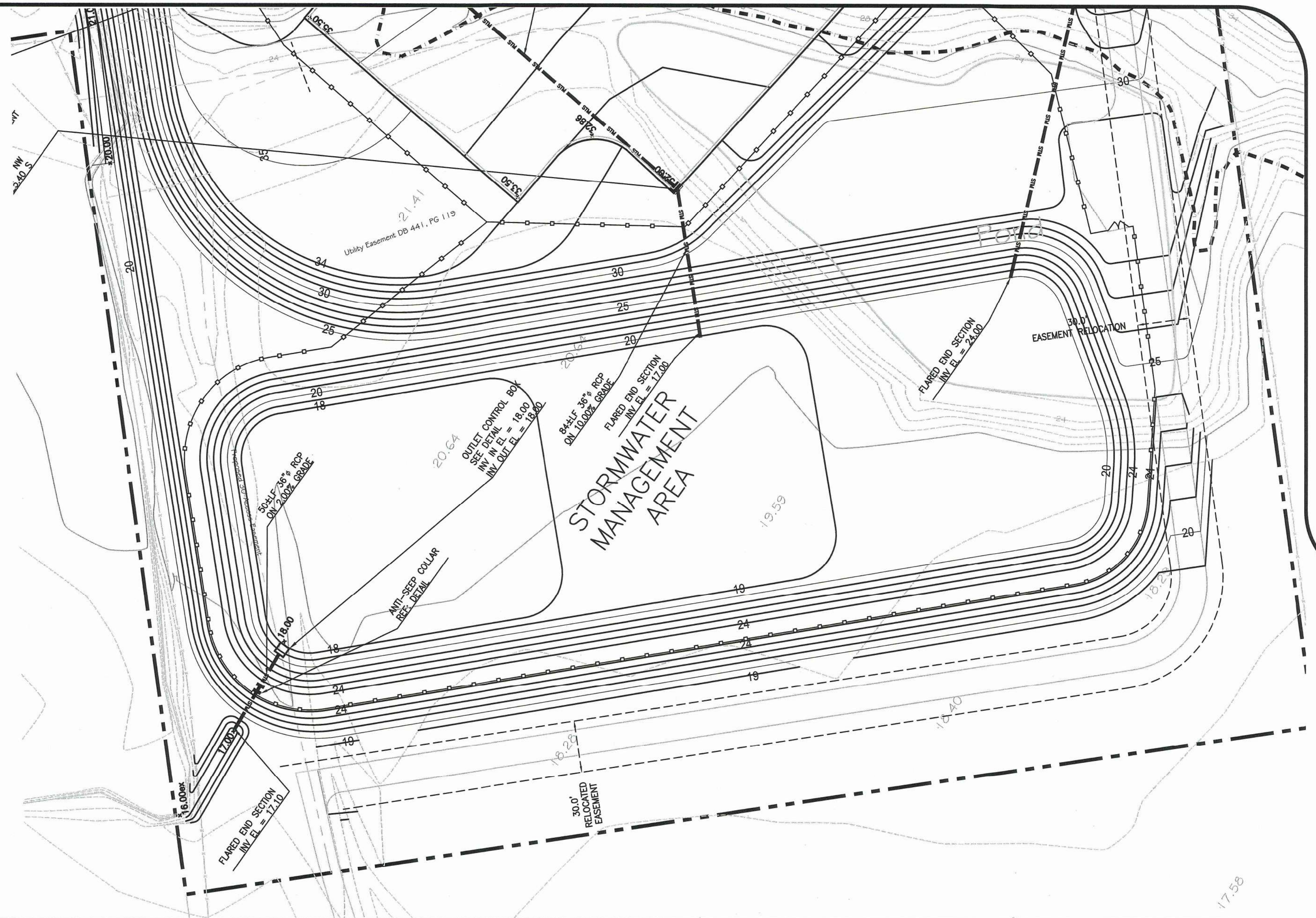
	Pre	Post	
Area (acres)	15.01	15.01	
Tc (min)	22.27	7.30	
C	76.00	88.00	
Storm Event	Q_{pre} (cfs)	Q_{post} (cfs)	Q_{post} < Q_{pre}
2-Year	13.73	7.34	OK
5-Year	23.65	16.21	OK
10-Year	29.84	21.51	OK
25-year	36.10	26.67	OK
50-year	41.88	31.17	OK
100-year	48.21	35.80	OK

LEGEND FOR PROPOSED IMPROVEMENTS

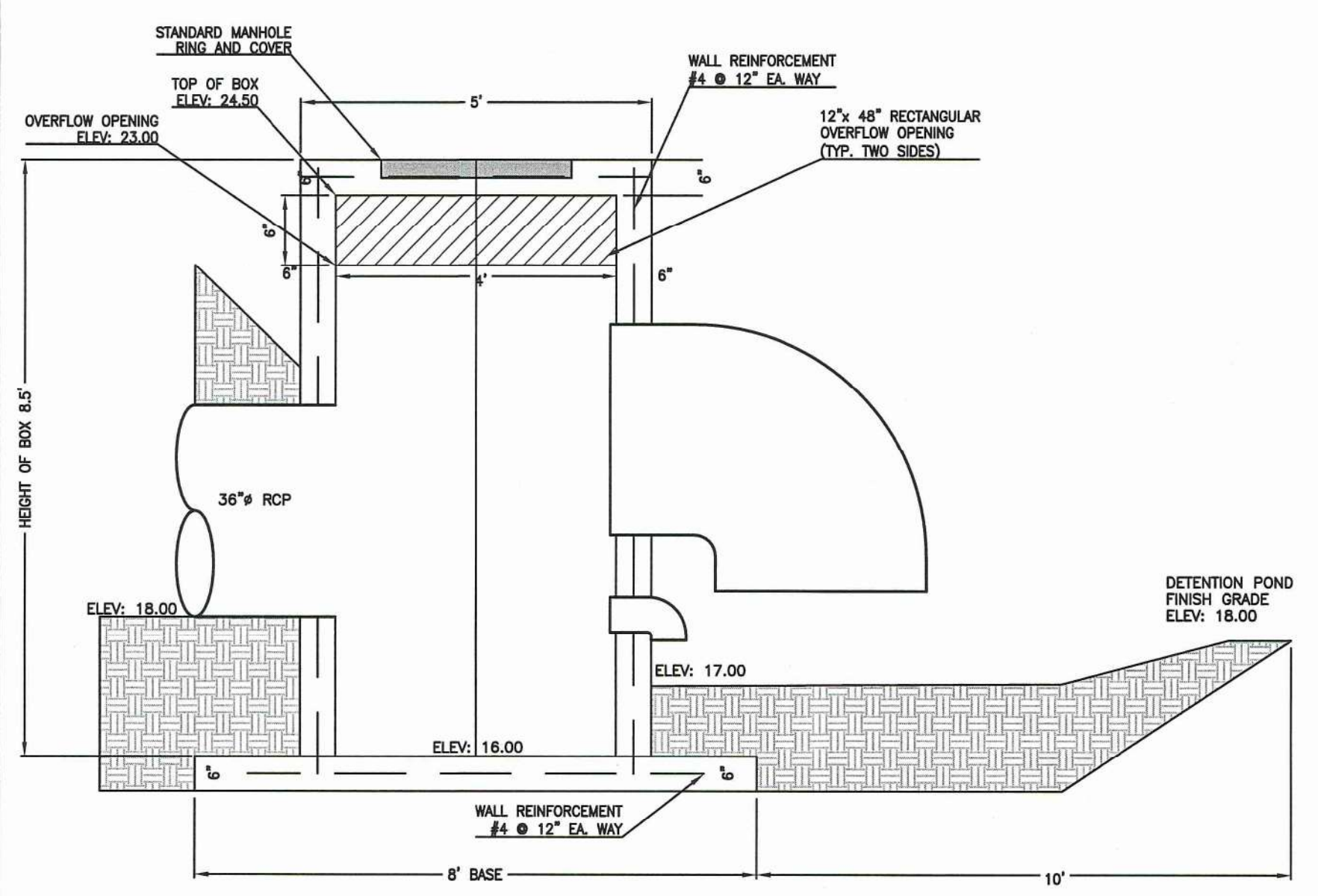
- PROPERTY LINE / R.O.W.
- PROPOSED BUILDING
- PROPOSED CURB AND GUTTER
- PROPOSED INLET
- PROPOSED CURB INLET
- PROPOSED STORM SEWER
- ✖12.10 PROPOSED ELEVATION

EXISTING LEGEND

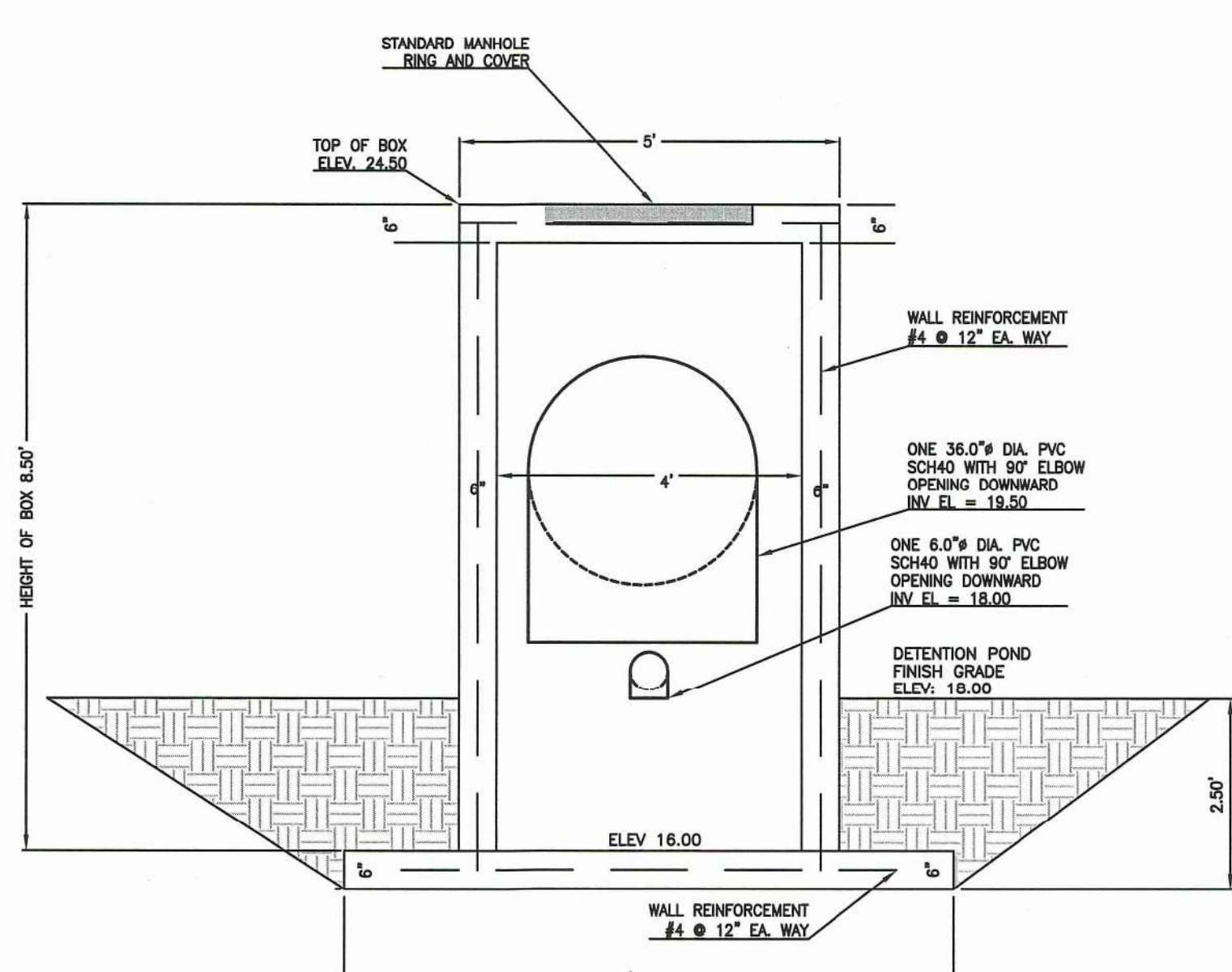
- SUBJECT PROPERTY BOUNDARY LINE
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- APR AS PER RECORD
- ROW RIGHT-OF-WAY
- G2- CONTOUR ELEVATION
- RCP REINFORCED CONCRETE PIPE
- CMP CORRUGATED METAL PIPE
- ⊗ ROAD SIGN POST



ANTI-SEEP COLLAR DETAIL



SIDE VIEW
NTS



FRONT VIEW
NTS

OUTLET CONTROL STRUCTURE

DRAINAGE BASIN AND POND NOTES:

- REFERENCE HYDROLOGIC SUMMARY VERSION 6A PROVIDED BY RESOURCE CONSULTING, LLC FOR SUPPORTING HYDRAULIC INFORMATION.
- REFERENCE GEOTECHNICAL INVESTIGATION AND REPORT PERFORMED BY TERRACON CONSULTANTS INC., TEL: 228-283-7050, FOR ALL EARTHWORK SPECIFICATIONS.
- REFERENCE SHEETS C4.1, C4.2, AND C4.3 FOR ALL DETAIL ON PROPER INSTALLATION AND MAINTENANCE OF BEST MANAGEMENT PRACTICES FOR SEDIMENT AND EROSION CONTROL.
- REFERENCE SITEWORK SPECIFICATIONS



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STORM WATER MANAGEMENT SYSTEM DETAIL





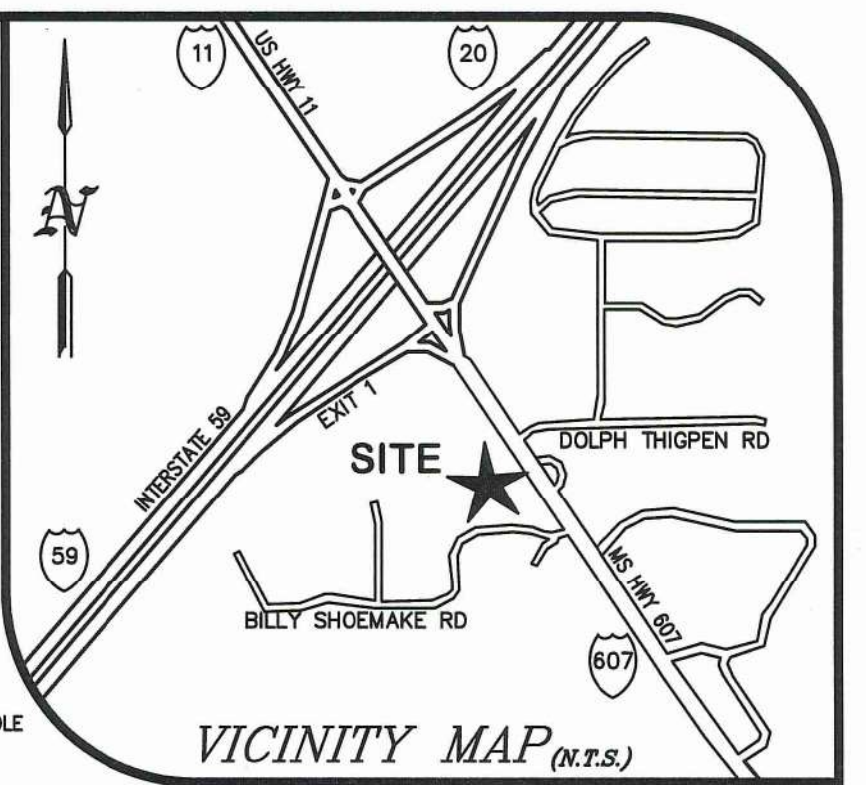
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LOVE'S TRAVEL STOP
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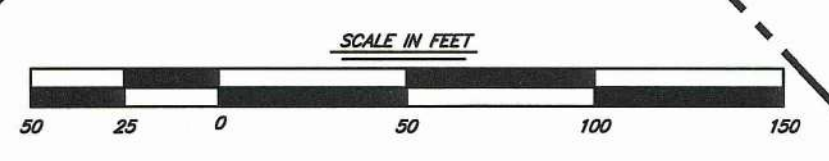


LEGEND FOR PROPOSED IMPROVEMENTS

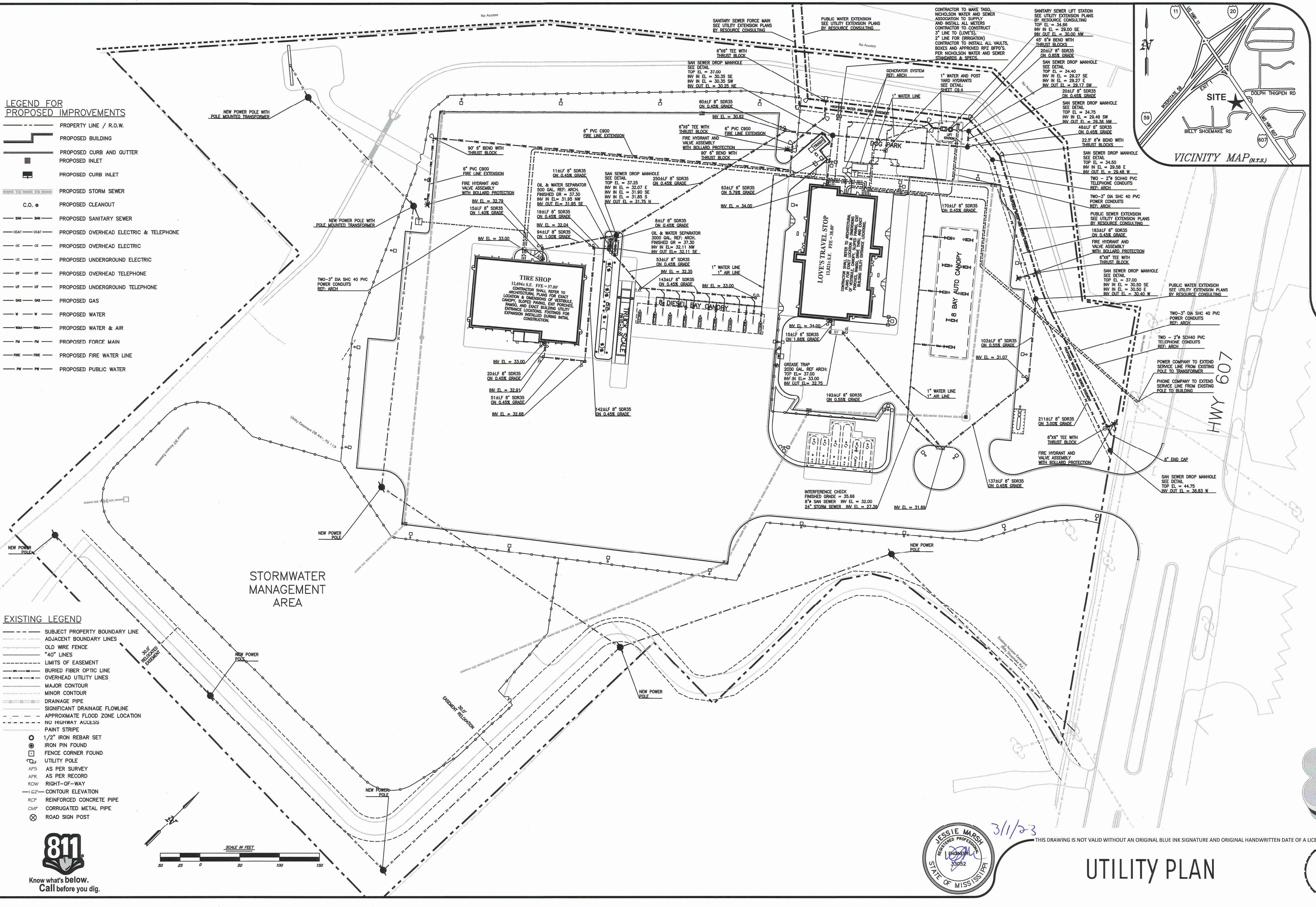
- PROPERTY LINE / R.O.W.
- PROPOSED BUILDING
- PROPOSED CURB AND GUTTER
- PROPOSED INLET
- PROPOSED CURB INLET
- PROPOSED STORM SEWER
- C.O. PROPOSED CLEANOUT
- PROPOSED SANITARY SEWER
- PROPOSED OVERHEAD ELECTRIC & TELEPHONE
- PROPOSED OVERHEAD ELECTRIC
- PROPOSED UNDERGROUND ELECTRIC
- PROPOSED OVERHEAD TELEPHONE
- PROPOSED UNDERGROUND TELEPHONE
- PROPOSED GAS
- PROPOSED WATER
- PROPOSED WATER & AIR
- PROPOSED FORCE MAIN
- PROPOSED FIRE WATER LINE
- PROPOSED PUBLIC WATER

EXISTING LEGEND

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- AS PER RECORD
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- CONTOUR ELEVATION
- REINFORCED CONCRETE PIPE
- CORRUGATED METAL PIPE
- ROAD SIGN POST



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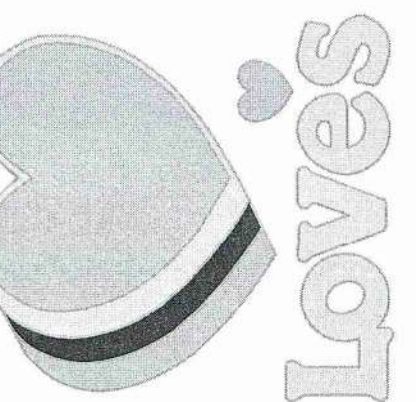
UTILITY PLAN



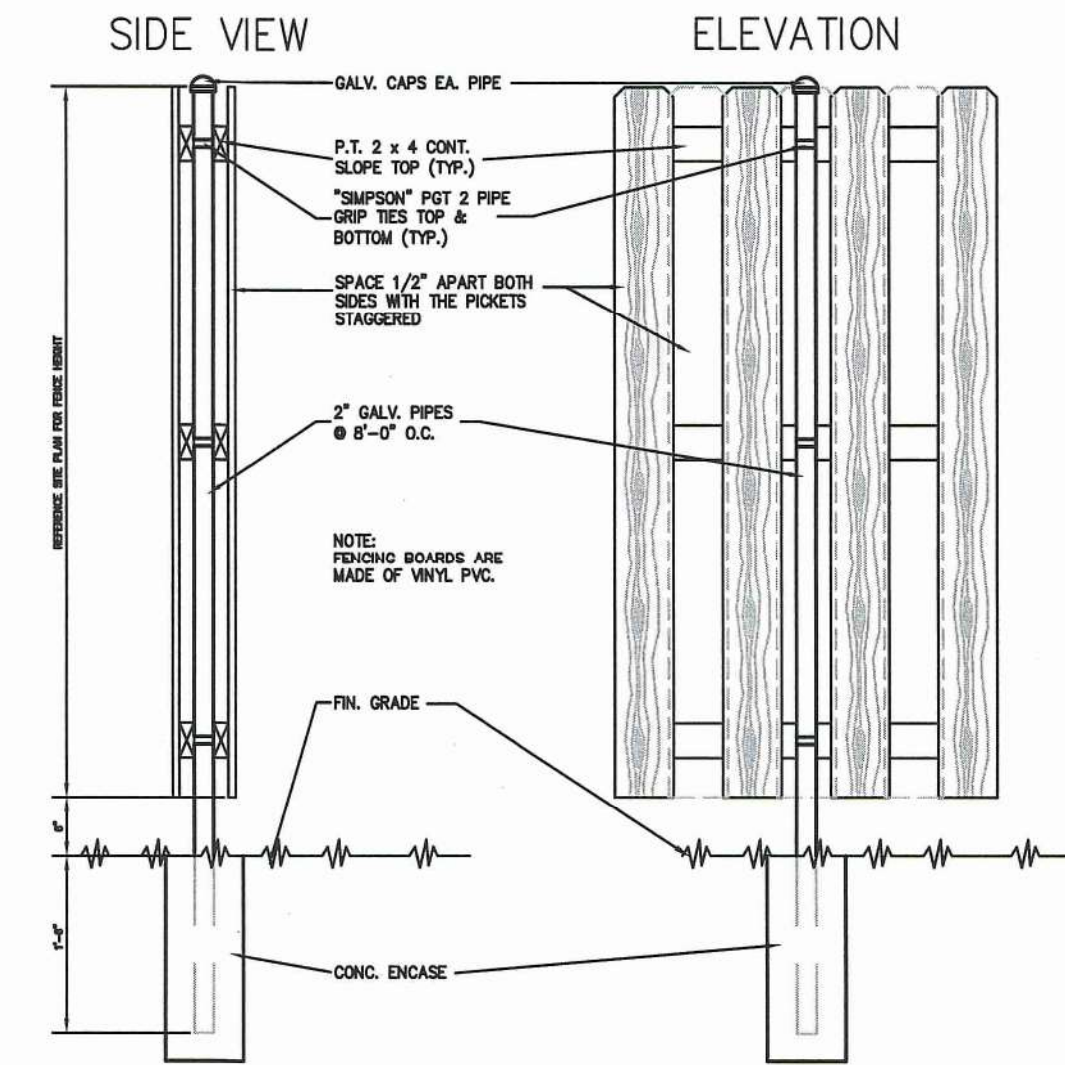
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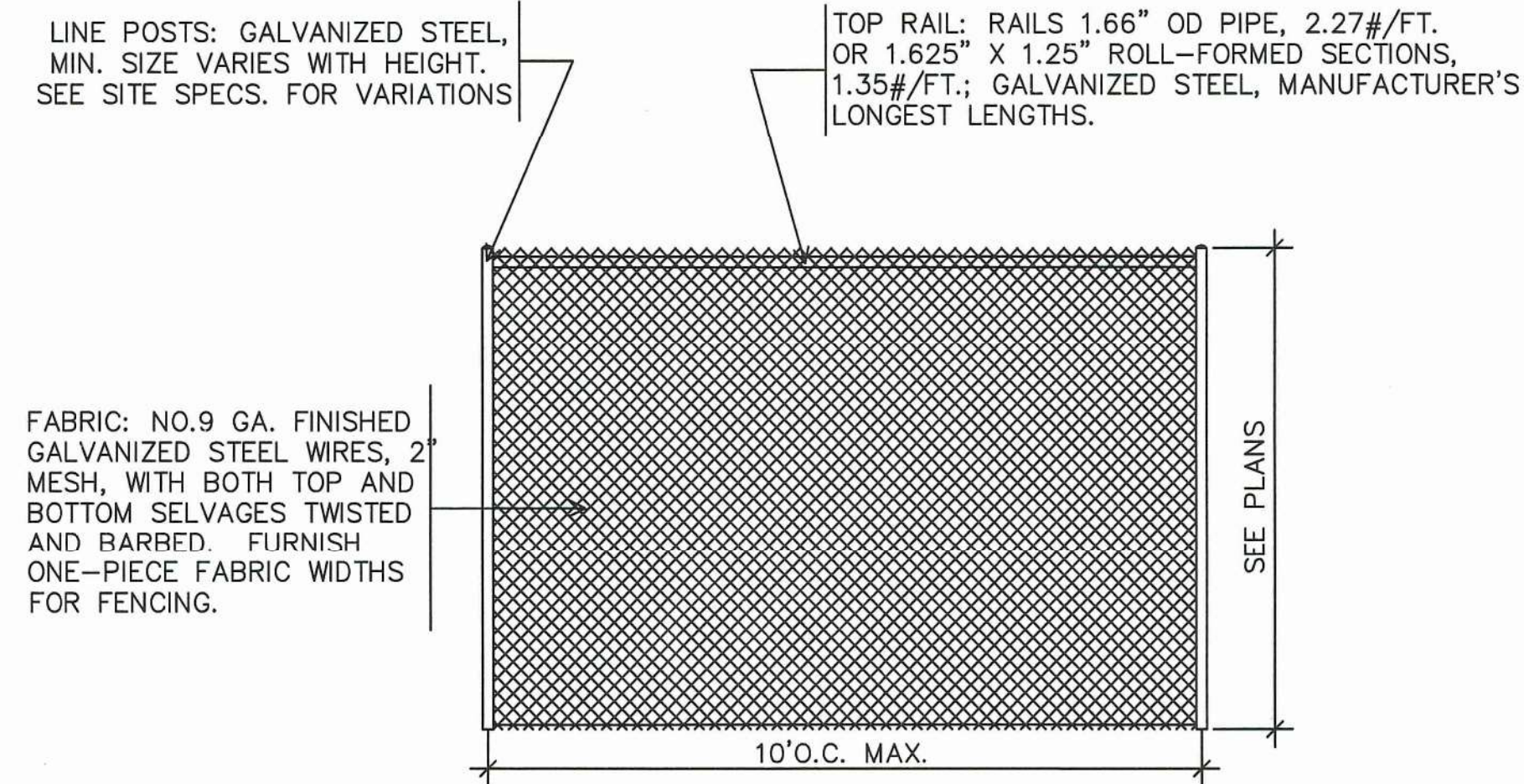
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 PEARL RIVER COUNTY, MISSISSIPPI



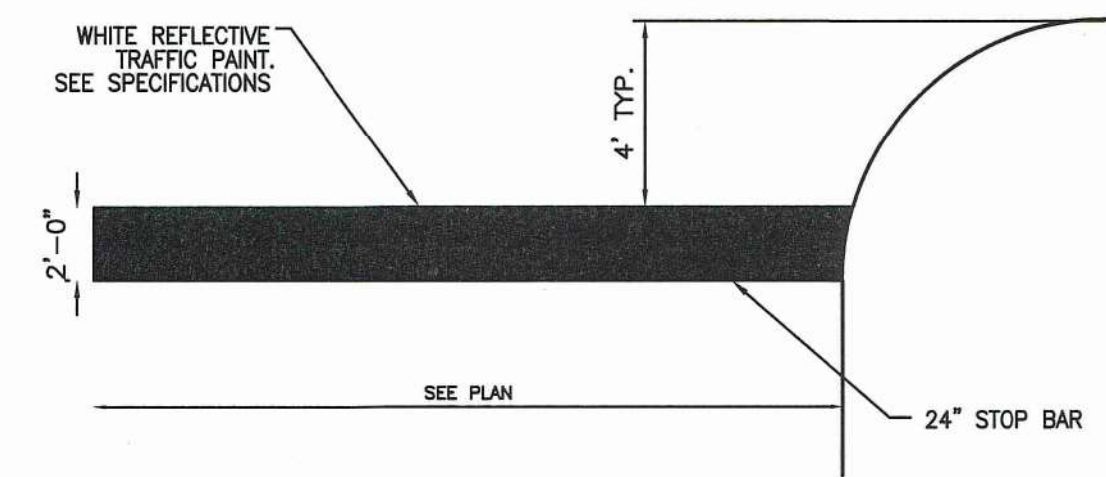
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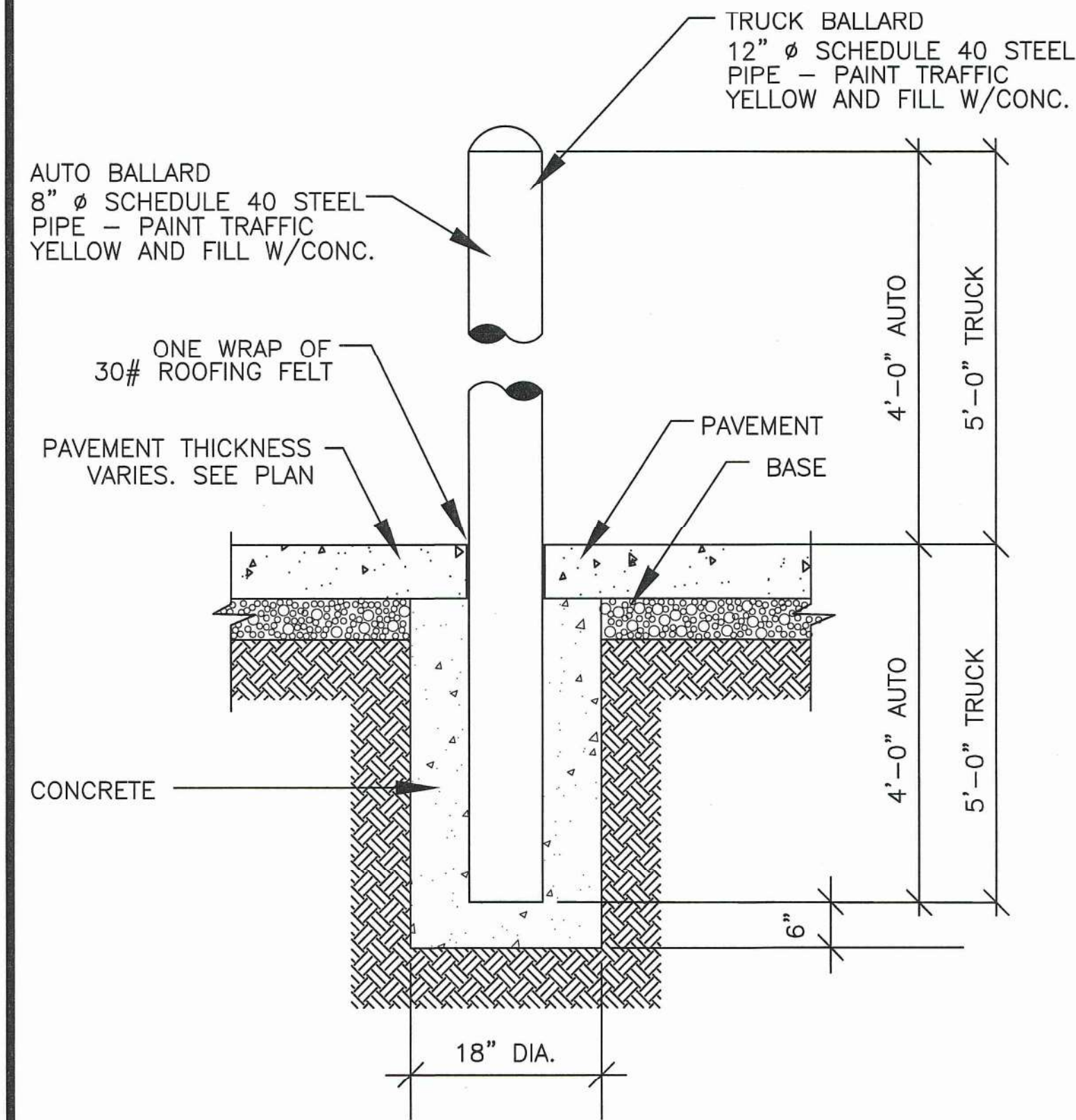
VINYL PVC FENCE DETAIL
 N.T.S.



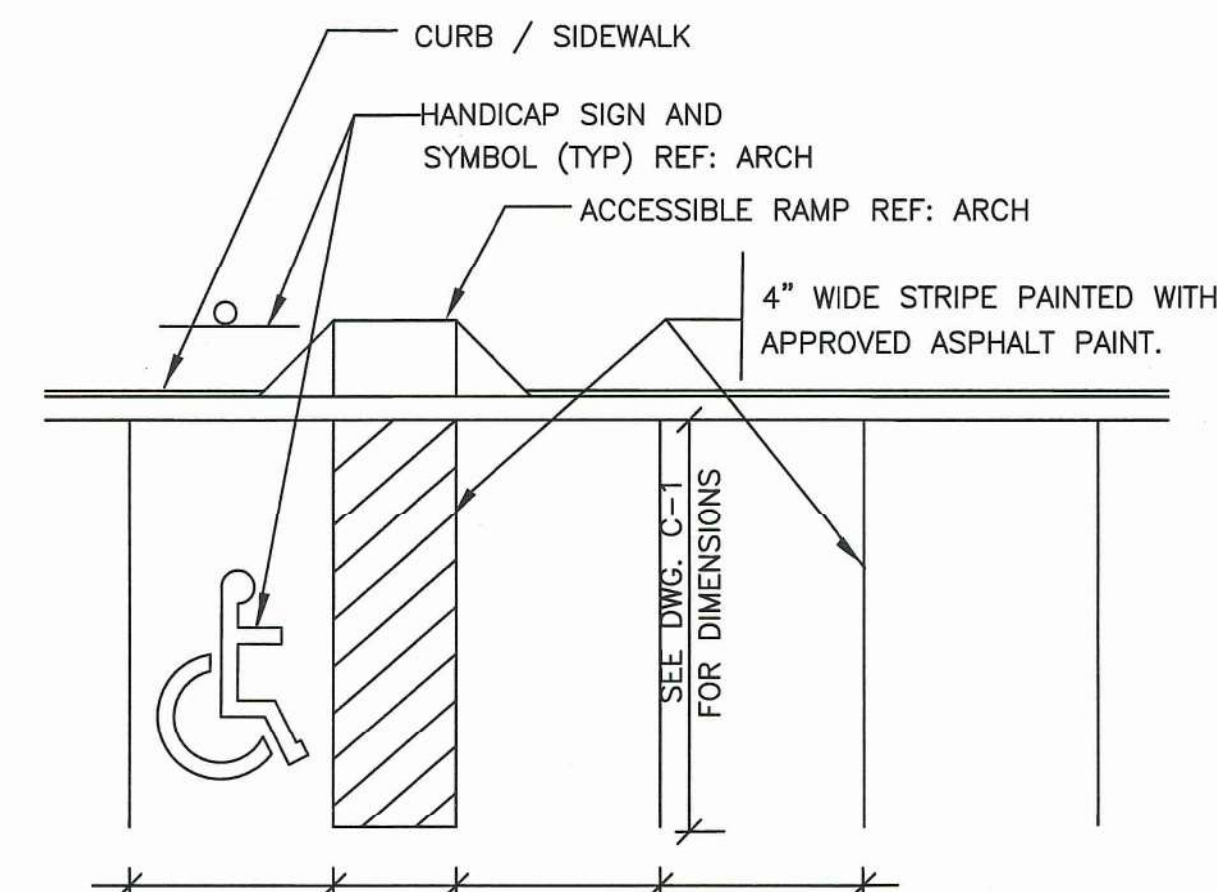
CHAIN LINK FENCE DETAIL
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STOP BAR AND PAVEMENT MARKINGS DETAIL
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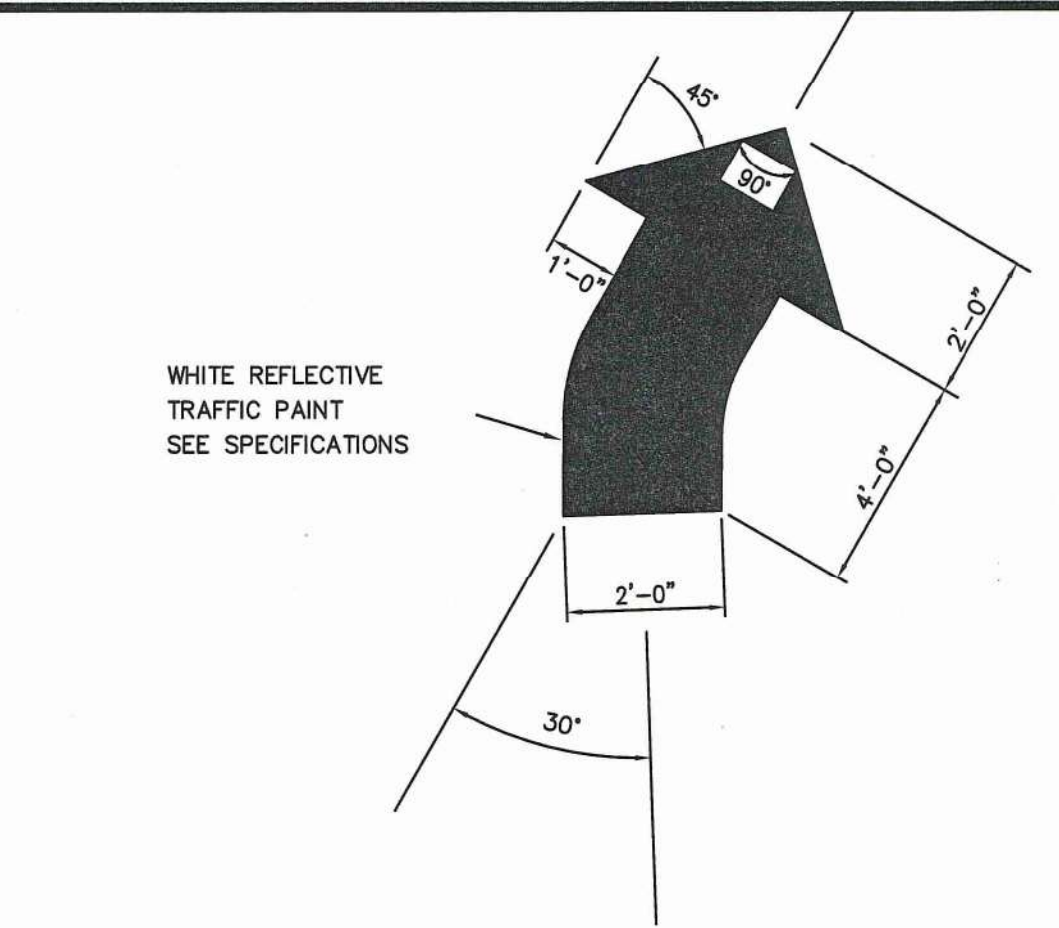


PIPE BOLLARD DETAIL
 N.T.S. CS27

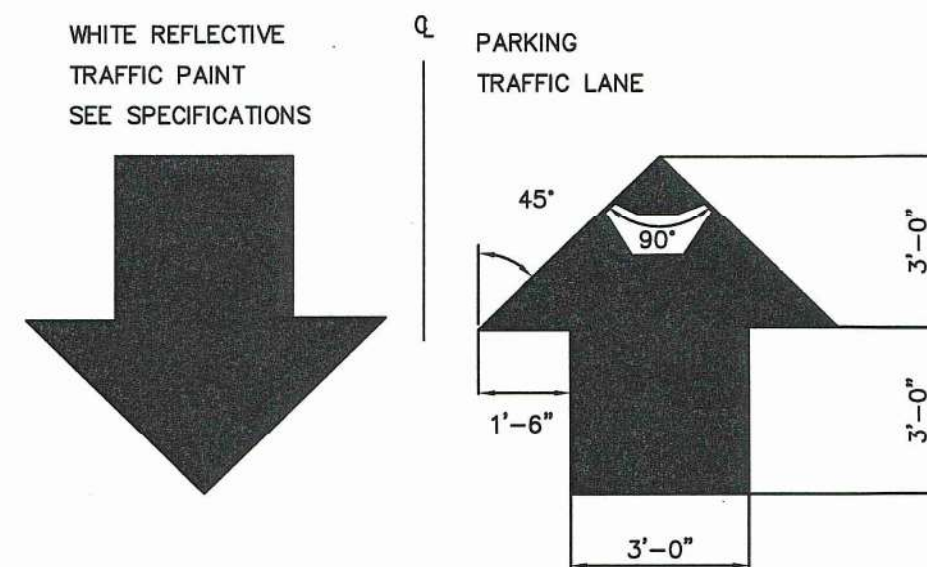


90° PARKING DETAIL
 NO SCALE CS21

REFERENCE ARCHITECTURAL PLANS FOR ALL ACCESSIBLE DETAILS FOR RAMPS, PARKING, SIGNAGE, ETC.

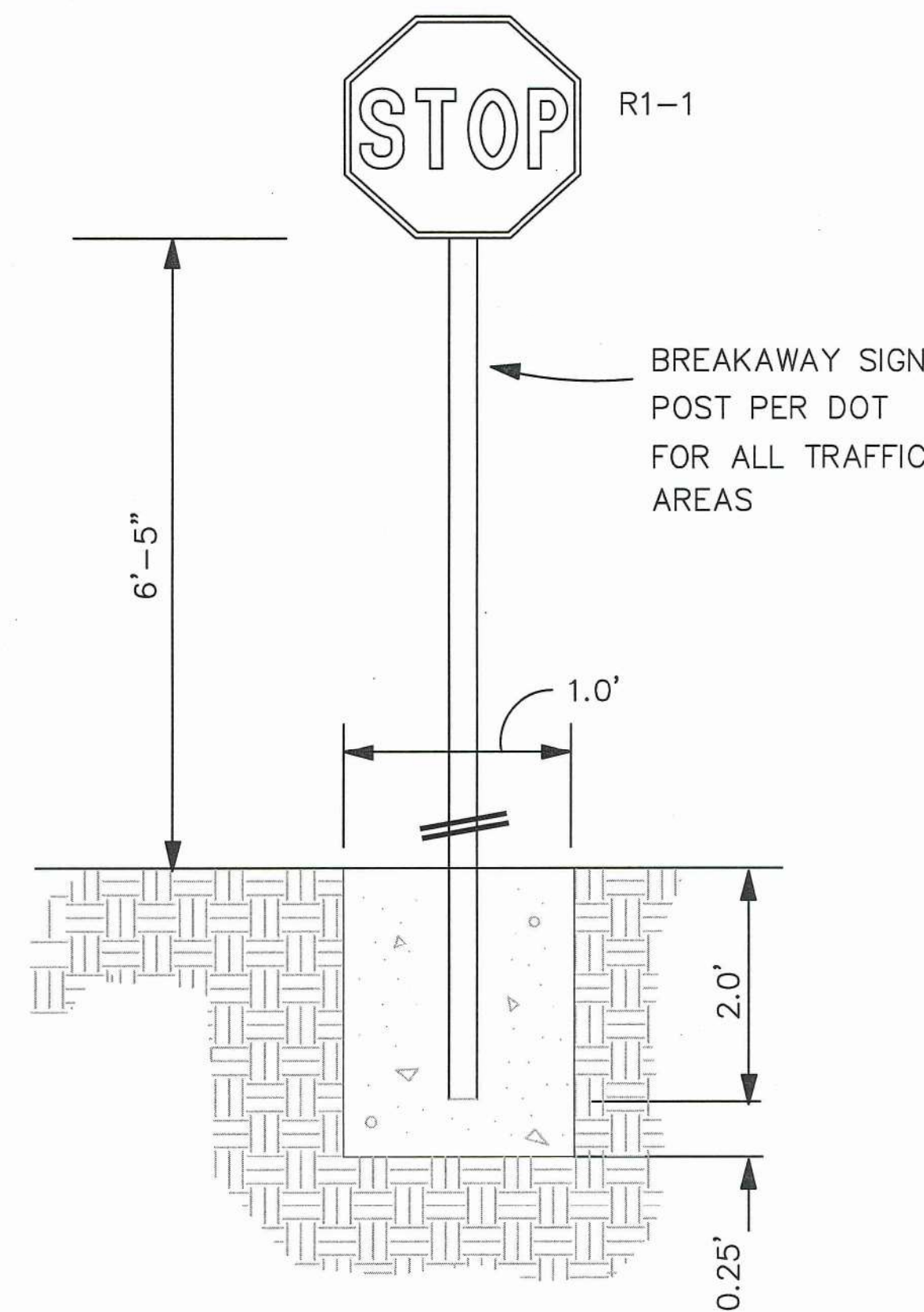


DIRECTIONAL "TURN" ARROW DETAIL
 N.T.S. CS16



DIRECTIONAL ARROW DETAIL
 N.T.S. CS09

ALL ONSITE DIRECTIONAL ARROWS SHOWN ON THE SITE PLAN ARE SOLELY FOR THE PURPOSE OF ILLUSTRATING THE PROPOSED TRAFFIC CIRCULATION PATTERNS. THE LOVES CONSTRUCTION DEPARTMENT WILL INDICATE TO THE CONTRACTOR WHERE TO STIPE EACH AND ALL DIRECTIONAL TRAFFIC ARROWS IN THE FIELD.



STOP SIGN
 N.T.S. CS44



Know what's below.
 Call before you dig.



THIS DRAWING IS NOT VALID WITHOUT AN ORIGINAL BLUE INK SIGNATURE AND ORIGINAL HANDWRITTEN DATE OF A LICENSED PROFESSIONAL ENGINEER.

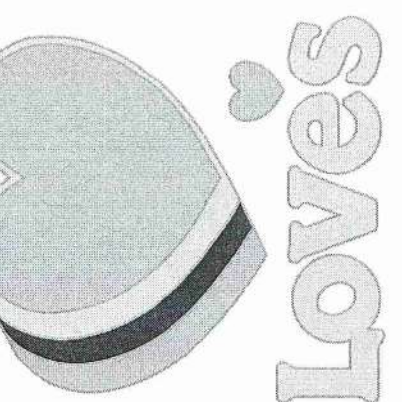
SITE DETAILS



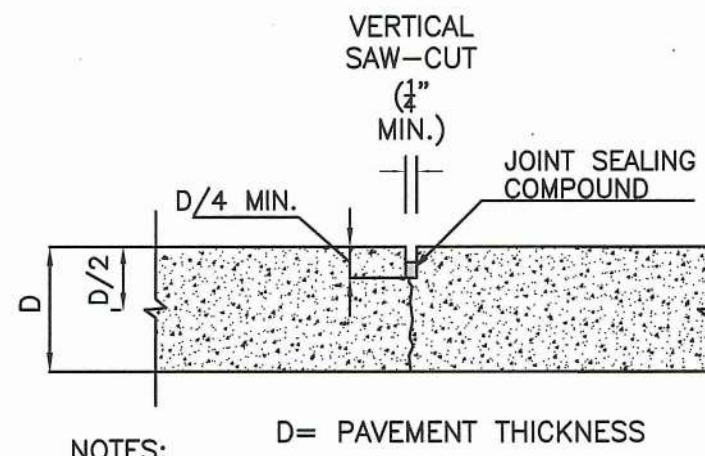
RESOURCECONSULTING
CIVIL ENGINEERING
6700 Jefferson Highway - Resource Suite 4A
Baton Rouge, Louisiana 70806
ResourceConsulting@hotmail.com
Tel: (225) 761-9909, Fax: (225) 766-6672



LOVE'S TRAVEL STOP
INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
PEARL RIVER COUNTY, MISSISSIPPI

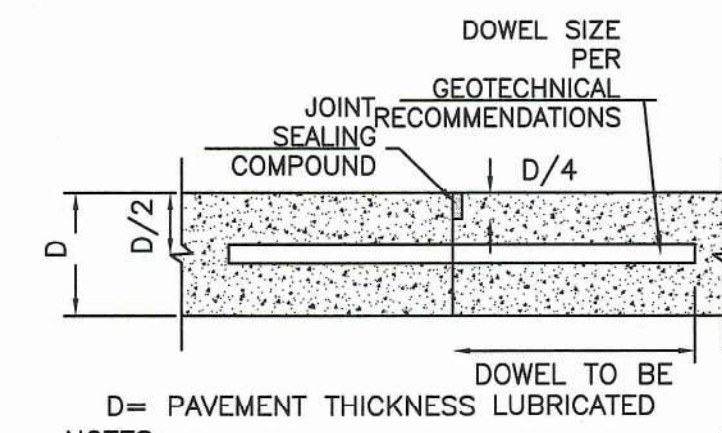


C9.1



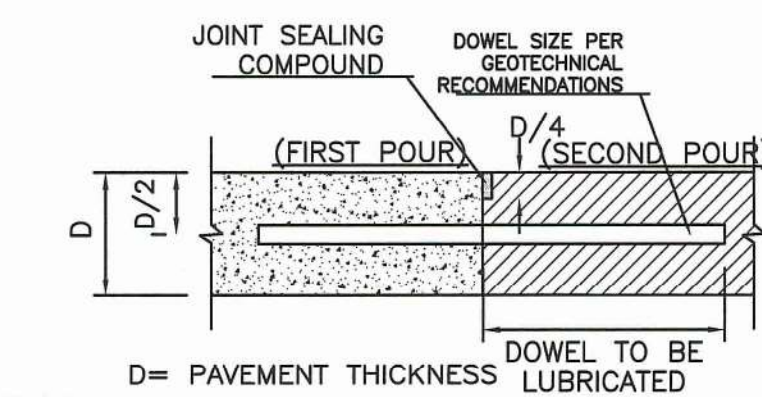
- NOTES:**
1. MAXIMUM CONTRACTION JOINT SPACING IS DEPENDENT UPON THE PAVEMENT THICKNESS. REFER TO TABLE BELOW FOR SPACING REQUIREMENTS.
 2. ENSURE CONTRACTION JOINT PATTERNS DIVIDE PAVEMENT INTO PANELS AS CLOSE TO SQUARE AS POSSIBLE.

CONTRACTION JOINT DETAIL
N.T.S.



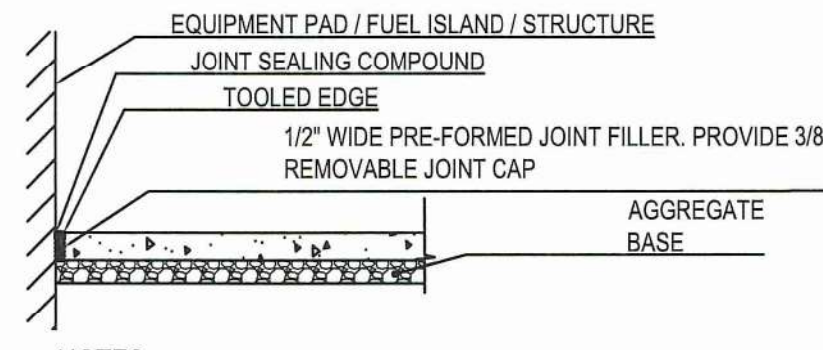
- NOTES:**
1. DOWELS SHALL BE SUPPORTED BY AN APPROVED DEVICE.
 2. ALL BARS AND BASKETS SHALL BE EPOXY COATED.
 3. DOWELED CONTRACTION JOINTS ARE REQUIRED WHERE PAVEMENT DEPTH IS 7" OR GREATER.
 4. DOWELS SHALL BE SMOOTH BARS AND FREE MOVEMENT SHALL BE PROVIDED BY APPLYING A COATING OF GREASE AS A BOND-BREAKING MATERIAL JUST PRIOR TO PLACING THE CONCRETE.

DOWELED CONTRACTION JOINT DETAIL
N.T.S.



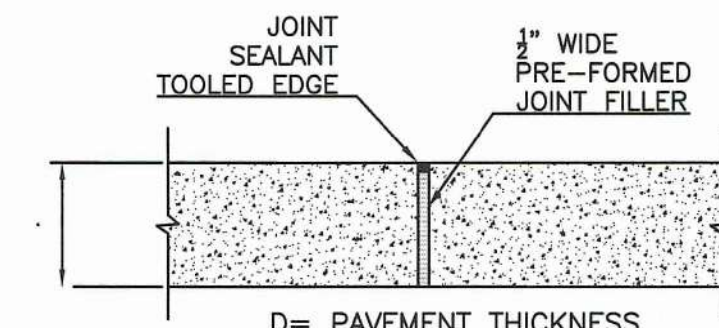
- NOTES:**
1. DOWELS SHALL BE SUPPORTED BY AN APPROVED DEVICE.
 2. ALL BARS AND BASKETS SHALL BE EPOXY COATED.
 3. PLACE CONSTRUCTION JOINTS AT END OF PLACEMENTS AND AT LOCATIONS WHERE PLACEMENT OPERATIONS ARE STOPPED FOR A PERIOD OF MORE THAN 1/2 HOUR.
 4. DOWELS SHALL BE SMOOTH BARS AND FREE MOVEMENT SHALL BE PROVIDED BY APPLYING A COATING OF GREASE AS A BOND-BREAKING MATERIAL JUST PRIOR TO PLACING THE CONCRETE.
 5. IN EMERGENCY SITUATIONS SUCH AS LACK OF MATERIALS, SUDDEN CHANGES IN WEATHER, OR EQUIPMENT BREAKDOWN, A CONSTRUCTION JOINT SHOULD BE INSTALLED IN PLACE OF THE NEAREST CONTRACTION JOINT LOCATION.
 6. CONSTRUCTION JOINTS ARE NOT TO BE LOCATED CLOSER THAN 10'-0" TO ANOTHER PARALLEL JOINT.
 7. WHERE SLABS OF DIFFERENT THICKNESSES COME TOGETHER AT JOINTS, THE SUBGRADE/SUBBASE UNDER THE THINNER PAVEMENT SECTIONS SHOULD BE SHAPED TO PROVIDE GRADUAL THICKNESS TRANSITION OVER A DISTANCE OF 4 FT OR MORE.

CONTRACTION JOINT DETAIL
N.T.S.

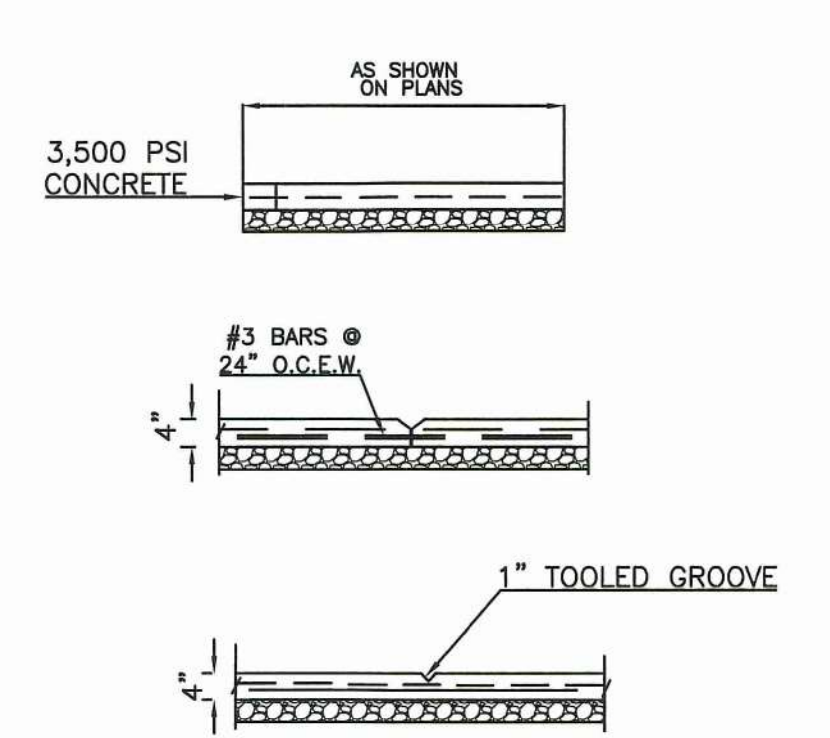
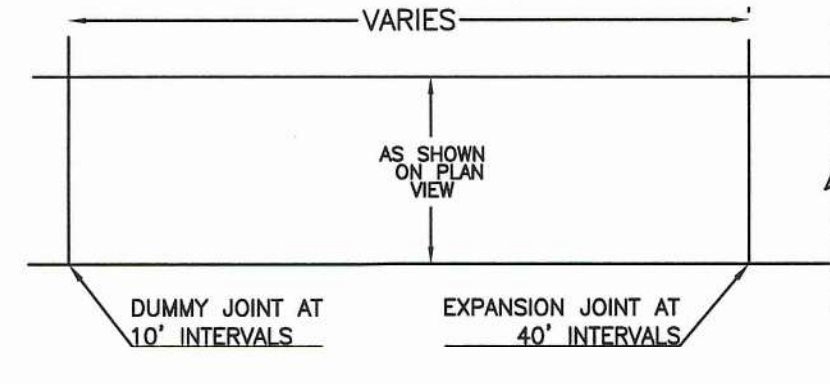


- NOTES:**
1. PLACE ISOLATION JOINTS WHERE CONCRETE PAVEMENT ABUTS SLABS, FUEL ISLANDS, CANOPY COLUMNS, BOLLARDS AND STRUCTURES OR FIXED OBJECTS.
 2. PRE-FORMED JOINT FILLER - NON-IMPREGNATED TYPE, CLOSED CELL RESILIENT POLYETHYLENE FOAM, 1/2" THICK UNLESS OTHERWISE NOTED.
 3. WHERE SLABS OF DIFFERENT THICKNESSES COME TOGETHER AT ISOLATION JOINTS, THE SUBGRADE/SUBBASE UNDER THE THINNER PAVEMENT SECTIONS SHOULD BE SHAPED TO PROVIDE GRADUAL THICKNESS TRANSITION OVER A DISTANCE OF 4 FT OR MORE.

ISOLATION JOINT DETAIL
N.T.S.



EXPANSION JOINT DETAIL
N.T.S.



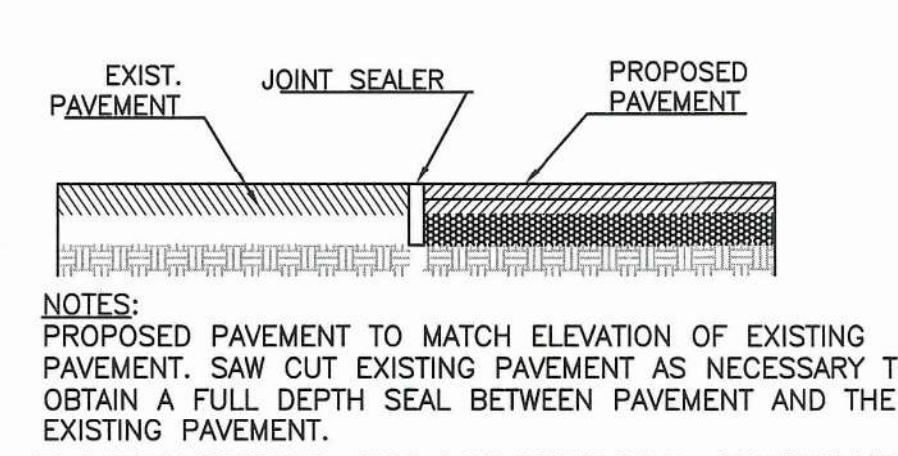
SIDEWALK DETAIL
N.T.S.

GENERAL JOINTING NOTES:

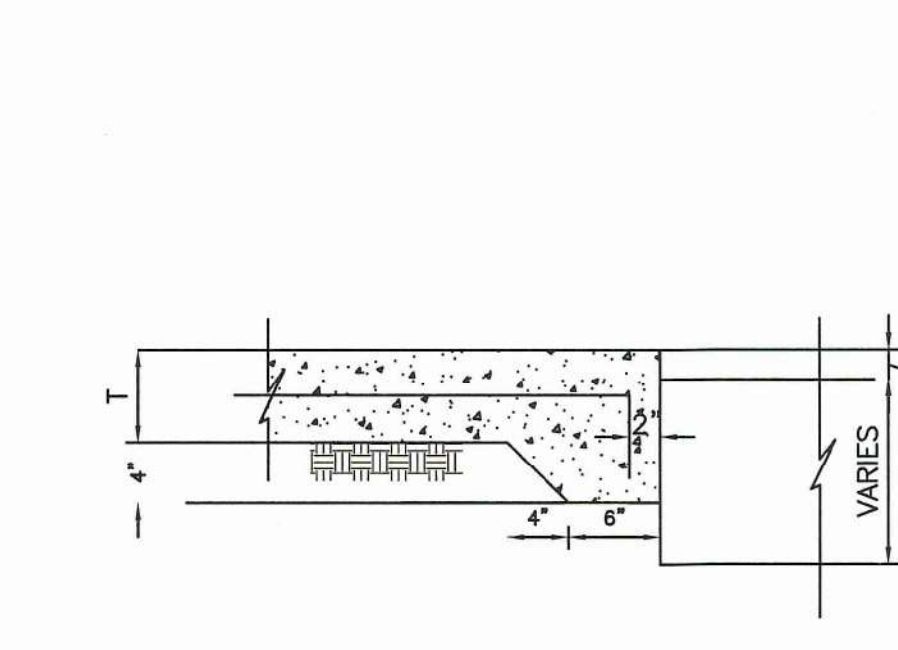
1. NOTES AND DETAILS BASED ON ACI 330.2R-17 AND 330.R-8 GUIDELINES. ALL RECOMMENDATIONS SHOULD BE CONFIRMED ON A PROJECT BY PROJECT BASIS WITH THE CIVIL AND/OR GEOTECHNICAL ENGINEER. IF THE DETAILS AND RECOMMENDATIONS PROVIDED DO NOT COMPLY WITH THE STANDARDS OF THE LATEST INDUSTRY STANDARDS, OR THE STANDARDS ESTABLISHED BY THE AUTHORITY HAVING JURISDICTION, THIS SHOULD BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE OWNER'S REPRESENTATIVE.
2. CONSTRUCTION AND TRANSVERSE JOINTS SHALL BE RE-SAWN AND ALL SAW LAITANCE VACCUMED FROM THE JOINT ONCE BOTH SIDES ARE POURED.
3. PAVEMENT JOINTS OF ANY TYPE SHOULD EXTEND THROUGH ANY CURB AND GUTTER.
4. CONCRETE CONTRACTOR SHALL SUBMIT THE CONCRETE MIX TO THE OWNER'S REPRESENTATIVE FOR REVIEW AND APPROVAL.
5. A SCALED DRAWING OF THE JOINTING PLAN FOR THE PAVEMENTS AND CURBS SHOULD BE SUBMITTED BY THE CONTRACTOR FOR REVIEW BY THE CIVIL AND GEOTECHNICAL ENGINEERS.

JOINT SPACING FOR UNREINFORCED CONCRETE (FROM ACI 330.2R-17 TABLE 4.4.4 & ACI 330.R-8 TABLE 3.5)	
PAVEMENT THICKNESS (IN)	MAXIMUM SPACING (FT)
4 - 4.5	10
5 - 5.5	12.50
6 OR GREATER	15

GENERAL JOINTING NOTES
N.T.S.



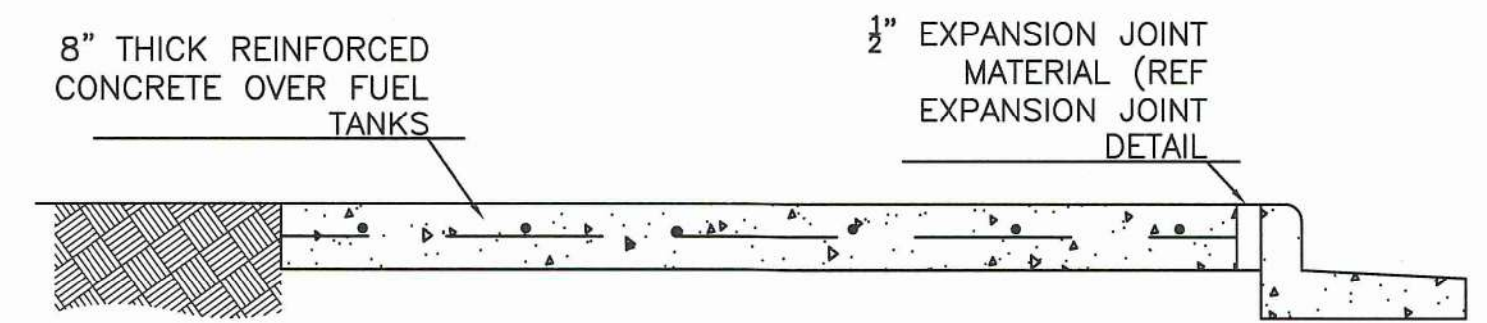
PAVEMENT TRANSITION DETAIL
N.T.S.



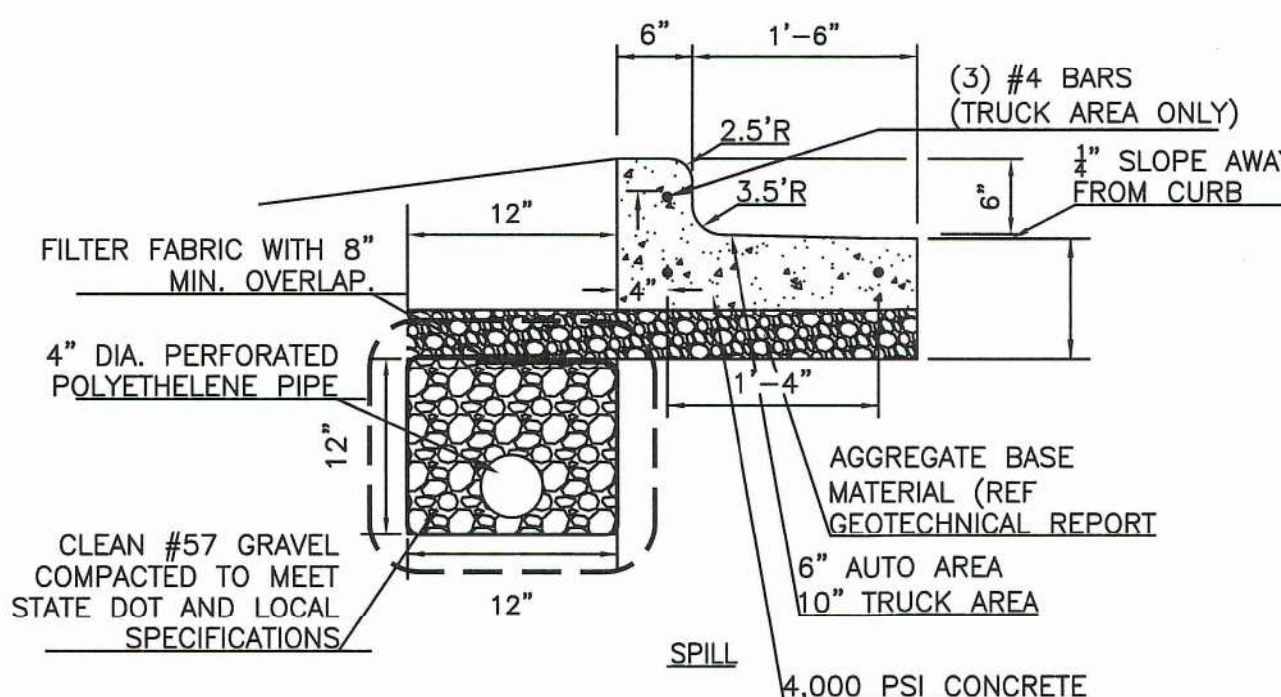
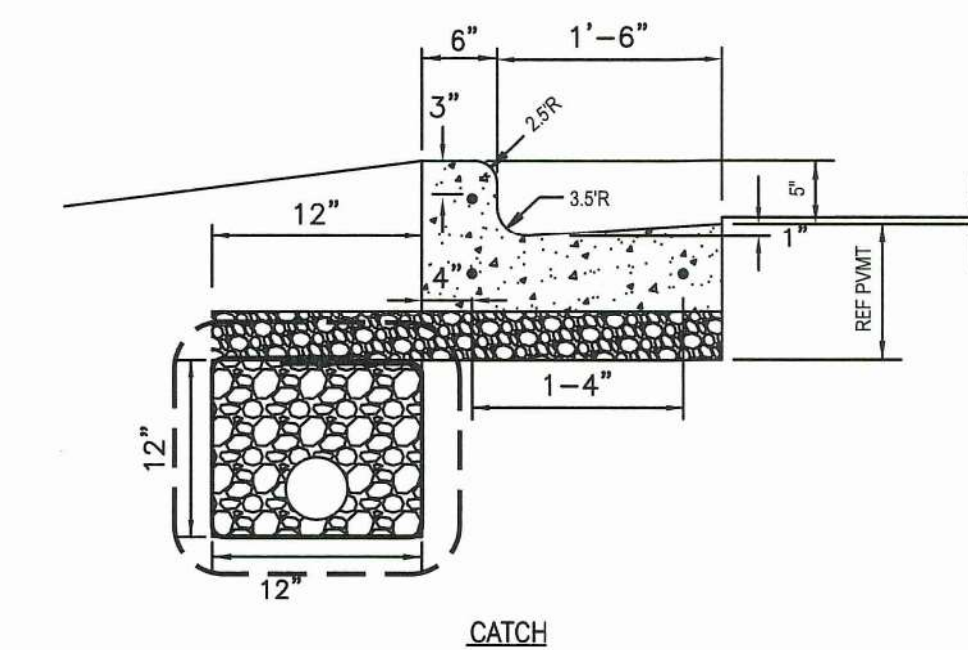
CONCRETE TO ASPHALT STANDARD PAVING DETAIL
N.T.S.

- SIDEWALK JOINT NOTES**
1. ALL JOINTS TO BE SEALED TO BE THOROUGHLY CLEANED BY HYDROBLASTING AND/OR SAND BLASTING METHODS. THE JOINTS ARE TO BE FREE FROM ALL DUST COATINGS, ANY CONTAMINATES, AND FREE FROM ALL MOISTURE THAT MIGHT INTERFERE WITH THE PROPER AND SATISFACTORY BONDING OF THE JOINT SEALANT MATERIAL. THE JOINT WILL BE BLOWN OUT WITH DRY COMPRESSED AIR IMMEDIATELY PRIOR TO APPLYING SEALANT.
 2. CONSTRUCTION EQUIPMENT AND OTHER VEHICLES AND PEDESTRIANS WHICH MAY CAUSE DAMAGE TO THE JOINTS SHALL NOT BE ALLOWED ON THE PAVEMENT & SIDEWALKS BEFORE THE JOINT SEALANT BECOMES TACK FREE.
 3. USE 3/4" X 4" EXPANSION JOINT MATERIAL ALONG SIDEWALK CURB AND ALONG BUILDING.
 4. USE 3/4" X 4" EXPANSION JOINT MATERIAL AROUND POLES OR OTHER OBSTRUCTIONS IN WALK AND FOR JOINTS SHOWN AS EXPANSION JOINT ON THE PLAN VIEW OF THE SITE.
 5. SAW CUT JOINTS NOT ALLOWED. USE DEEP TOOLED JOINTS.
 6. SIDEWALK EXPANSION JOINTS ARE NOT DOWELED.

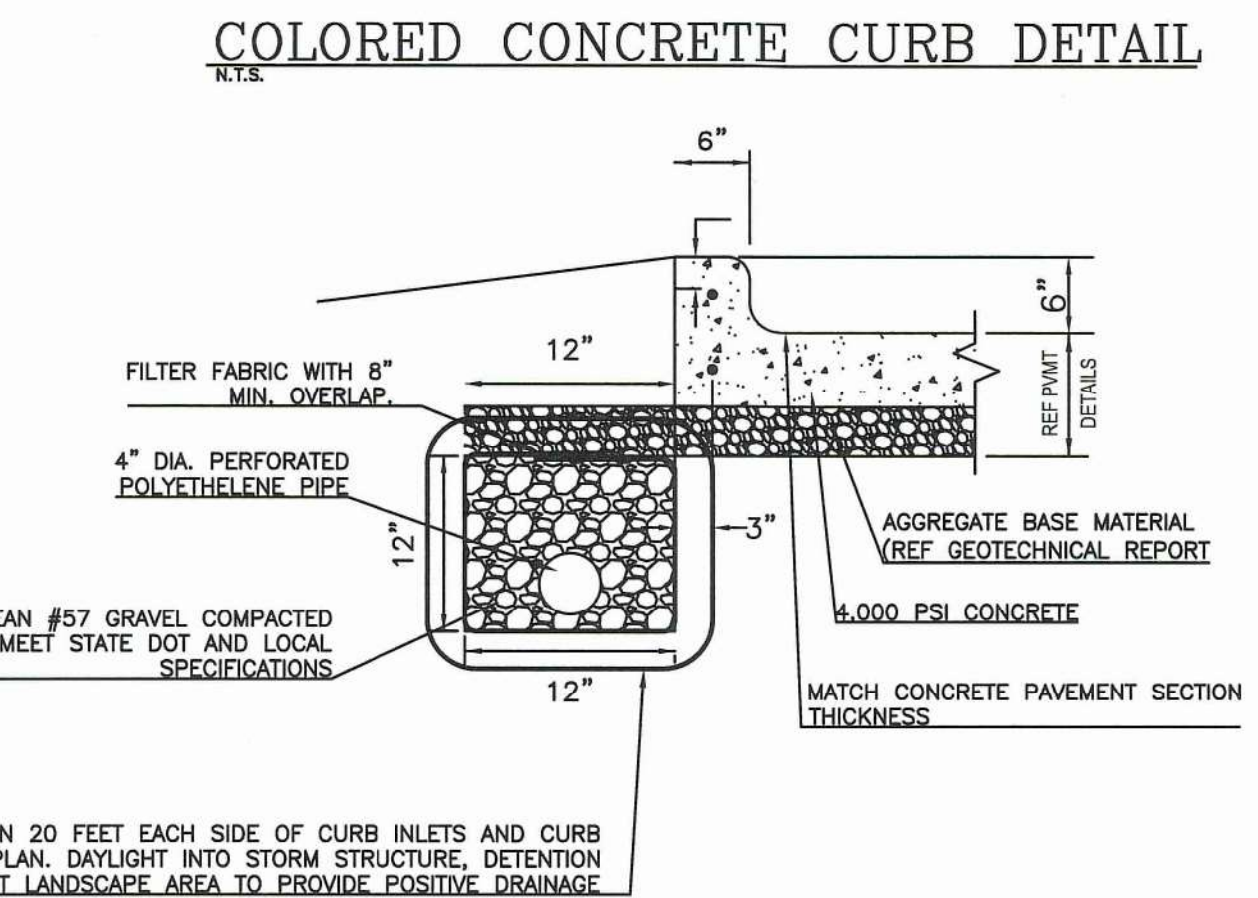
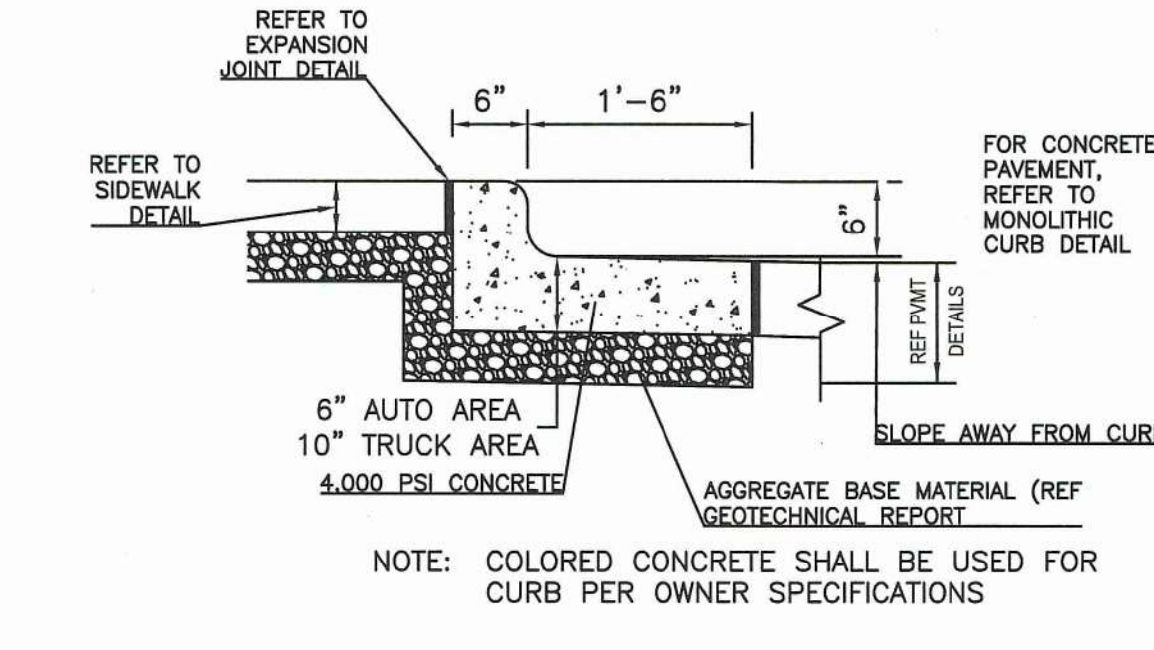
- JOINT SEALANT NOTES**
- **JOINT SEALERS FOR SIDEWALKS SHALL CONFORM TO THE FOLLOWING:
- (1) LOW MODULUS SILICONE JOINT SEALANT, MACHINE EXTRUDED OR APPLIED BY GUN. GREY IN COLOR.
 - (2) (1) LOW MODULUS SILICONE JOINT SEALANT, MACHINE EXTRUDED OR APPLIED BY GUN. FOR SLAB, SEALANT SHALL BE DOW CORNING 890 SL, DOW CORNING 888 OR EQUAL (JOINT MUST BE TOOLED). FOR CURBS, SEALANT SHALL BE DOW CORNING 888 OR EQUAL (JOINT MUST BE TOOLED).



TYPICAL SECTION THROUGH PAVING OVER FUEL TANKS
N.T.S.



CONCRETE CURB AND GUTTER DETAIL
N.T.S.



6" MONOLITHIC CONCRETE CURB DETAIL
N.T.S.



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PAVEMENT DETAILS

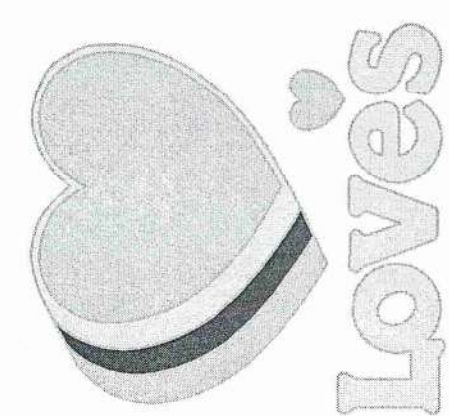




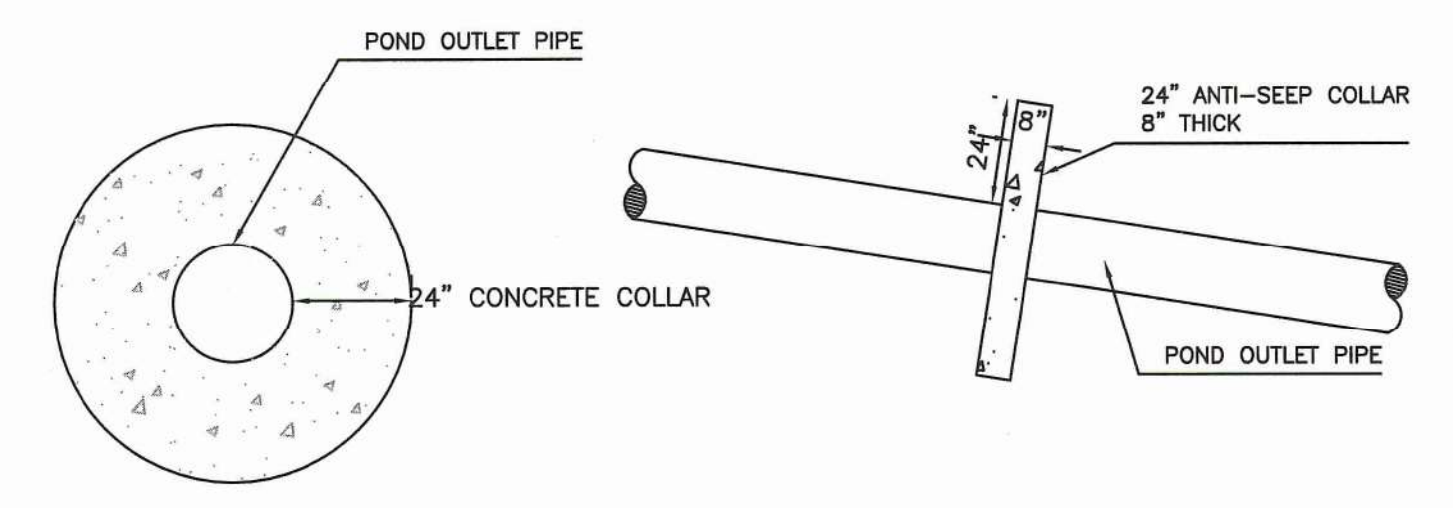
RESOURCECONSULTING
 CIVIL ENGINEERING
 6700 Jefferson Highway - Resource Suite 4A
 Baton Rouge, Louisiana 70806
 ResourceConsulting@hotmail.com
 Tel: (225) 761-9905, Fax: (225) 766-6672



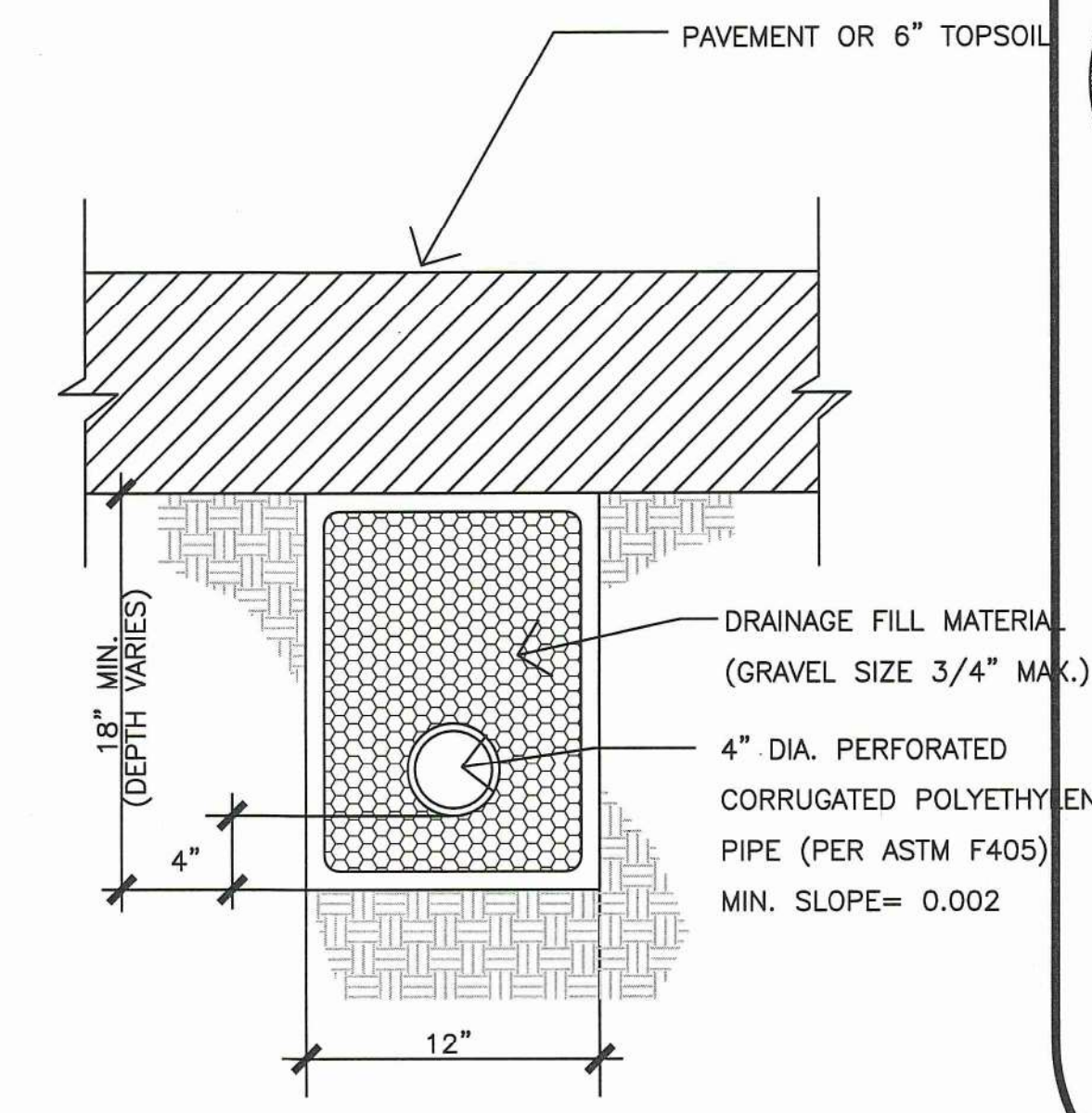
LOVE'S TRAVEL STOP
 INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
 PEARL RIVER COUNTY, MISSISSIPPI



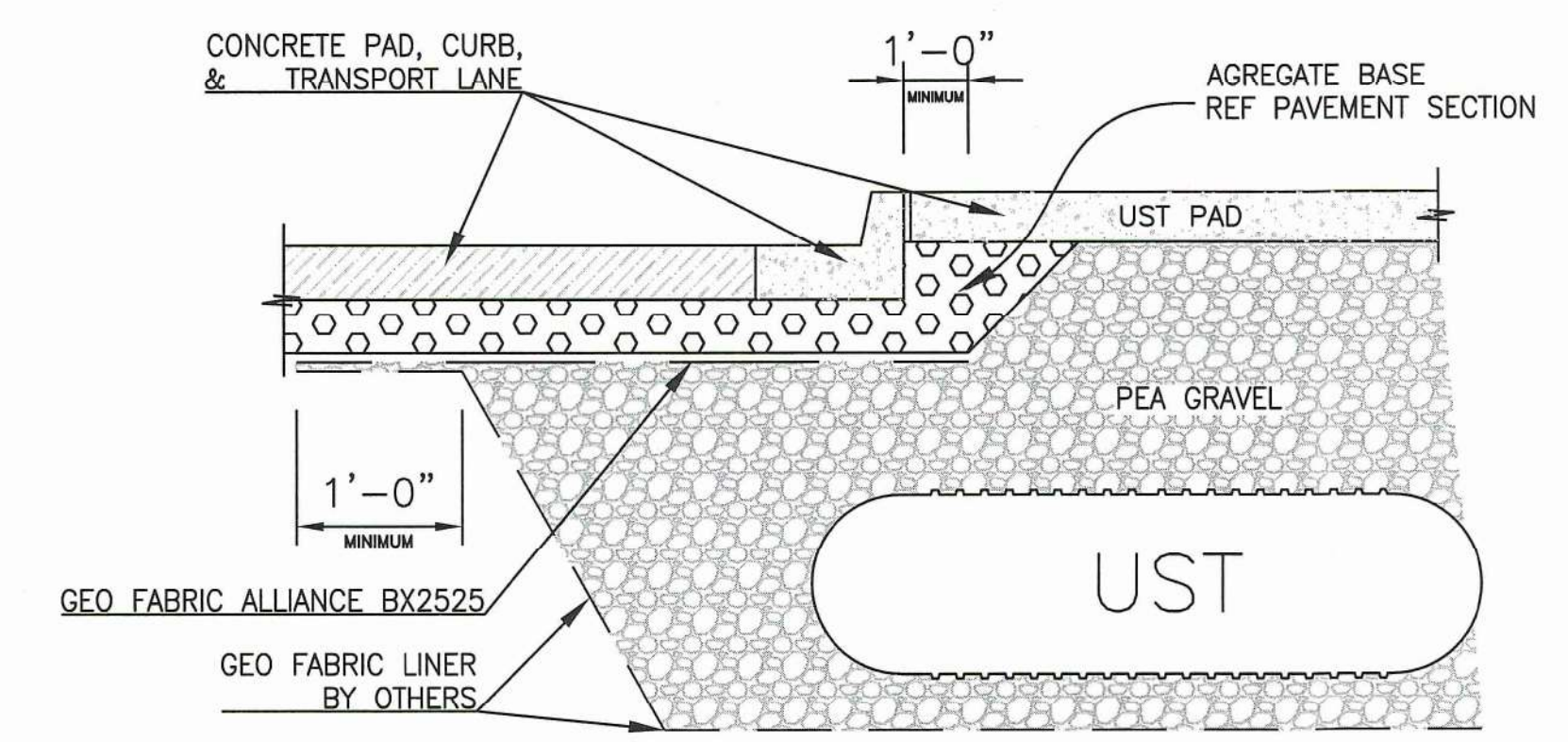
C9.2



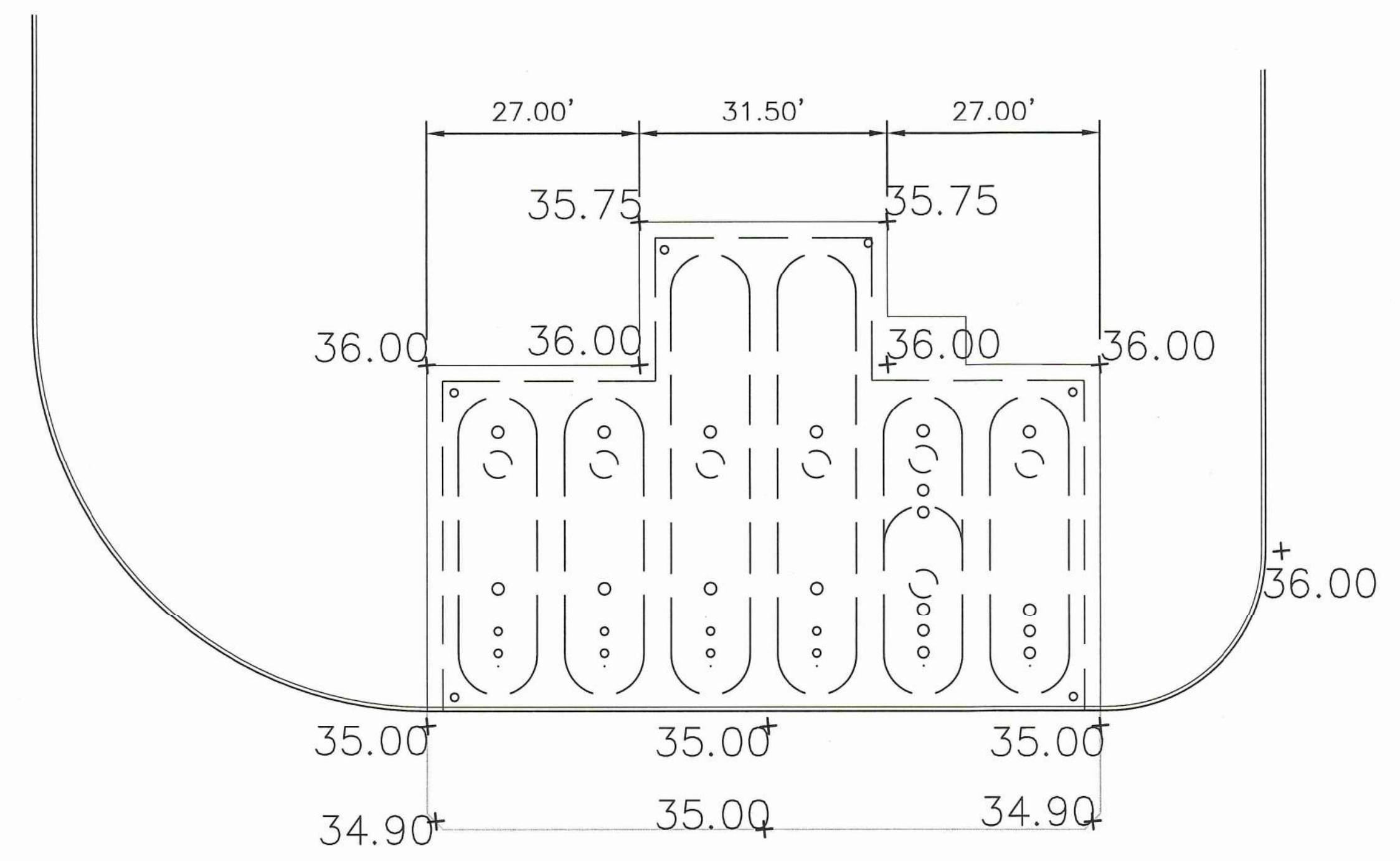
ANTI-SEEP COLLAR DETAIL
 NO SCALE



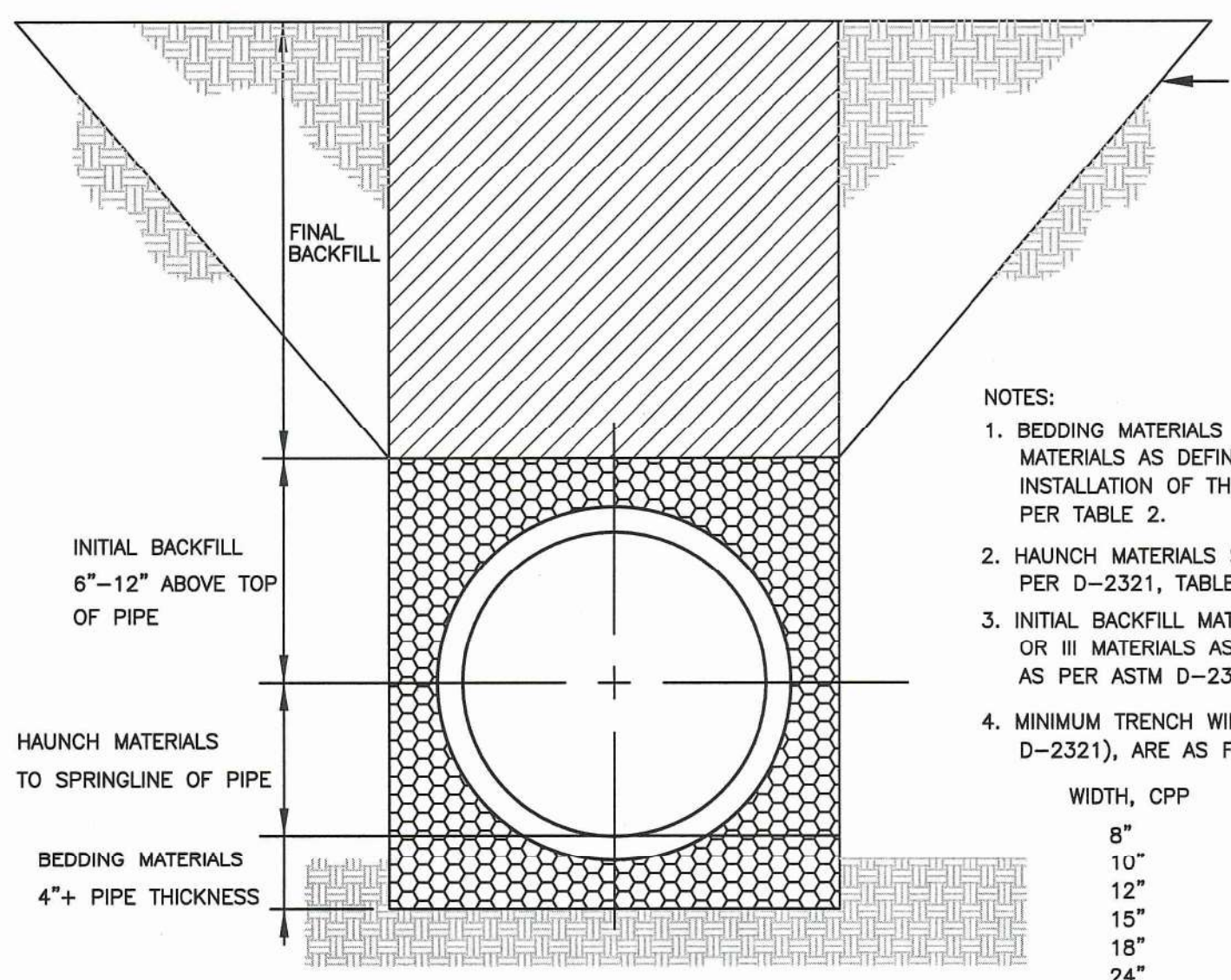
LINE SUBDRAIN TRENCH WITH
 MIRAFI 140N DRAINAGE FABRIC OR EQUAL
FRENCH DRAIN DETAIL
 NO SCALE CG13



SECTION THRU PAVING OVER FUEL TANKS
 NO SCALE



UST UNLOADING PAD AREA DETAIL
 NO SCALE



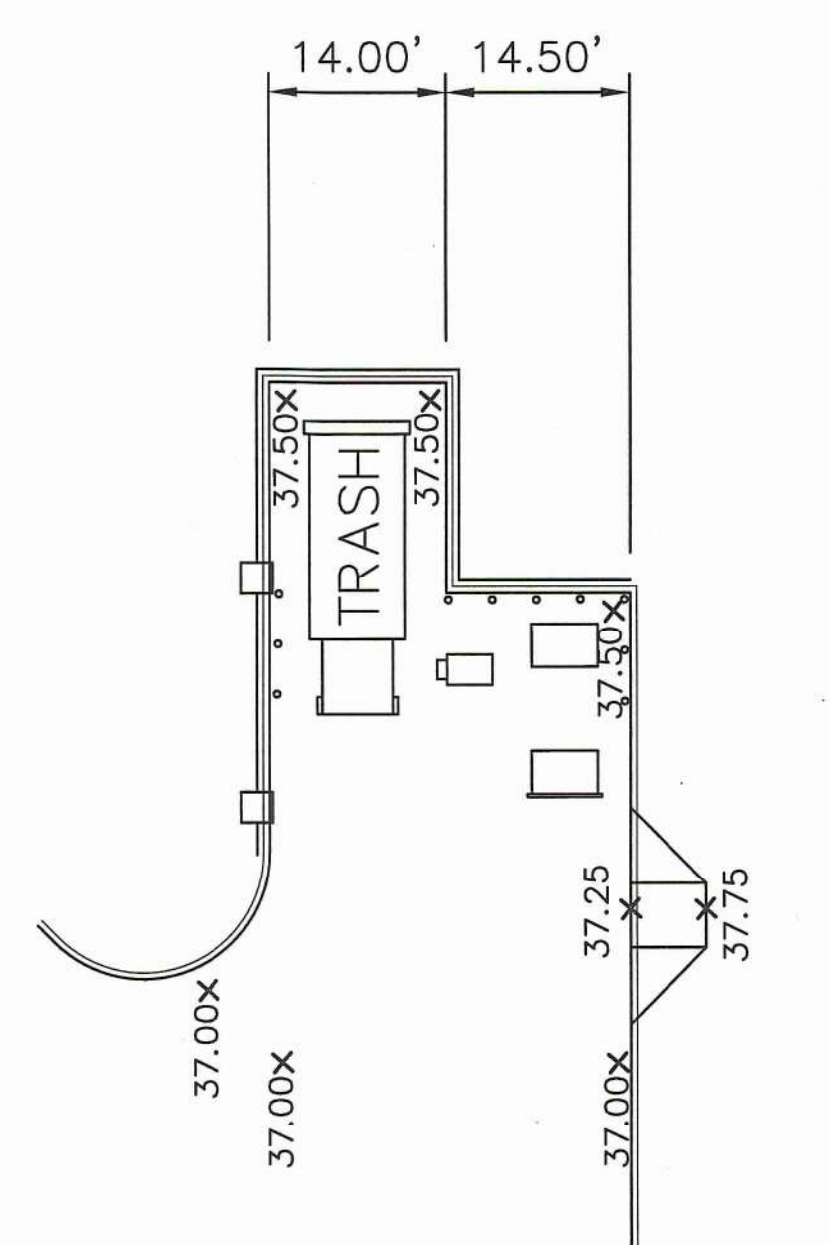
STORM DRAIN BEDDING
 NO SCALE (FOR CPP ONLY) CG25

- NOTES:
1. BEDDING MATERIALS SHALL BE CLASS I, II, OR III MATERIALS AS DEFINED IN TABLE 1, ASTM D-2321. INSTALLATION OF THESE MATERIALS SHALL BE AS PER TABLE 2.
 2. HAUNCH MATERIALS SHALL BE CLASS I, II, OR III PER D-2321, TABLE 2.
 3. INITIAL BACKFILL MATERIALS SHALL BE CLASS I, II, OR III MATERIALS AS REQUIRED ABOVE, RESTRICTED AS PER ASTM D-2321, TABLE 2.
 4. MINIMUM TRENCH WIDTHS (AS REQUIRED BY ASTM D-2321), ARE AS FOLLOWS:

WIDTH, CPP	MIN. TRENCH
8"	25"
10"	27"
12"	30"
15"	34"
18"	39"
24"	48"
30"	66"
36"	78"
42"	83"
48"	89"

A LESSER TRENCH WIDTH WILL BE PERMITTED ONLY WHERE IT IS DETERMINED THE CONTRACTOR CAN UTILIZE EQUIPMENT CAPABLE OF MEETING THE REQUIRED INSTALLATION DENSITIES.

5. WHERE THE TRENCH BOTTOM IS UNSTABLE, CONTRACTOR SHALL EXCAVATE TO A DEPTH AS REQUIRED BY THE ENGINEER AND REPLACE AS AN ALTERNATE, TRENCH BOTTOM MAY BE STABILIZED USING A GEOFABRIC.



COMPACTOR AREA DETAIL
 NO SCALE



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GRADING & DRAINAGE
 DETAILS

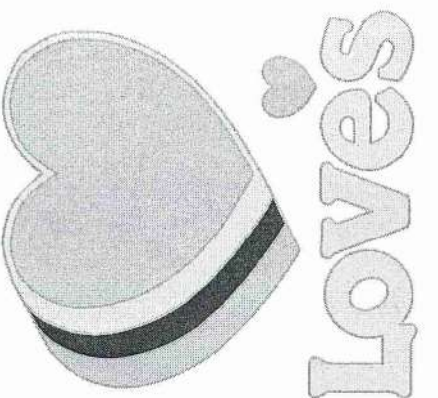




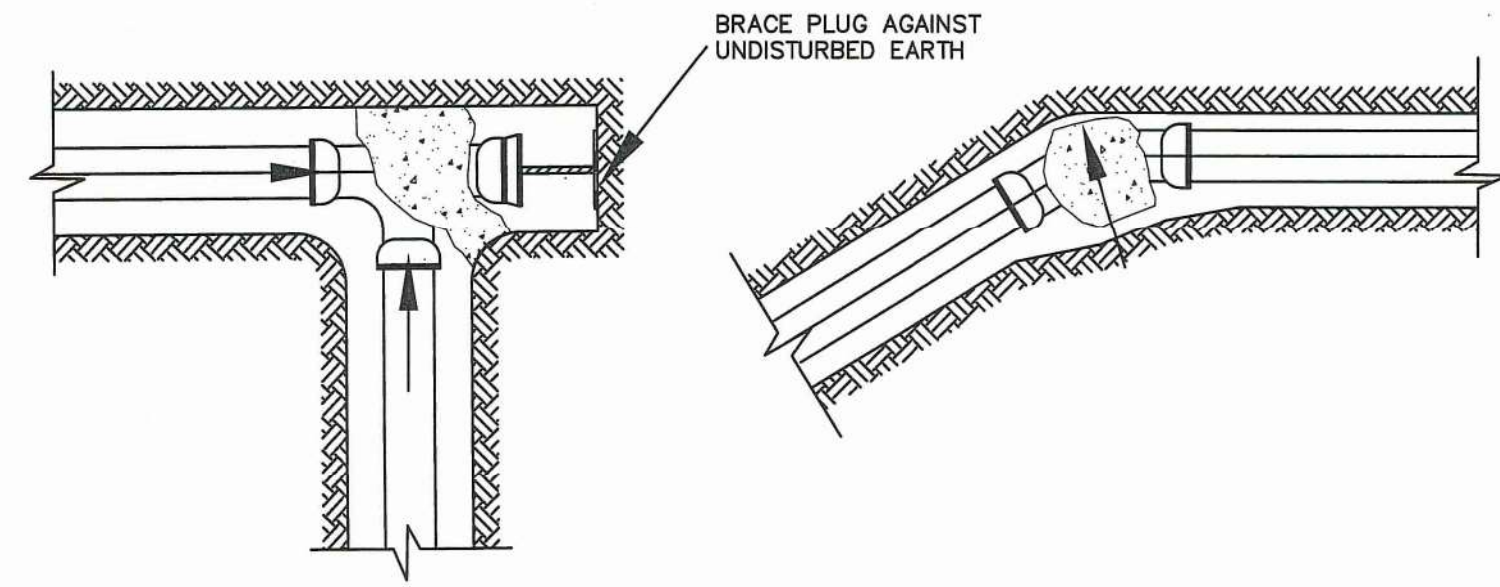
RESOURCECONSULTING
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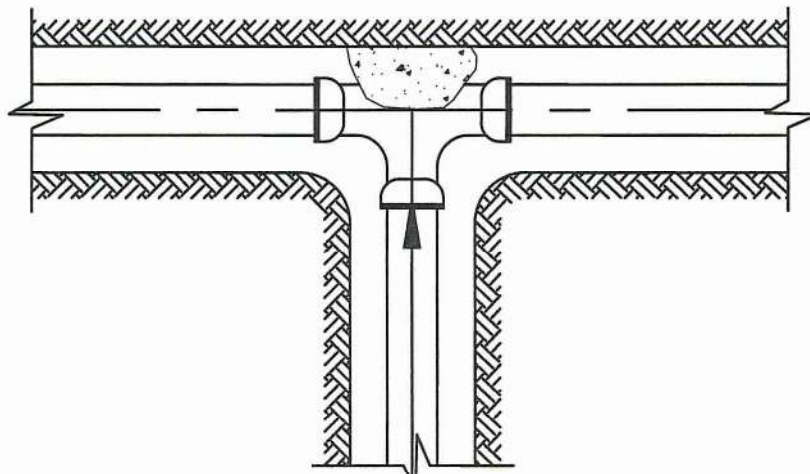


C9.4

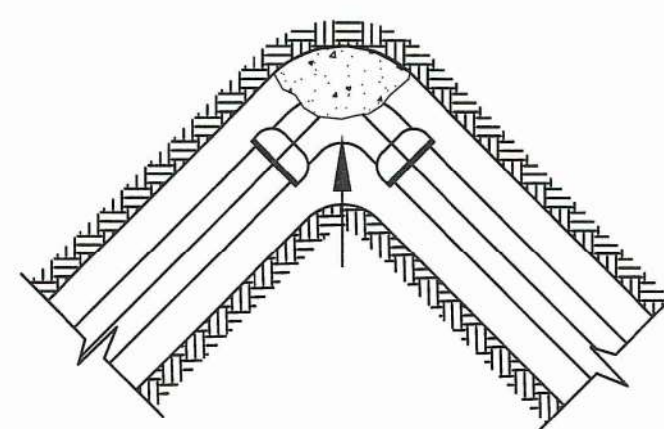


PLUGGED TEE
NTS

VERTICAL BEND
NTS



TEE
NTS



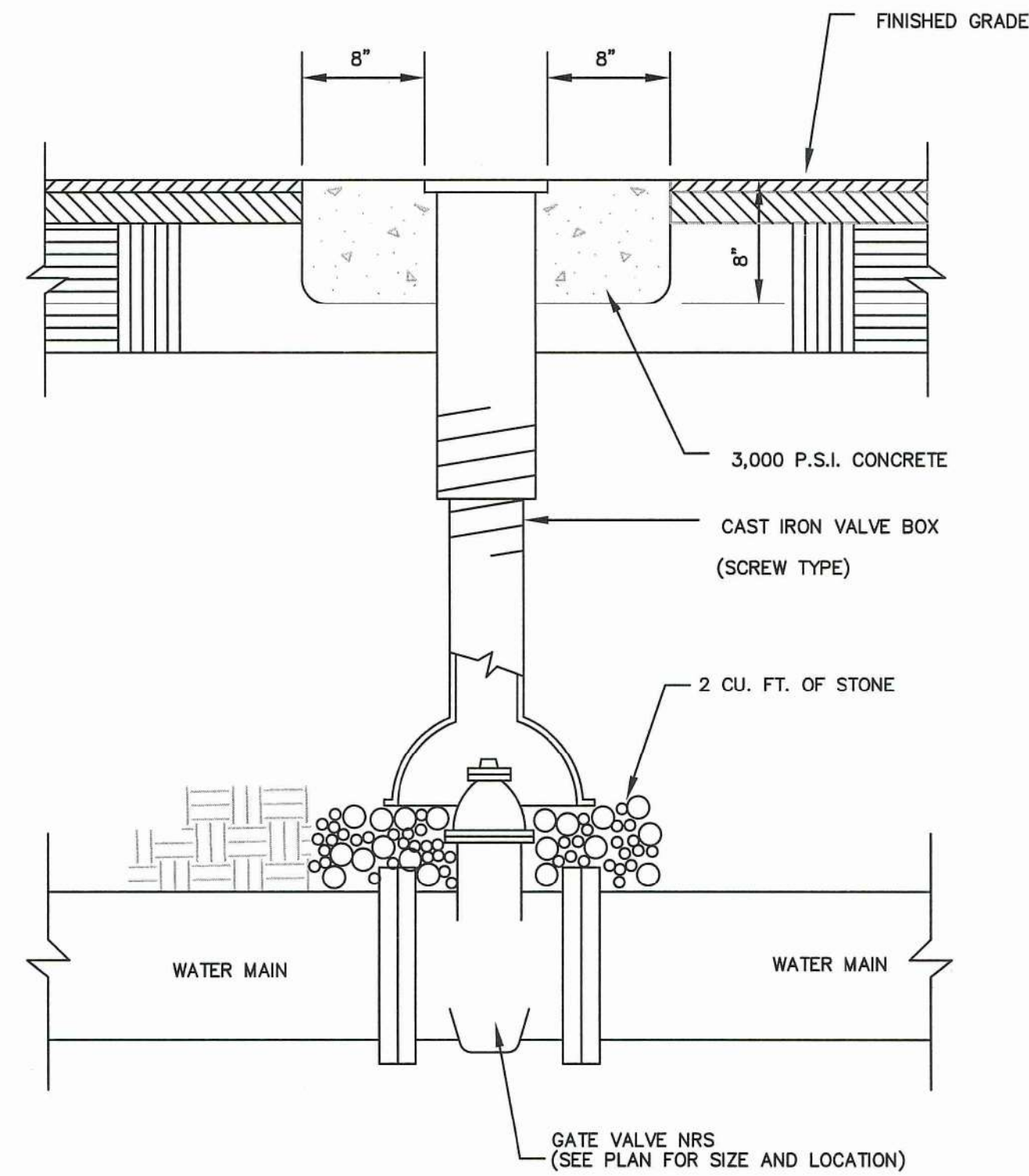
90° ELBOW
NTS

1. ALL BLOCKS MUST BEAR AGAINST UNDISTURBED EARTH.
2. ARROWS INDICATE DIRECTION OF THRUST.
3. ALL FITTINGS SHOWN IN PLAN EXCEPT VERTICAL BEND.
4. CONCRETE COMPRESSIVE STRENGTH SHALL BE MINIMUM 3000 PSI AT 28 DAYS.

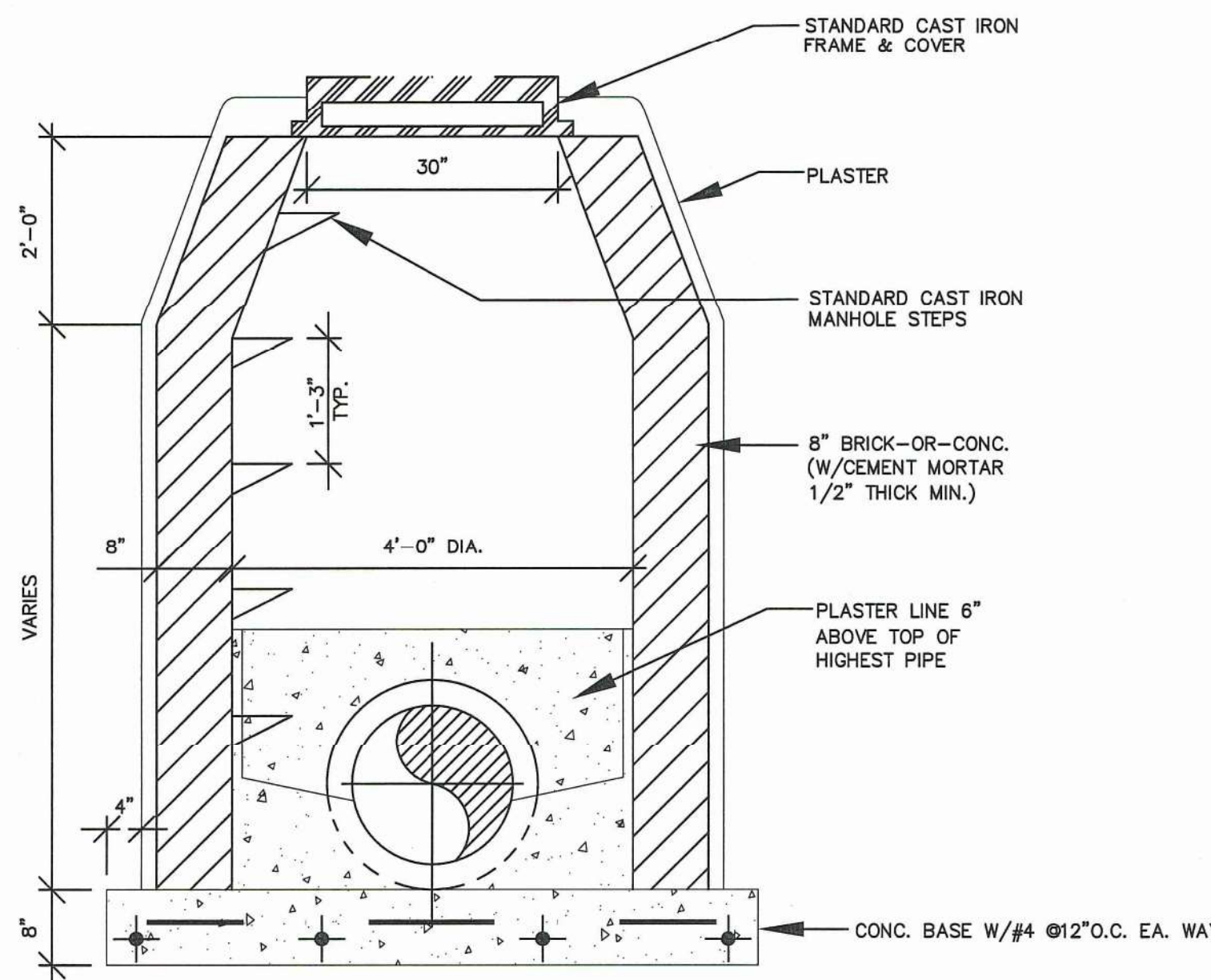
PROVIDE MINIMUM BEARING AREA IN S.F. AS FOLLOWS
BASED ON 150 PSI TEST PRESSURE AND 2000 PSF SCH. BEARING (UNLESS OTHERWISE DETAILED)

PIPE SIZE	TEE DEADENDS	11 1/4" BEND	22 1/2" BEND	45° BEND	90° BEND
4"	1.0	0.5	0.5	0.8	1.3
6"	2.2	0.5	0.8	1.5	3.0
8"	3.8	0.8	1.5	2.9	5.3
10"	6.0	1.2	2.3	4.5	8.4
12"	8.5	1.7	3.3	6.5	12.1
14"	11.5	2.3	4.5	8.9	16.4
16"	15.2	3.0	5.8	11.5	21.4

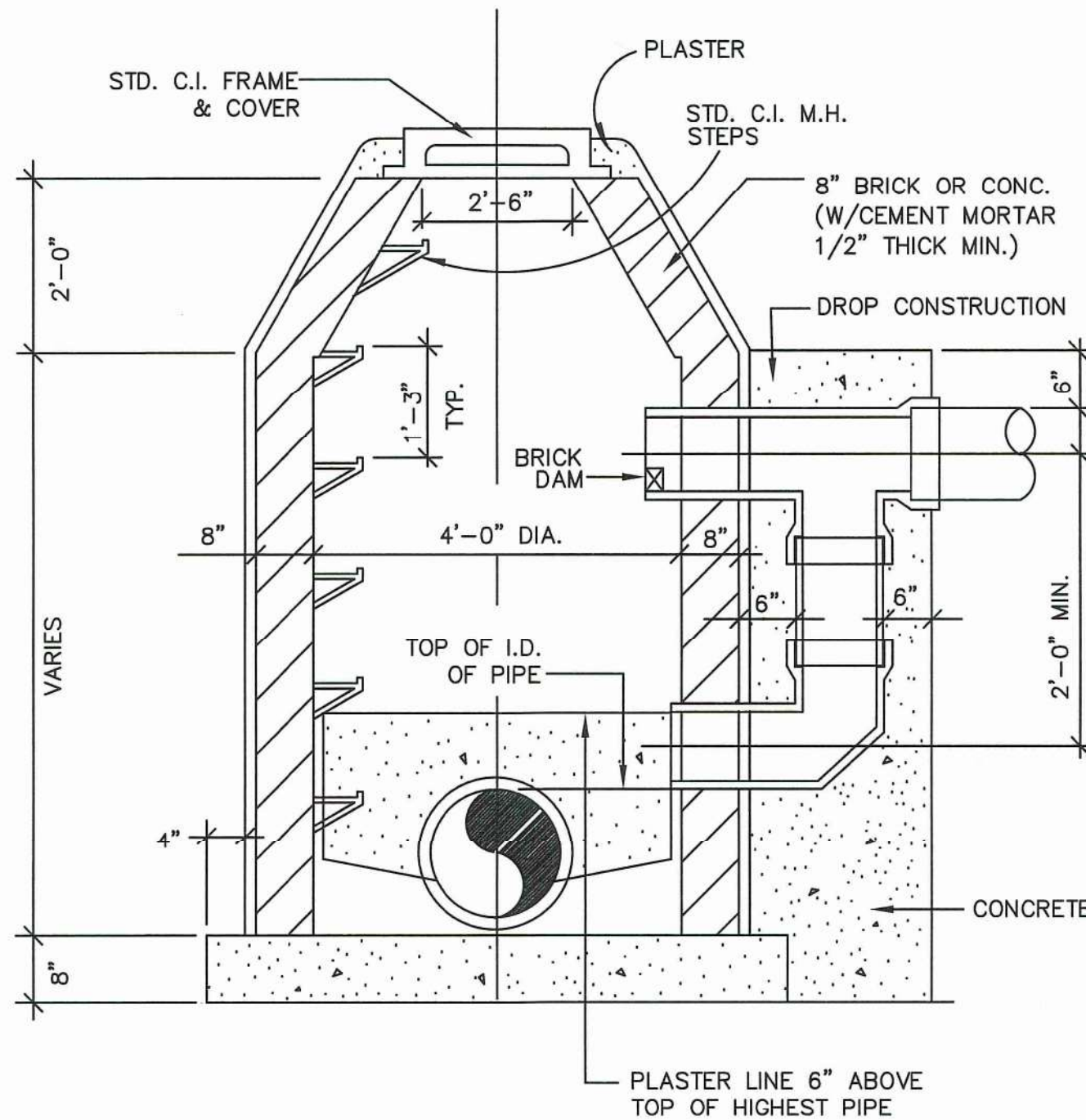
THRUST BLOCK DETAIL
NTS. CUI2



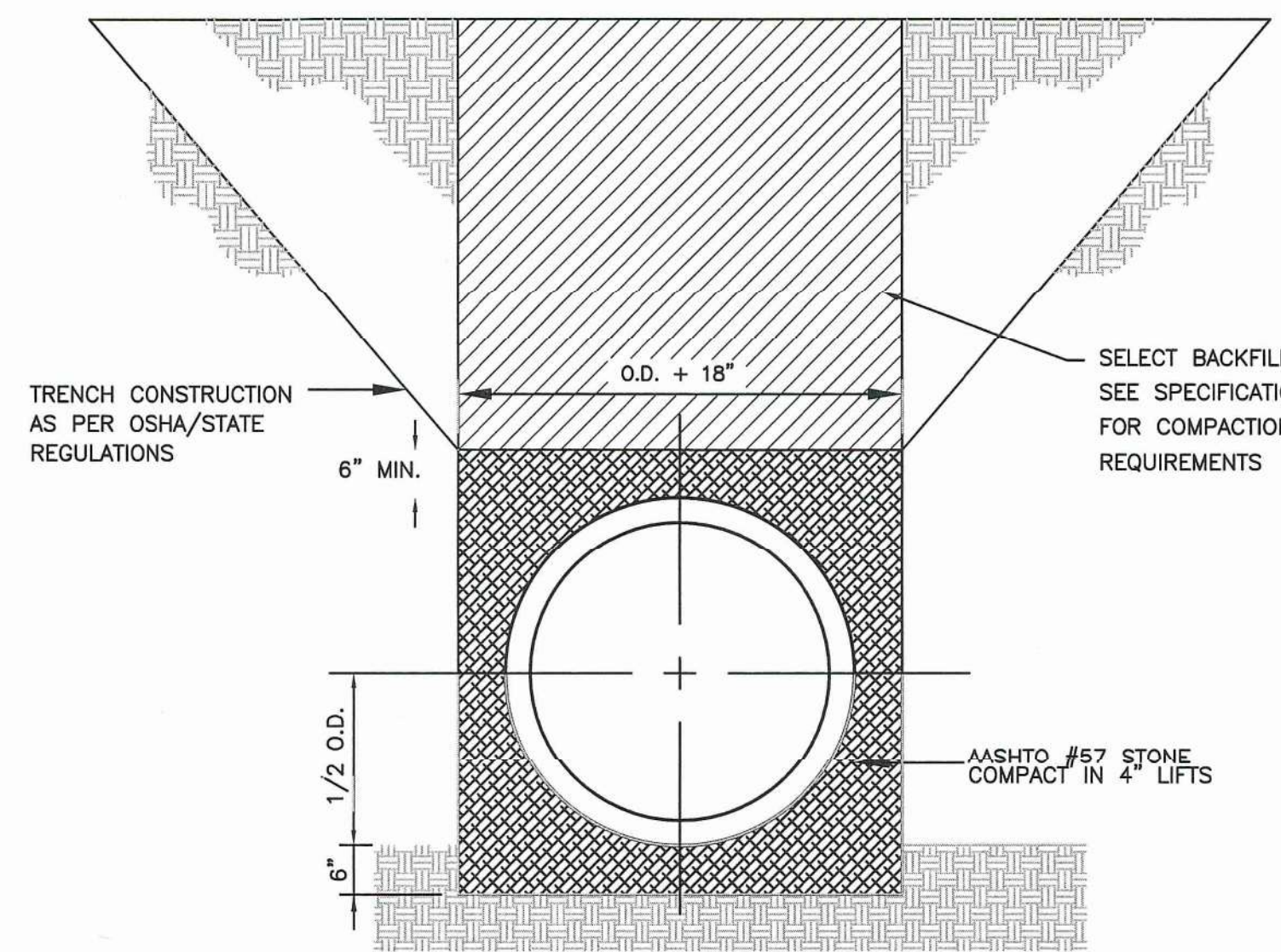
VALVE BOX SETTING
NTS. CUI4



MANHOLE / JUNCTION BOX DETAIL
NO SCALE. CUI7



SANITARY SEWER DROP MANHOLE
NO SCALE. CUI4



TRENCH & BEDDING DETAIL
NTS. (FOR WATER & SANITARY LINES ONLY). CUI00

- NOTES:
1. BEDDING MATERIALS SHALL BE CLASS I, II, OR III MATERIALS AS DEFINED IN TABLE 1, ASTM D-2321. INSTALLATION OF THESE MATERIALS SHALL BE AS PER TABLE 2.
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A LESSEY TRENCH WIDTH WILL BE PERMITTED ONLY WHERE IT IS DETERMINED THE CONTRACTOR CAN UTILIZE EQUIPMENT CAPABLE OF MEETING THE REQUIRED INSTALLATION DENSITIES.

5. WHERE THE TRENCH BOTTOM IS UNSTABLE, CONTRACTOR SHALL EXCAVATE TO A DEPTH AS REQUIRED BY THE ENGINEER AND REPLACE AS AN ALTERNATE, TRENCH BOTTOM MAY BE STABILIZED USING A GEOFABRIC.



3/1/23

THIS DRAWING IS NOT VALID WITHOUT AN ORIGINAL BLUE INK SIGNATURE AND ORIGINAL HANDWRITTEN DATE OF A LICENSED PROFESSIONAL ENGINEER.

UTILITY DETAILS

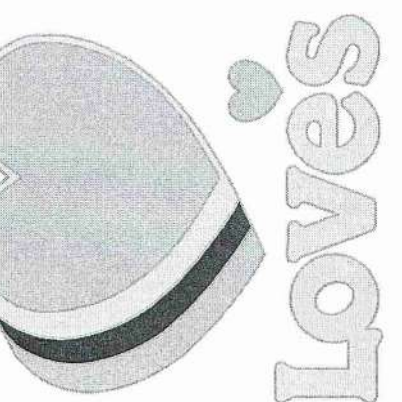




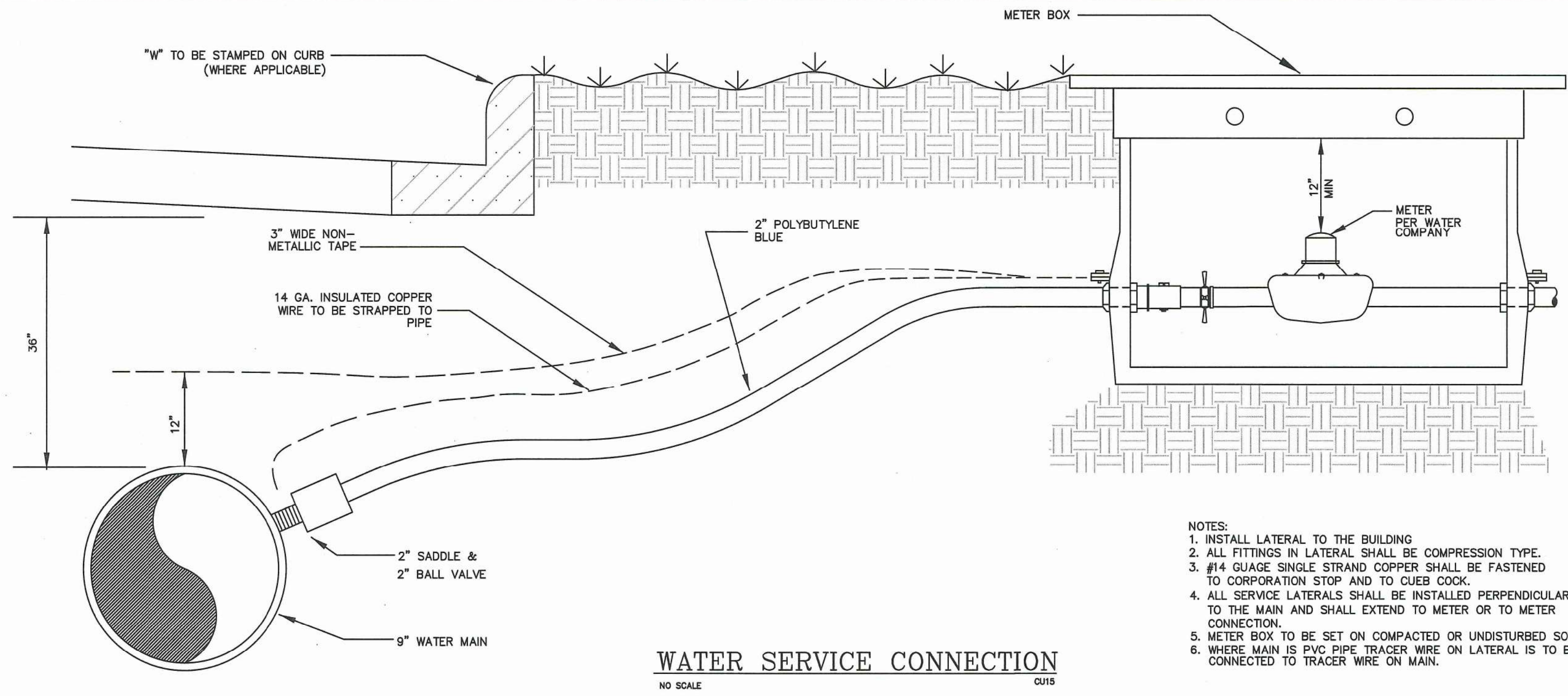
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LOVE'S TRAVEL STOP
INTERSTATE 59 EXIT 1 @ MS HIGHWAY 607
PEARL RIVER COUNTY, MISSISSIPPI

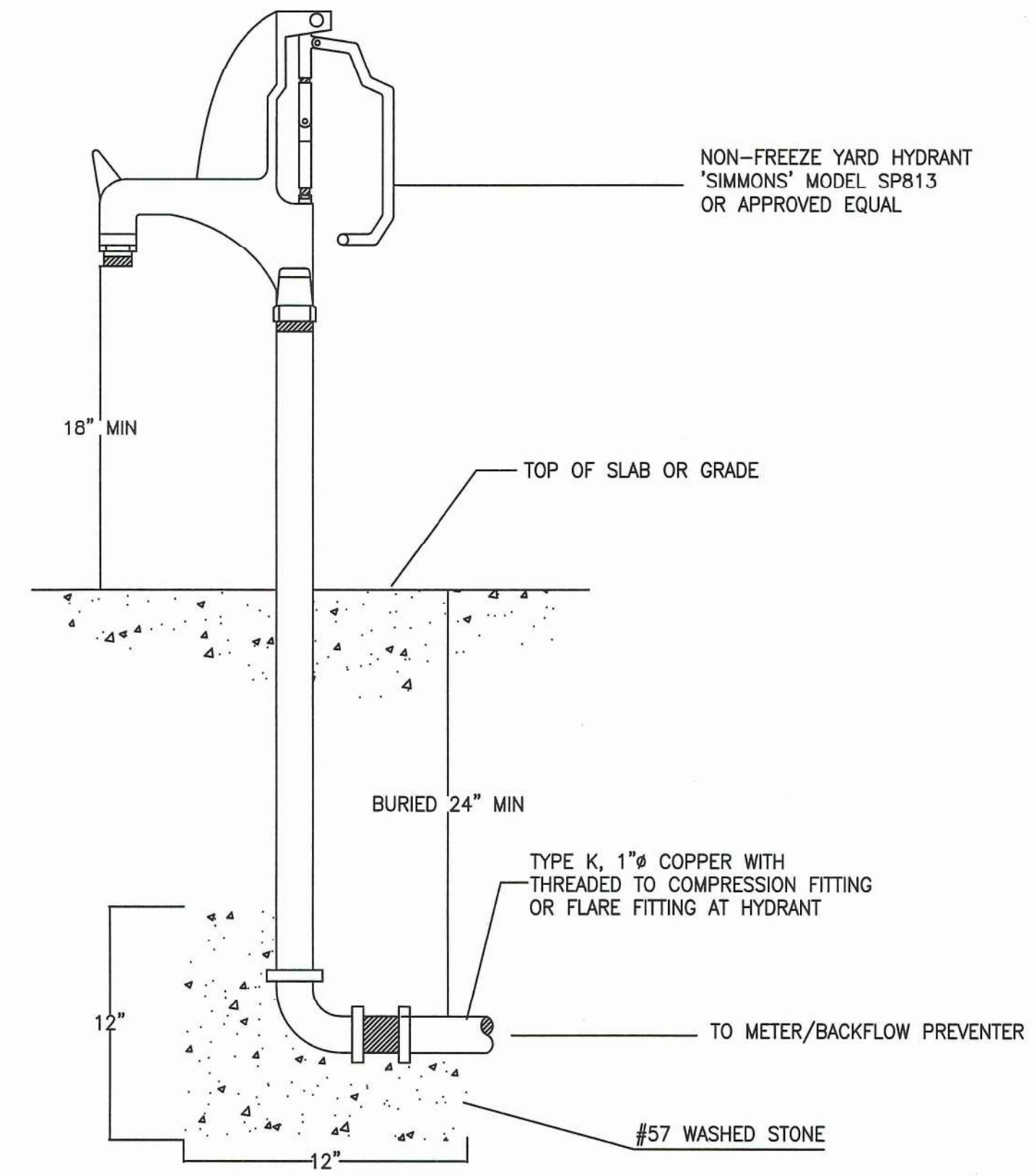


C9.5

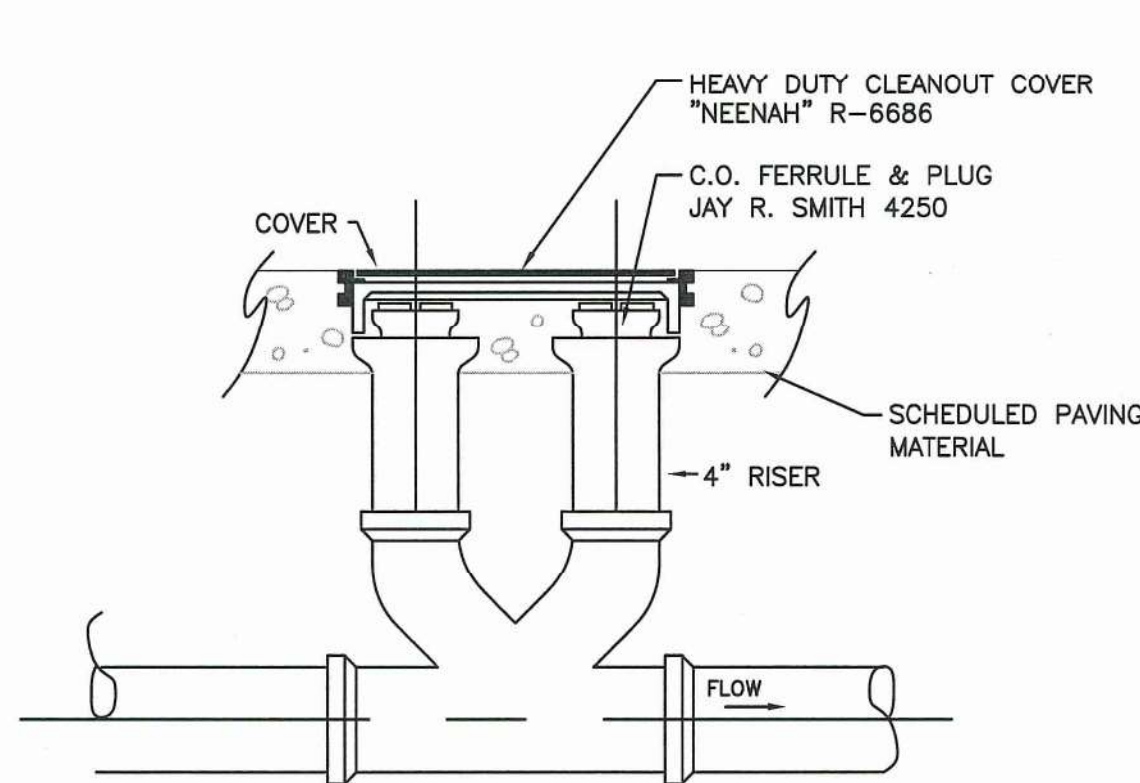


- NOTES:
1. INSTALL LATERAL TO THE BUILDING
 2. ALL FITTINGS IN LATERAL SHALL BE COMPRESSION TYPE.
 3. #14 GAUGE SINGLE STRAND COPPER SHALL BE FASTENED TO CORPORATION STOP AND TO CURB COCK.
 4. ALL SERVICE LATERALS SHALL BE INSTALLED PERPENDICULAR TO THE MAIN AND SHALL EXTEND TO METER OR TO METER CONNECTION.
 5. METER BOX TO BE SET ON COMPACTED OR UNDISTURBED SOIL.
 6. WHERE MAIN IS PVC PIPE TRACER WIRE ON LATERAL IS TO BE CONNECTED TO TRACER WIRE ON MAIN.

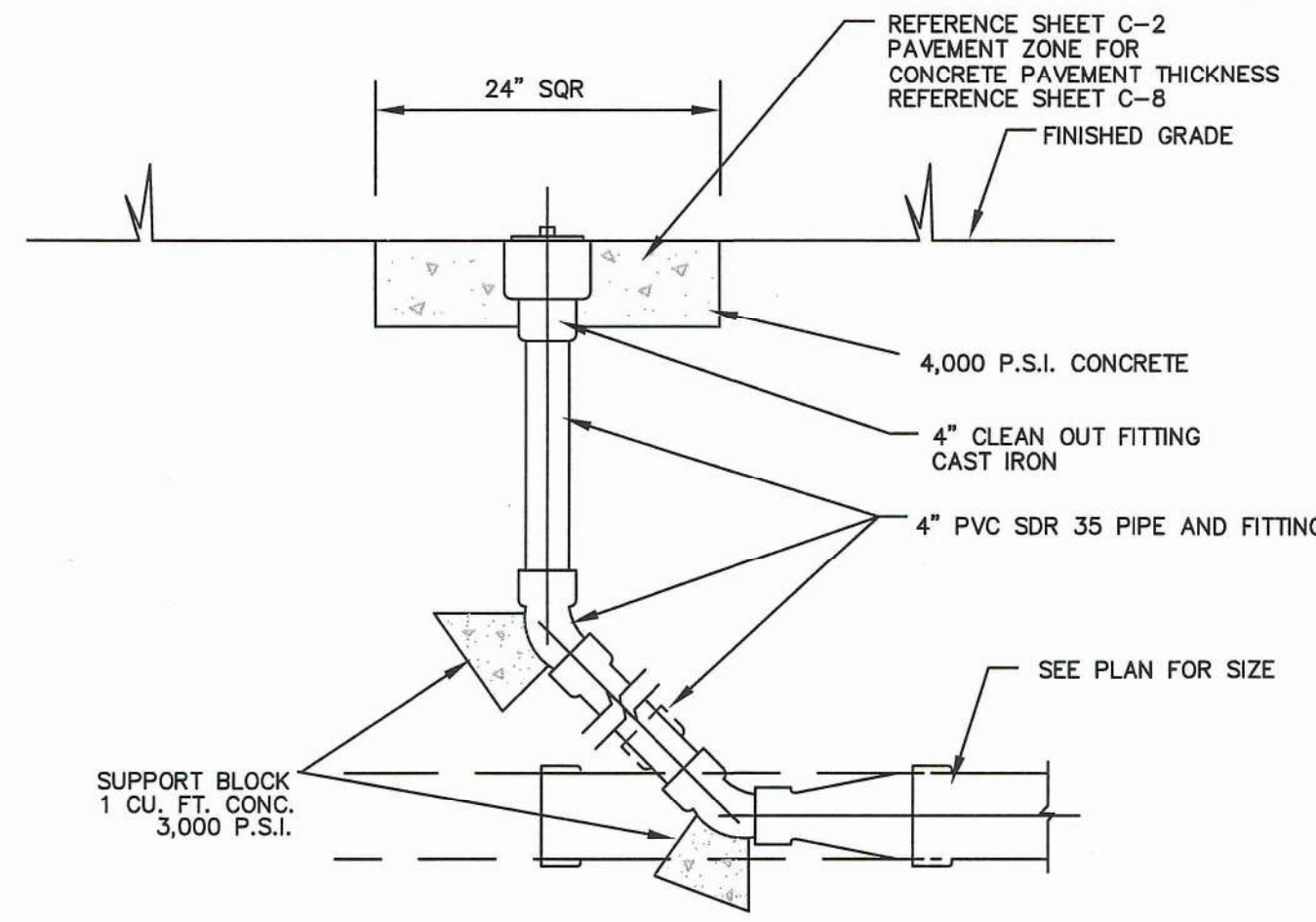
WATER SERVICE CONNECTION
NO SCALE
CU15



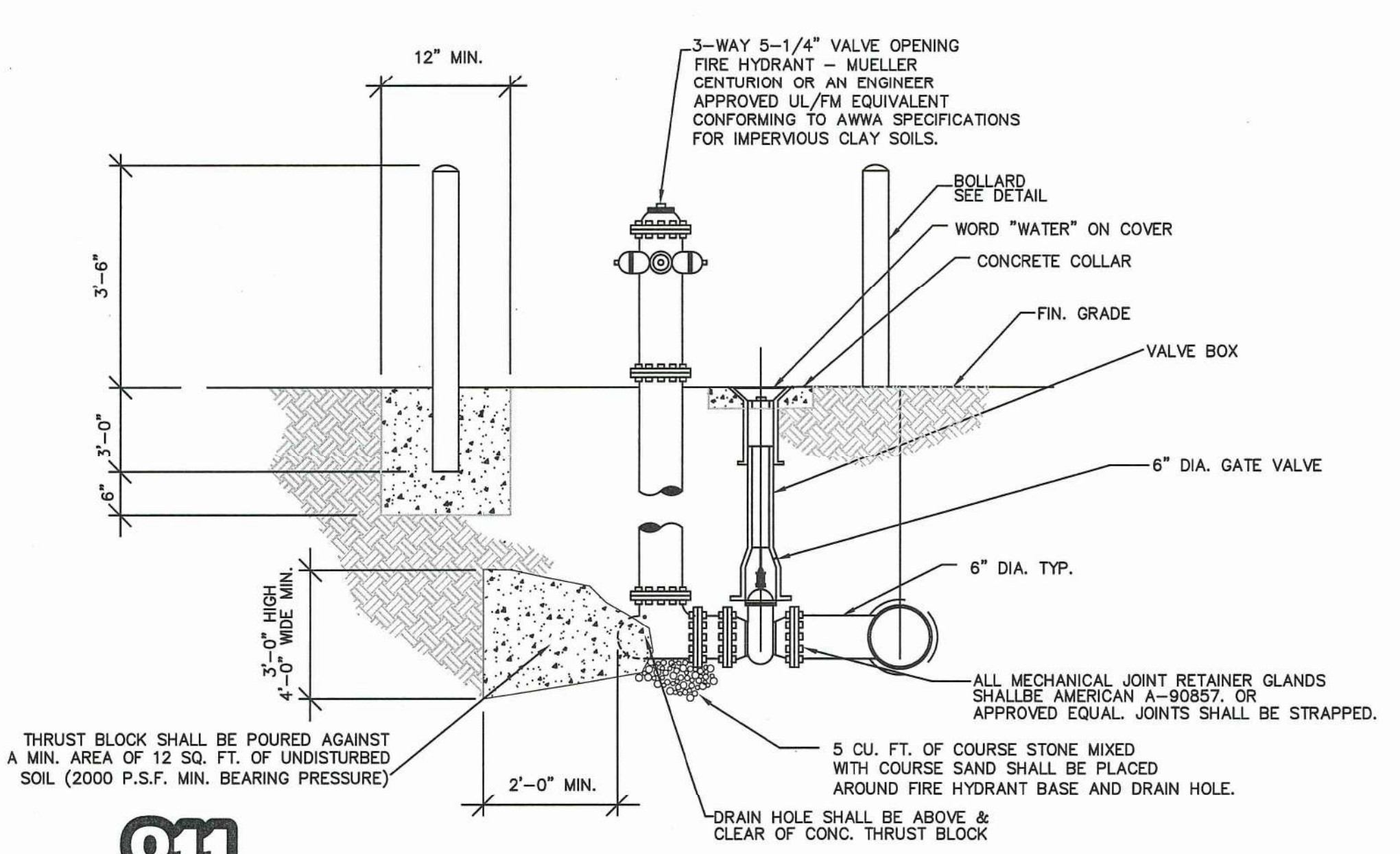
YARD HYDRANT DETAIL
Not To Scale



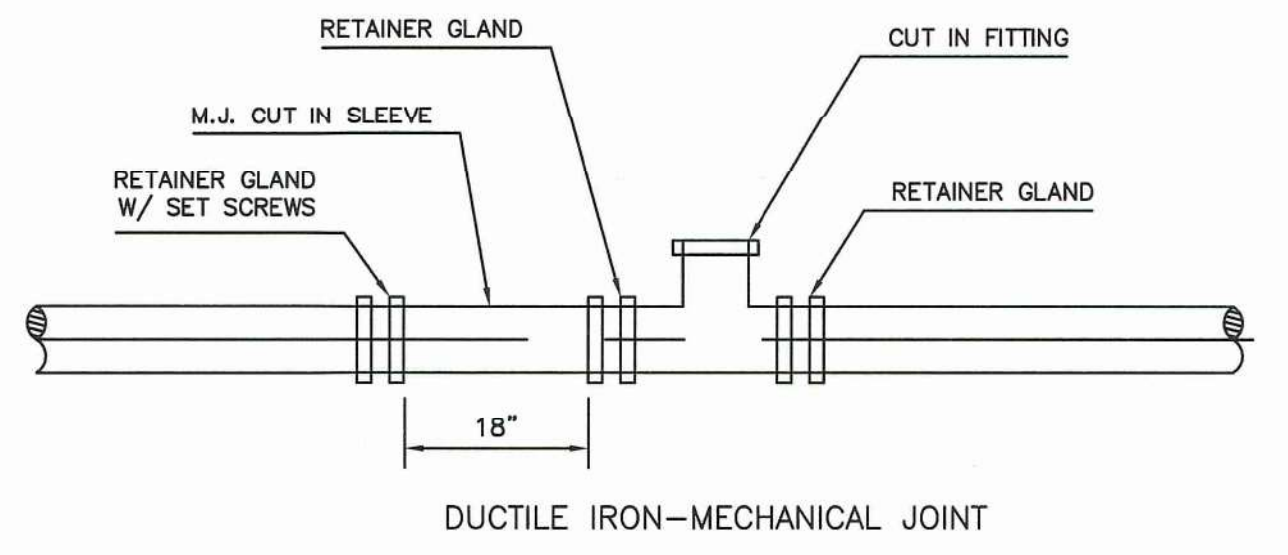
2-WAY CLEAN OUT DETAIL
N.T.S.



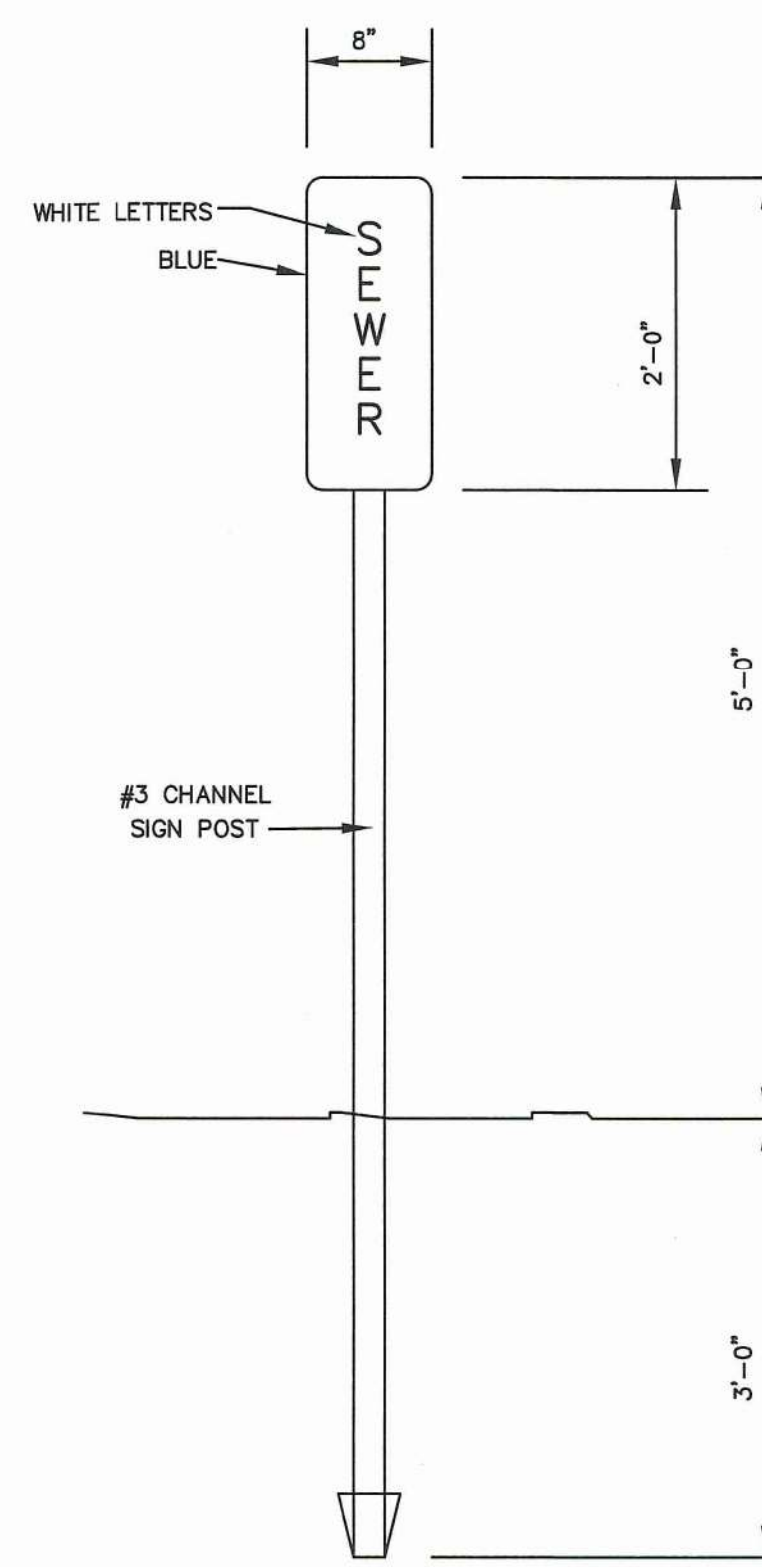
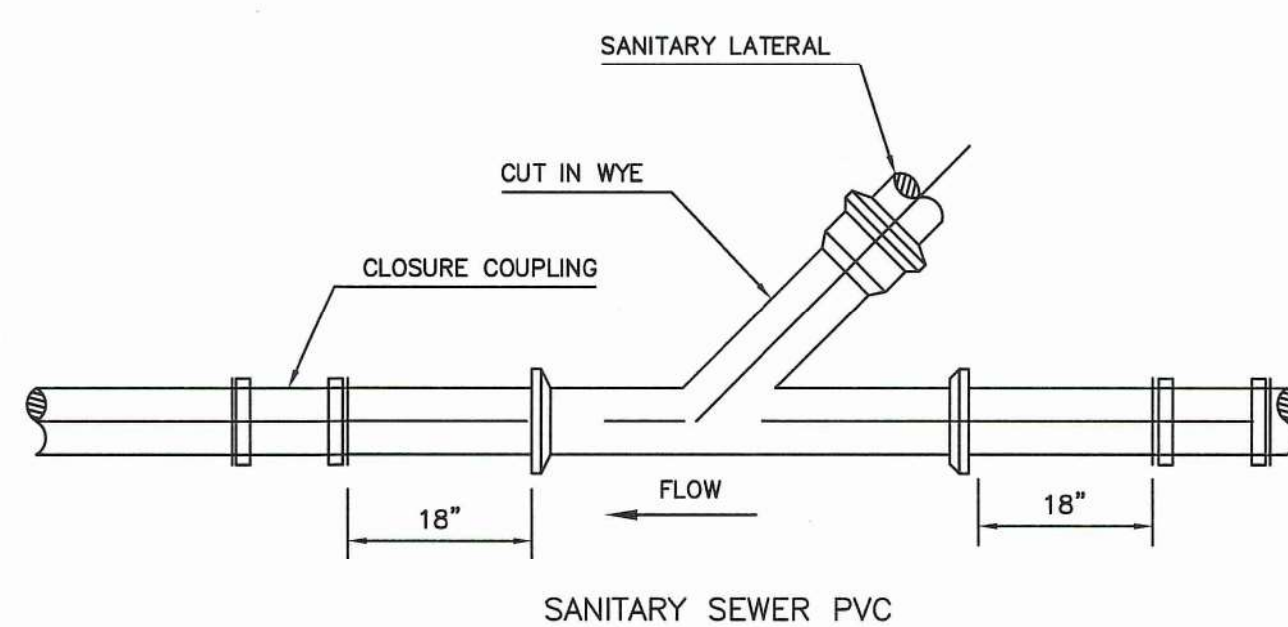
CLEAN OUT DETAIL
N.T.S.
CU01



FIRE HYDRANT DETAIL
NO SCALE
CU08

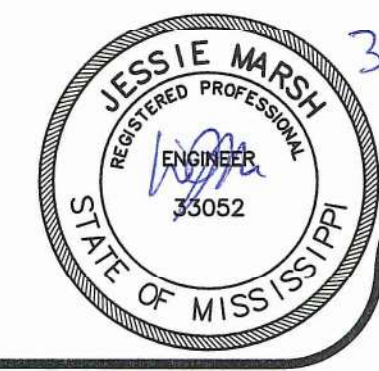


SANITARY SEWER CUT IN DETAIL
NO SCALE
165



DEAD-END UTILITY SIGN DETAIL
N.T.S.

NOTE:
NUTS, BOLTS, AND METAL WASHERS SHALL BE GALVANIZED OR CADMIUM PLATED.
THIS SIGN SHALL BE USED AT ALL OTHER UTILITY DEAD-ENDS BY REPLACING THE WORD "WATER" WITH THE PROPER UTILITY IDENTIFICATION.



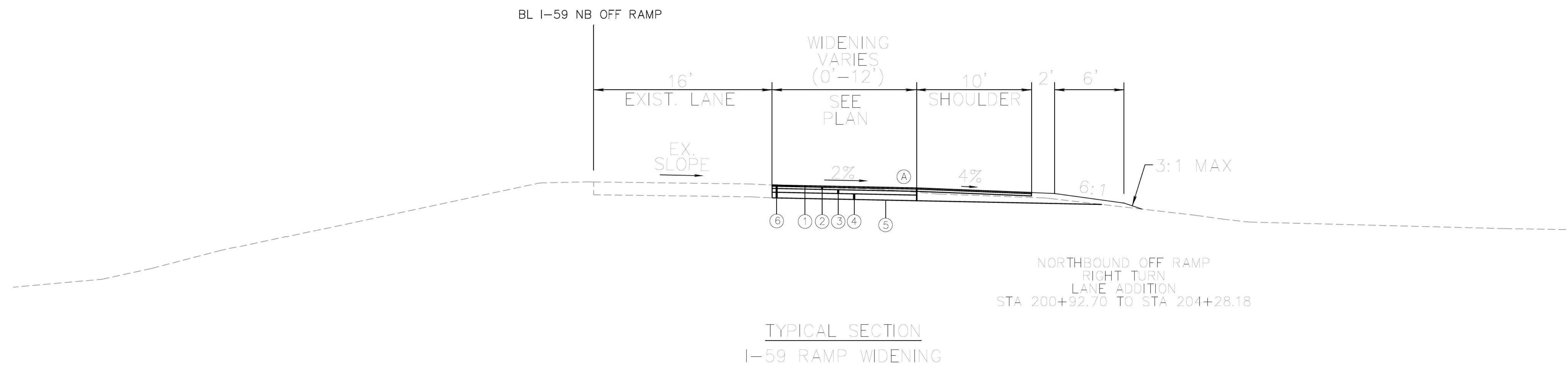
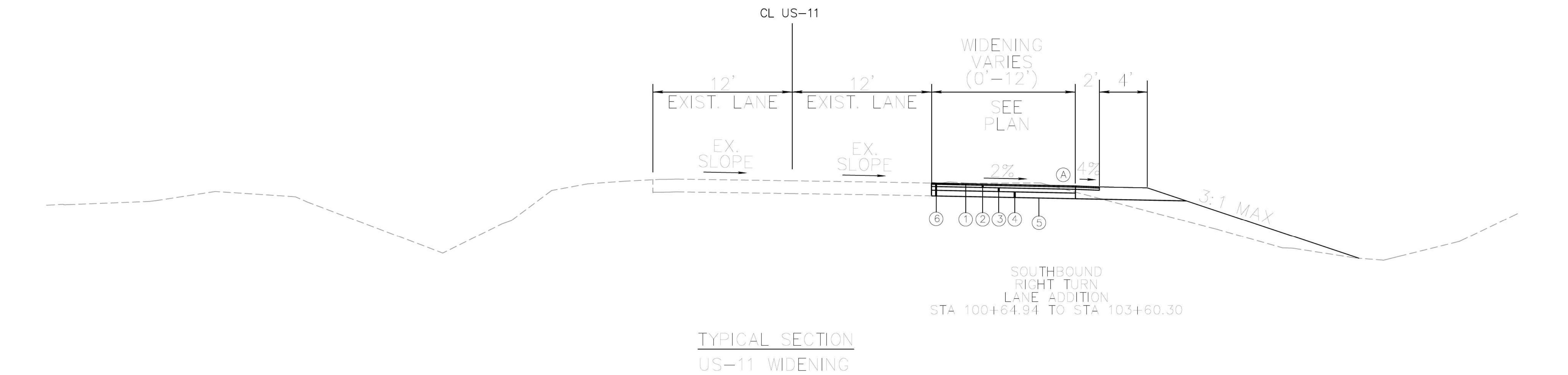
THIS DRAWING IS NOT VALID WITHOUT AN ORIGINAL BLUE INK SIGNATURE AND ORIGINAL HANDWRITTEN DATE OF A LICENSED PROFESSIONAL ENGINEER.

UTILITY DETAILS



Know what's below.
Call before you dig.

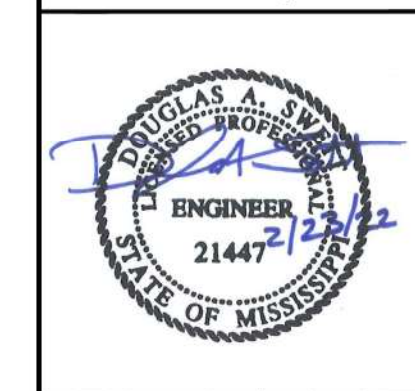
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- ① 1.5" ASPHALT PAVEMENT, HT (9.5 MM MIX) (1 @ 1.5")
- ② 2.5" ASPHALT PAVEMENT, HT (12.5 MM MIX) (1 @ 2.5")
- ③ 5.0" ASPHALT PAVEMENT, HT (19 MM MIX) (2 @ 2.5")
- ④ 8.0" CRUSHED STONE
- ⑤ GEOTEXTILE FABRIC, TYPE V (NON-WOVEN) (UNDER CRUSHED STONE)
- ⑥ SHOULDER PAVEMENT REMOVAL
- Ⓐ EXISTING PAVEMENT TO BE SAW-CUT 1' MIN FROM EDGE OF EXISTING TRAVEL WAY AND TRIMMED TO STRAIGHT VERTICAL LINE. ANY LOOSE OR DISTURBED PAVEMENT MUST BE REMOVED AND REPLACED.

No.	REVISIONS	DATE	BY

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KHA PROJECT	142362033
DATE	2/2022
SCALE	AS SHOWN
DESIGNED BY	BST
DRAWN BY	BST
CHECKED BY	CBB

TYPICAL SECTION

LOVE'S TRAVEL STOP
 TRAFFIC SIGNAL DESIGN
 PREPARED FOR
MDOT

PICAYUNE MS

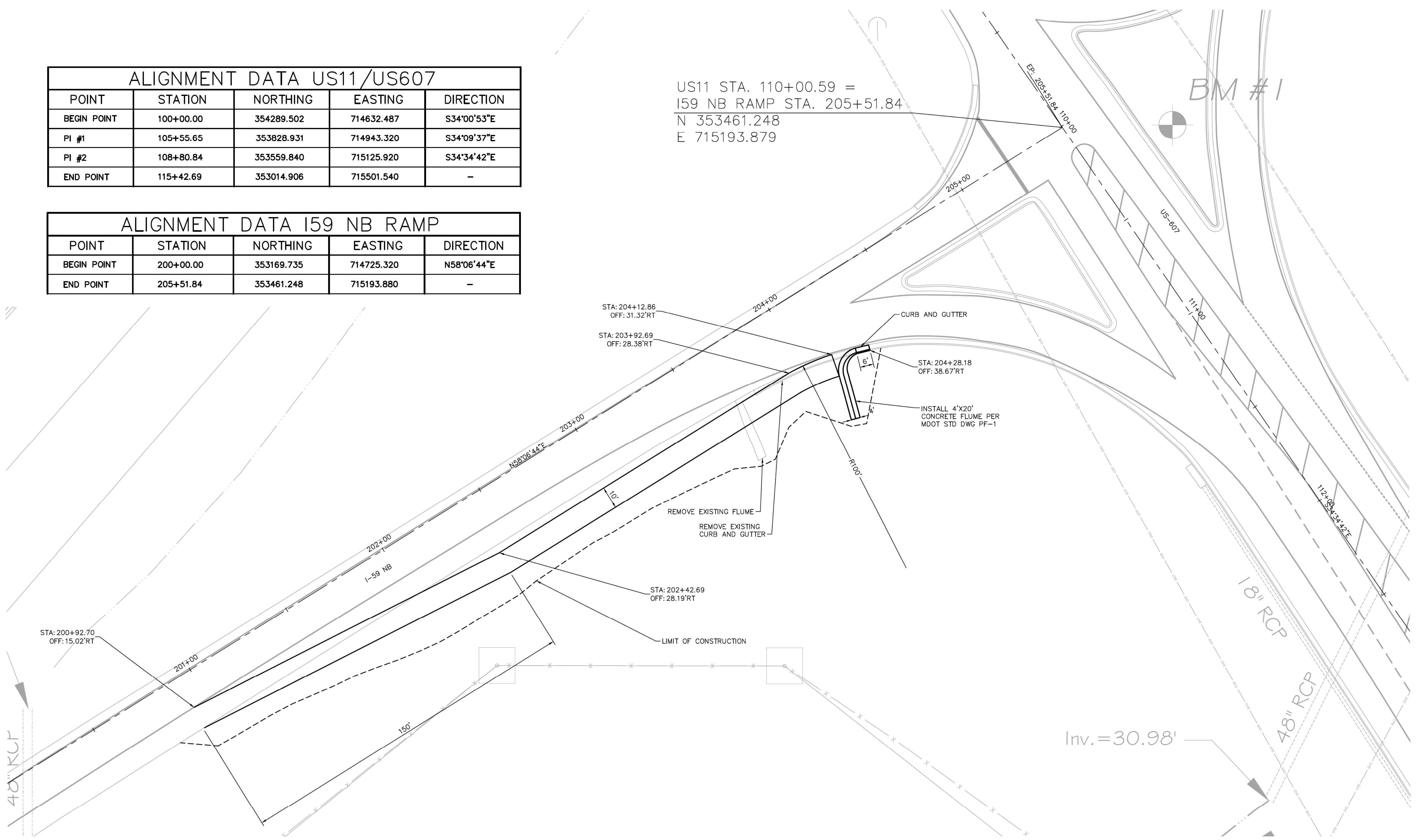
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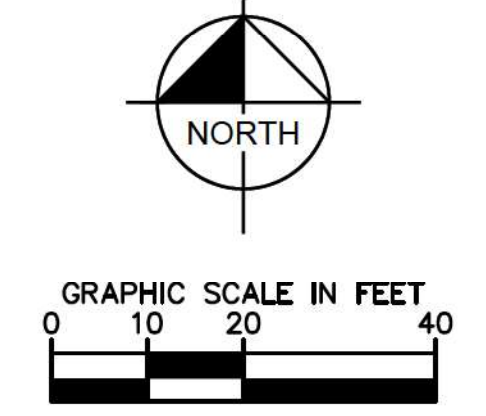
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PI #1	105+55.65	353828.931	714943.320	S34°09'37"E
PI #2	108+80.84	353559.840	715125.920	S34°34'42"E
END POINT	115+42.69	353014.906	715501.540	-

ALIGNMENT DATA I59 NB RAMP				
POINT	STATION	NORTHING	EASTING	DIRECTION
BEGIN POINT	200+00.00	353169.735	714725.320	N58°06'44"E
END POINT	205+51.84	353461.248	715193.880	-

US11 STA. 110+00.59 =
 I59 NB RAMP STA. 205+51.84
 N 353461.248
 E 715193.879



NOTE:
 1. CONTRACTOR TO COORDINATE WITH THE MISSISSIPPI DEPARTMENT OF TRANSPORTATION (MDOT) FOR ANY UTILITY RELOCATIONS WITHIN MDOT RIGHT-OF-WAY OR EASEMENTS. ALL PERMITTING WILL BE THE RESPONSIBILITY OF THE CONTRACTOR

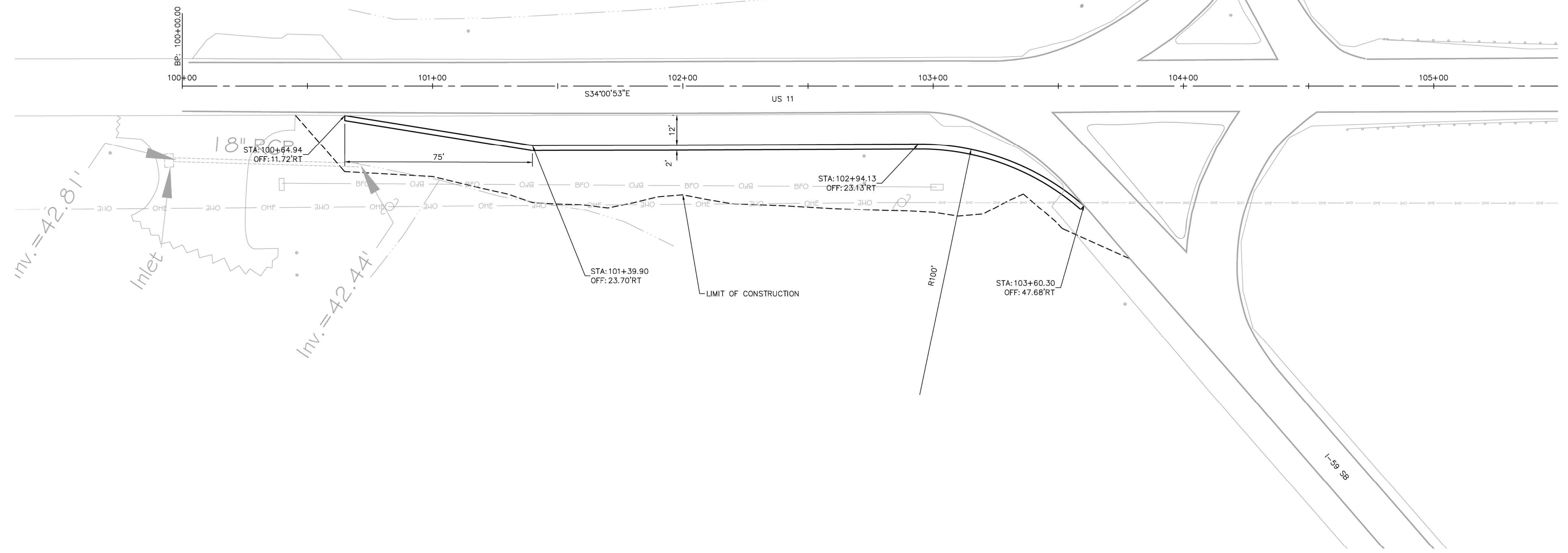


<p style="font-size: small; margin: 0;"> © 2010 KIMLEY-HORN AND ASSOCIATES, INC. 420 N 20TH ST, SHIPT TOWER STE 2200, BIRMINGHAM, AL 35203 PHONE: 205-558-5831 WWW.KIMLEY-HORN.COM </p>	
KHA PROJECT: 142362033 DATE: 2/2022 SCALE: AS SHOWN DESIGNED BY: BST DRAWN BY: BST CHECKED BY: CBB	ROADWAY LAYOUT I-59 NB
LOVE'S TRAVEL STOP TRAFFIC SIGNAL DESIGN PREPARED FOR MDOT	MS P/CAYUNE
SHEET NUMBER C3	REVISIONS No. DATE BY

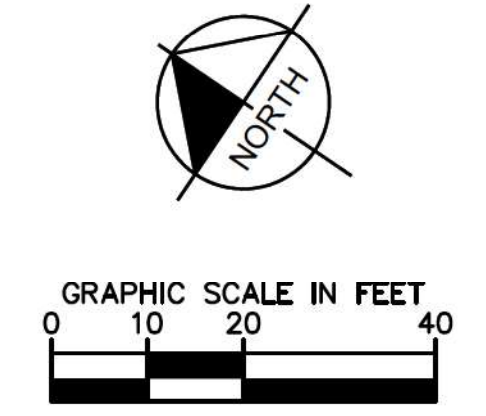
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ALIGNMENT DATA US11/US607				
POINT	STATION	NORTHING	EASTING	DIRECTION
BEGIN POINT	100+00.00	354289.502	714632.487	S34°00'53"E
PI #1	105+55.65	353828.931	714943.320	S34°09'37"E
PI #2	108+80.84	353559.840	715125.920	S34°34'42"E
END POINT	115+42.69	353014.906	715501.540	-

ALIGNMENT DATA I59 NB RAMP				
POINT	STATION	NORTHING	EASTING	DIRECTION
BEGIN POINT	200+00.00	353169.735	714725.320	N58°06'44"E
END POINT	205+51.84	353461.248	715193.880	-



NOTE:
 1. CONTRACTOR TO COORDINATE WITH THE MISSISSIPPI DEPARTMENT OF TRANSPORTATION (MDOT) FOR ANY UTILITY RELOCATIONS WITHIN MDOT RIGHT-OF-WAY OR EASEMENTS. ALL PERMITTING WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.



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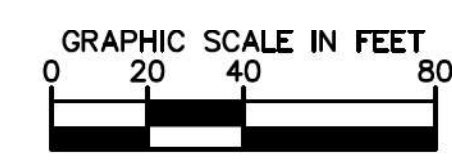
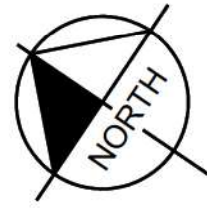
KHA PROJECT	142362033
DATE	2/2022
SCALE	AS SHOWN
DESIGNED BY	BST
DRAWN BY	BST
CHECKED BY	CBB

ROADWAY LAYOUT
I-59 SB

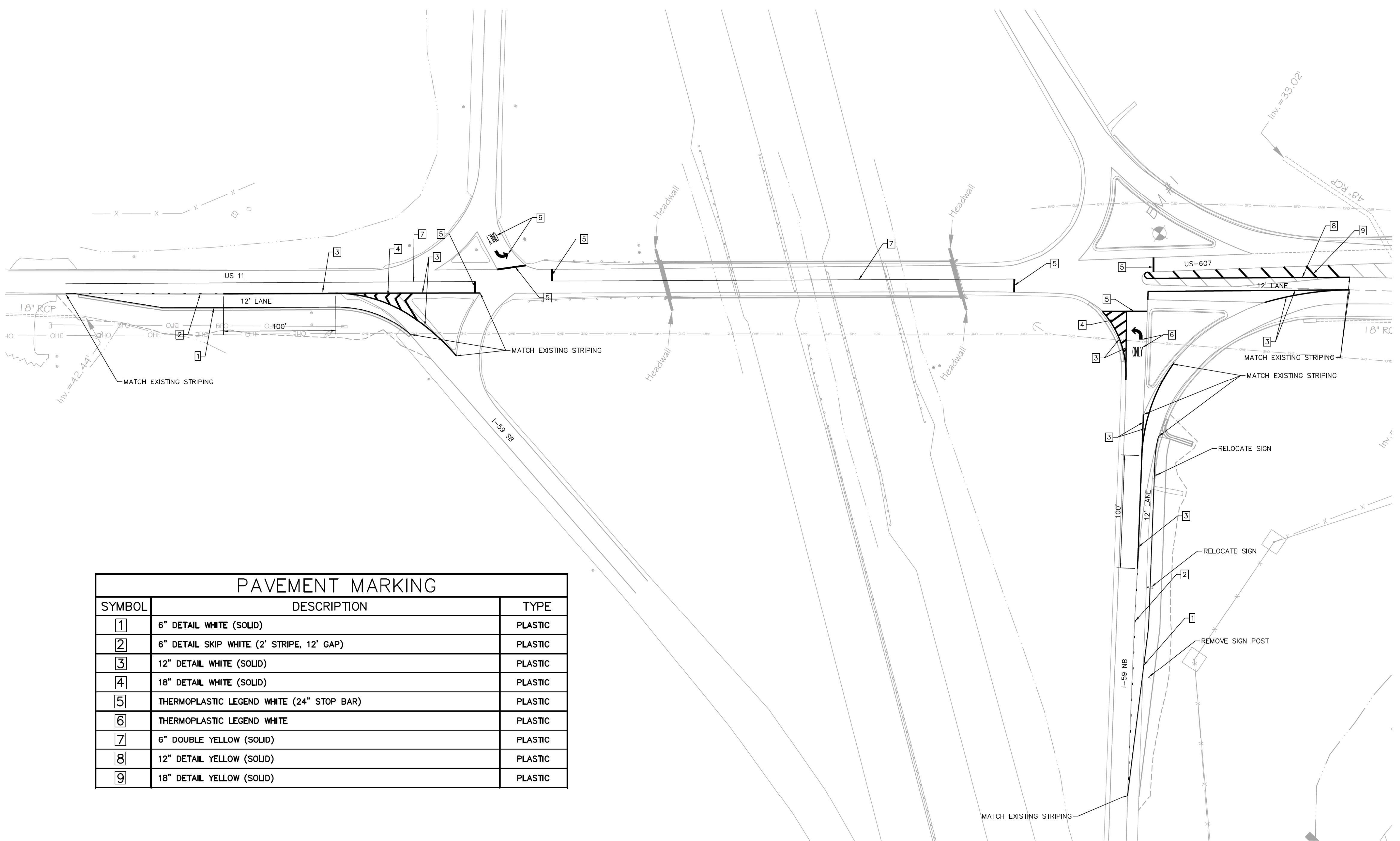
LOVE'S TRAVEL STOP
TRAFFIC SIGNAL DESIGN
PREPARED FOR
MDOT

SHEET NUMBER
C4

Plotted By: Tibbs, Seth - Sheet: Sct.Loves.MDOT - Layout: Layout1 - February 23, 2022 - 09:58:31am - K:\BIR-TPTO\142362033-Loves Pearl River MS\CA3\CA3-b PAVEMENT MARKING PLAN.dwg
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PAVEMENT MARKING		
SYMBOL	DESCRIPTION	TYPE
1	6" DETAIL WHITE (SOLID)	PLASTIC
2	6" DETAIL SKIP WHITE (2' STRIPE, 12' GAP)	PLASTIC
3	12" DETAIL WHITE (SOLID)	PLASTIC
4	18" DETAIL WHITE (SOLID)	PLASTIC
5	THERMOPLASTIC LEGEND WHITE (24" STOP BAR)	PLASTIC
6	THERMOPLASTIC LEGEND WHITE	PLASTIC
7	6" DOUBLE YELLOW (SOLID)	PLASTIC
8	12" DETAIL YELLOW (SOLID)	PLASTIC
9	18" DETAIL YELLOW (SOLID)	PLASTIC



NOTE:
 1. CONTRACTOR TO REMOVE ANY EXISTING PAVEMENT MARKING AND SIGNING THAT CONFLICTS WITH THE PROPOSED MARKING AS SHOWN ON THIS PLAN.

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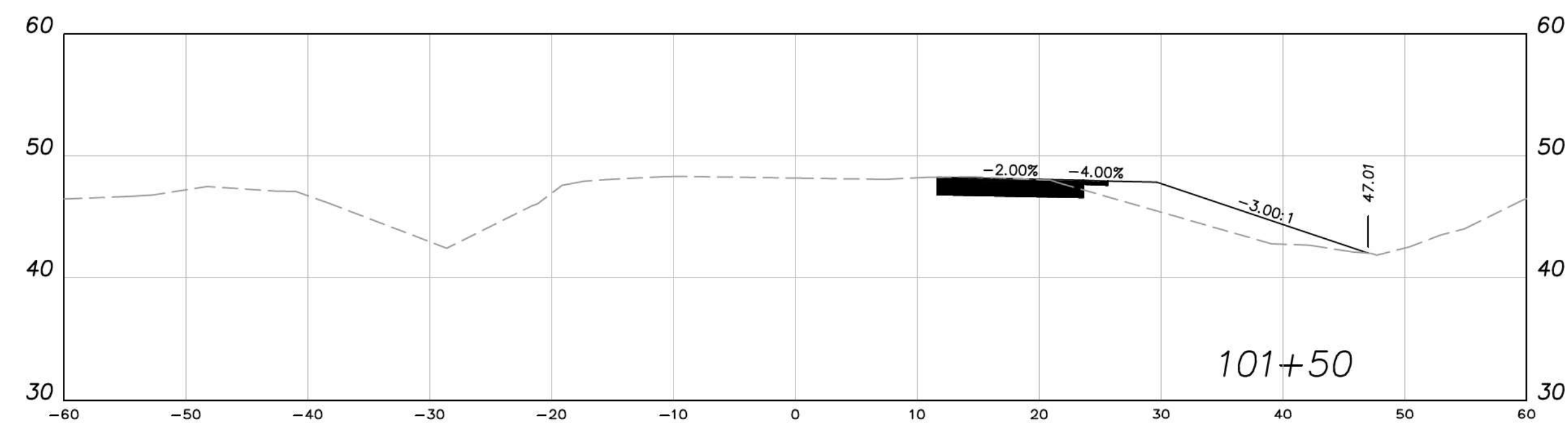
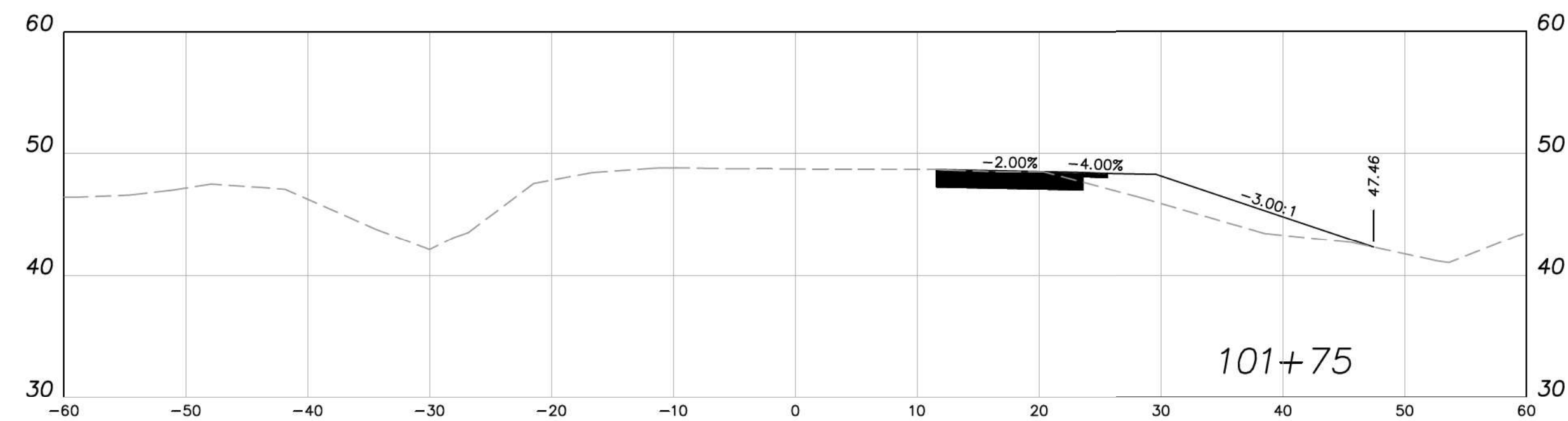
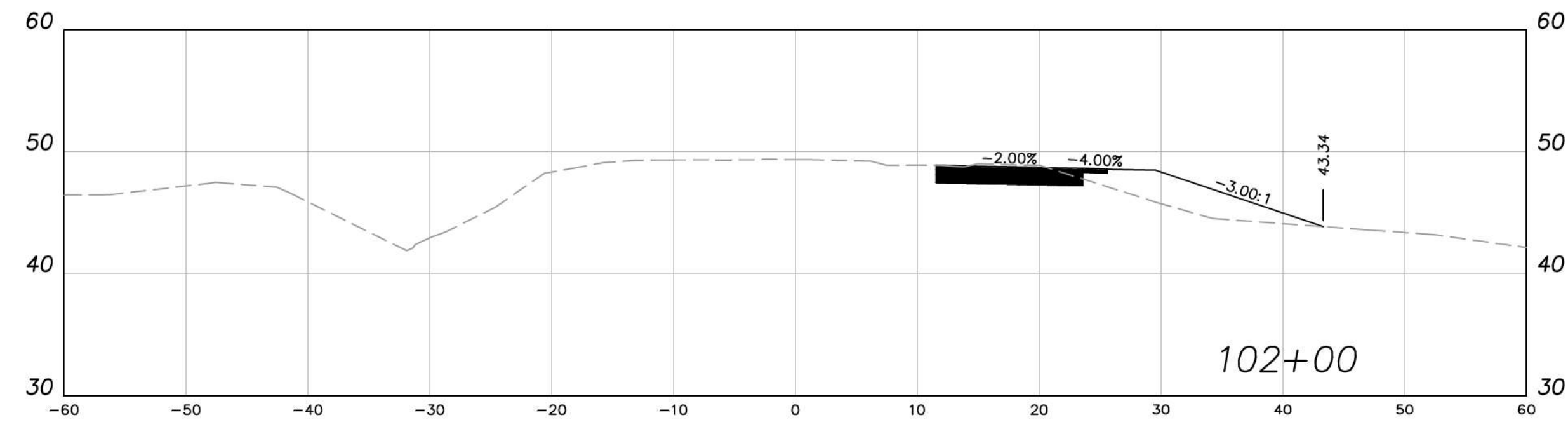
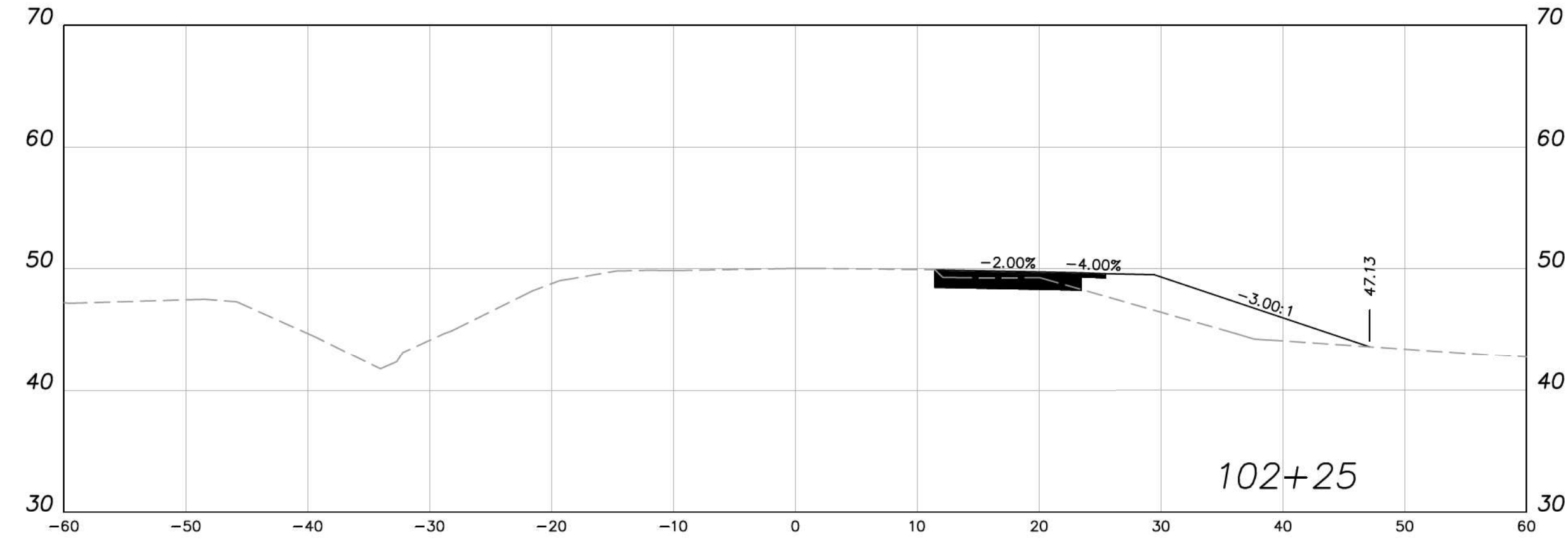
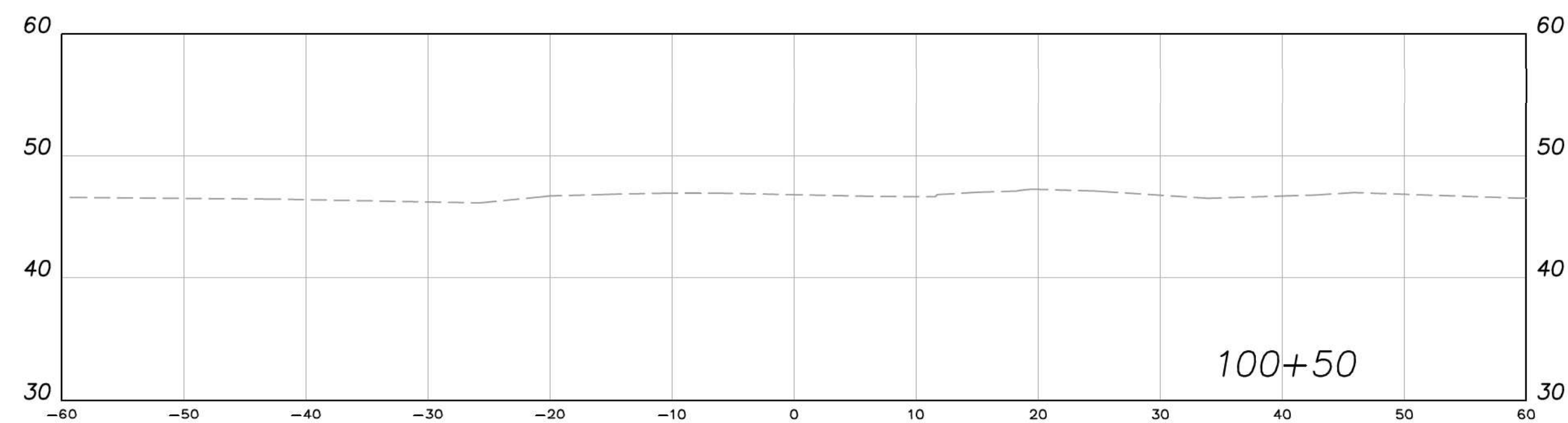
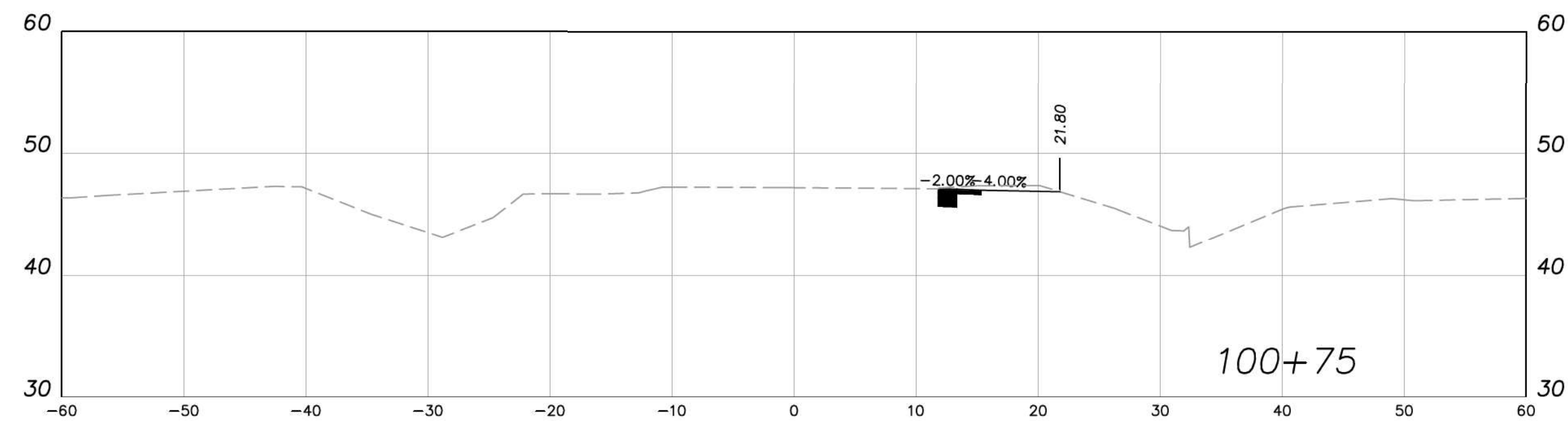
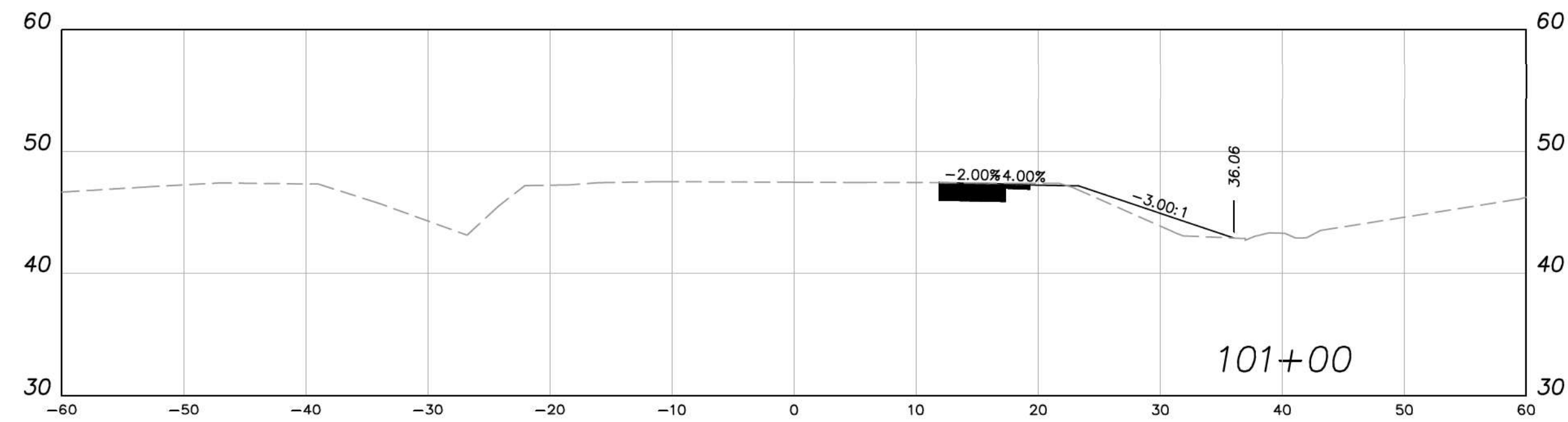
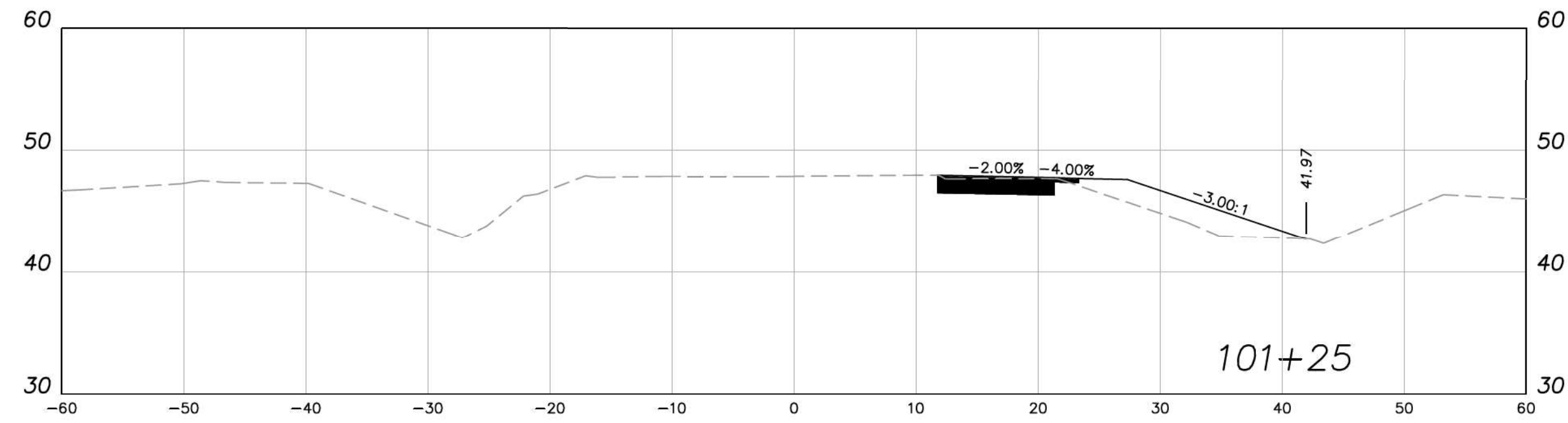
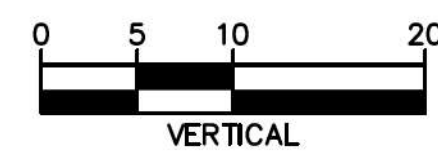
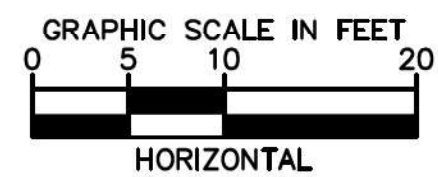
KHA PROJECT	142362033
DATE	2/2022
SCALE	AS SHOWN
DESIGNED BY	BST
DRAWN BY	BST
CHECKED BY	CBB

PAVEMENT MARKING PLAN

LOVE'S TRAVEL STOP
 TRAFFIC SIGNAL DESIGN
 PREPARED FOR
MDOT

PICAYUNE MS

Plotted By: Tibbs, Seth; Sheet: Srt.Loves.MDOT; Layout: C-6; February 23, 2022; 08:58:41pm; K:\BIBB_TPTD\142362033\Leaves_Traffic_Signals\ROADWAY_CROSS_SECTIONS.dwg
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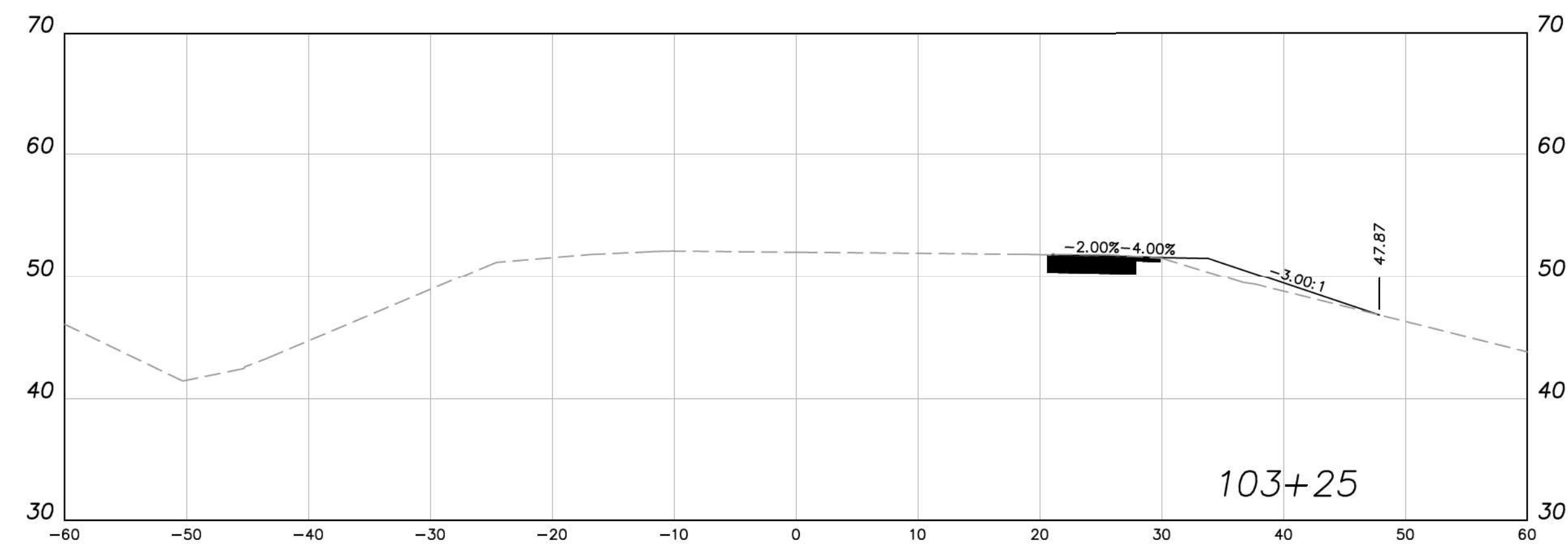
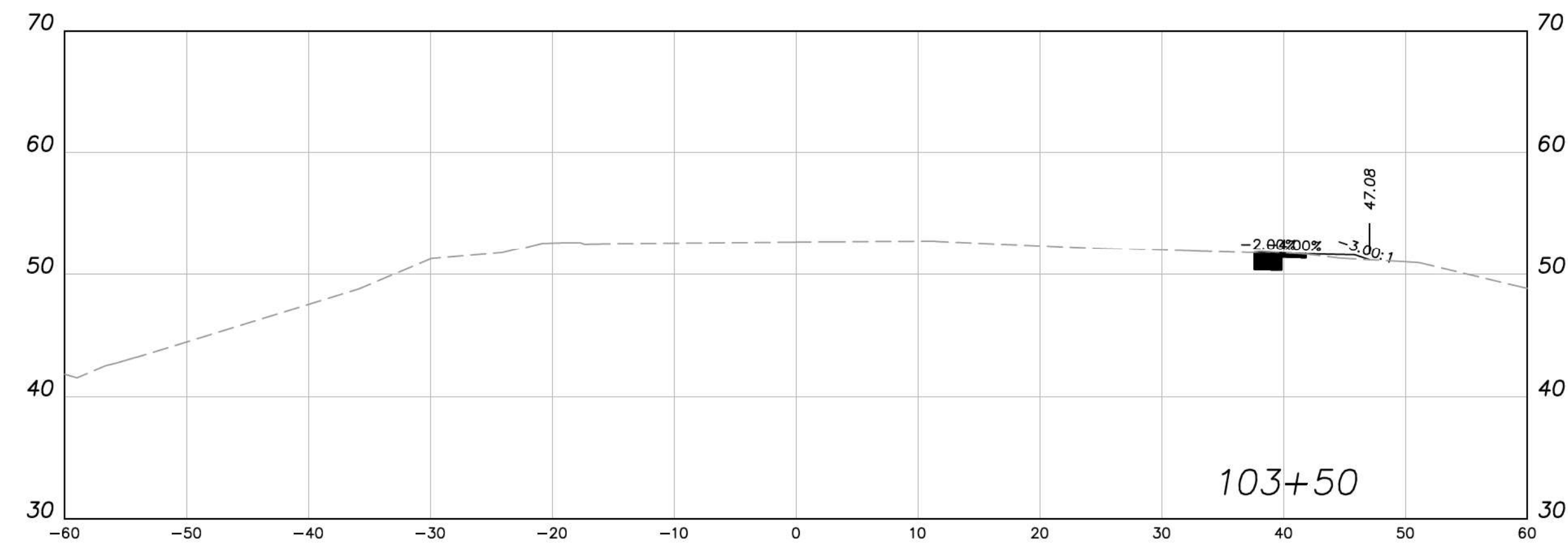
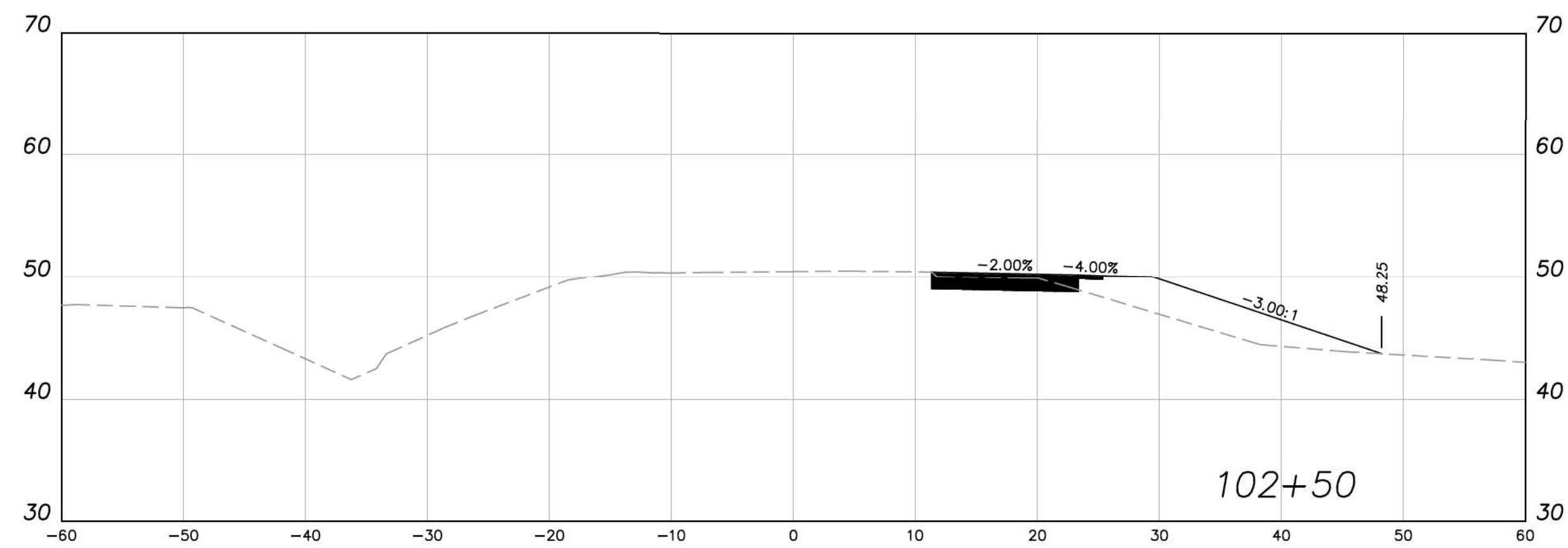
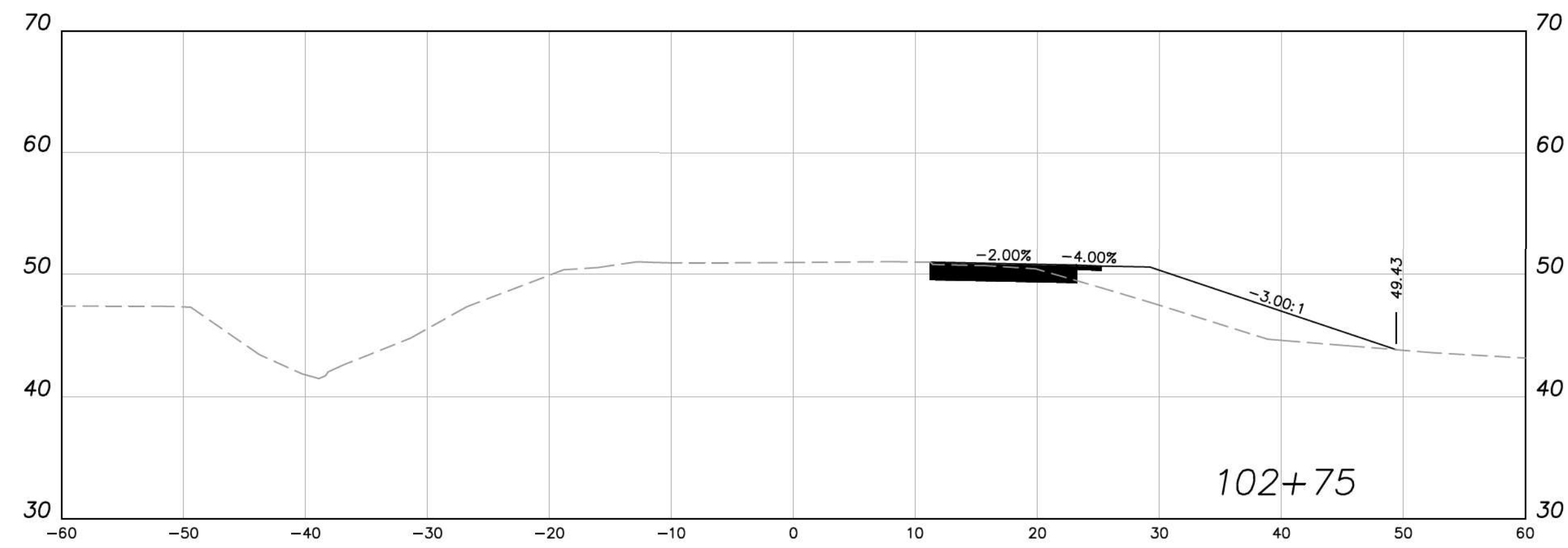
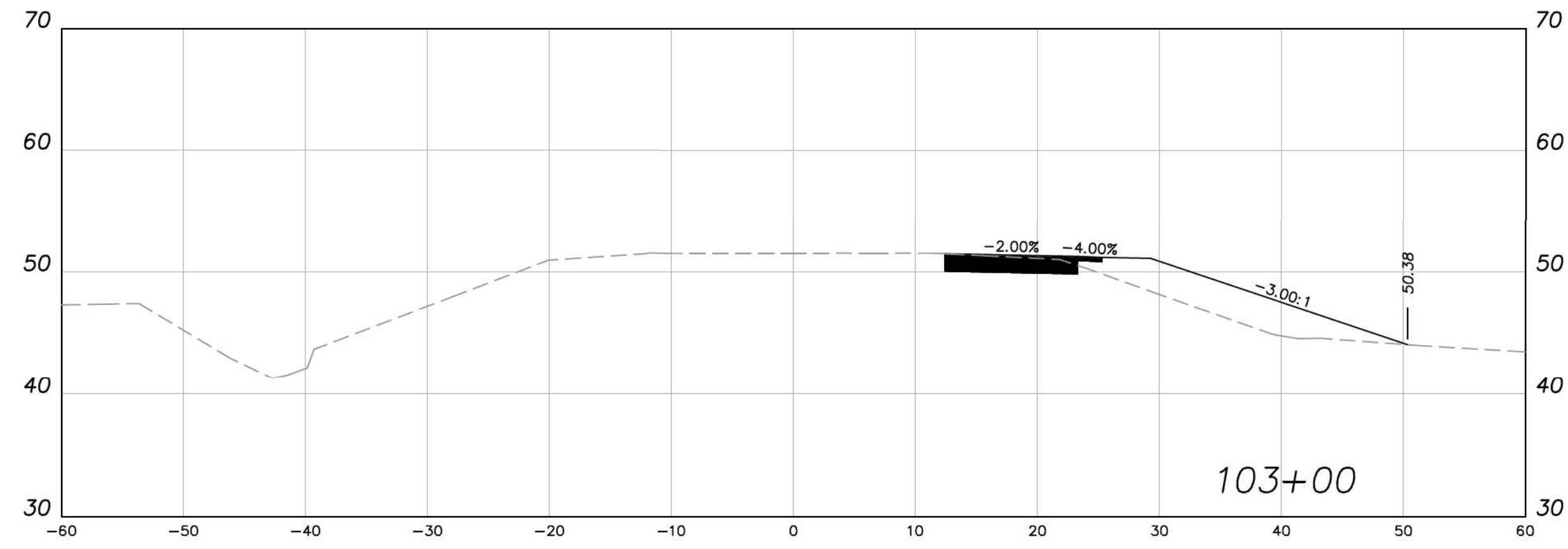
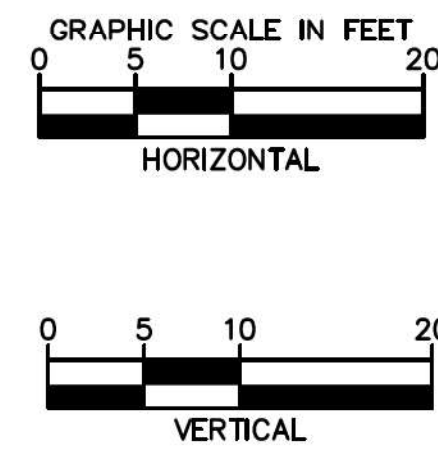


KHA PROJECT	142362033
DATE	2/2022
SCALE AS SHOWN	AS SHOWN
DESIGNED BY	IBST
DRAWN BY	IBST
CHECKED BY	CBB

ROADWAY CROSS SECTION

LOVE'S TRAVEL STOP
 TRAFFIC SIGNAL DESIGN
 PREPARED FOR
MDOT
 PICAYUNE MS

Plotted By: Tibbs, Seth - Sheet: Srt.Loves.MDOT - Layout: C-7 - February 23, 2022 - 08:58:42am - K:\BIR-TD\142362033-Loves Feed River MS\CAD\0-9 ROADWAY CROSS SECTIONS.dwg
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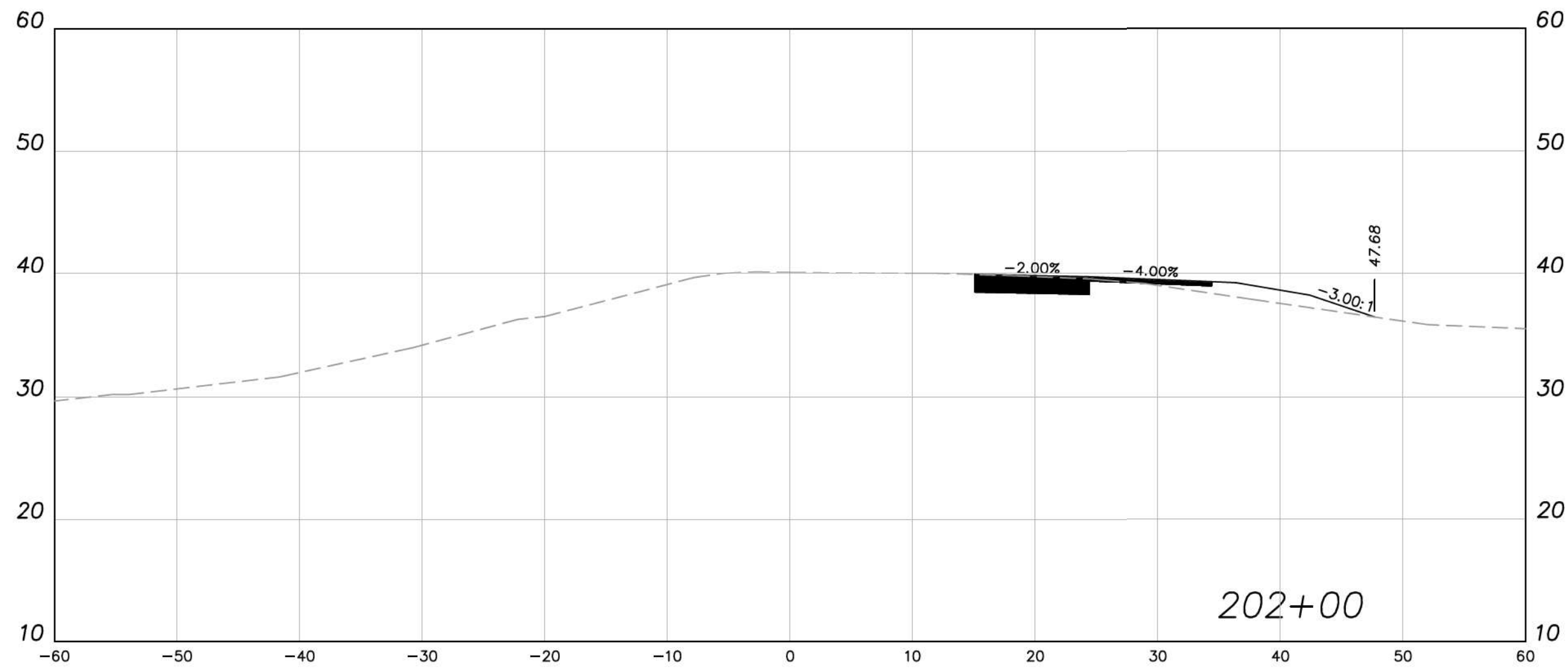
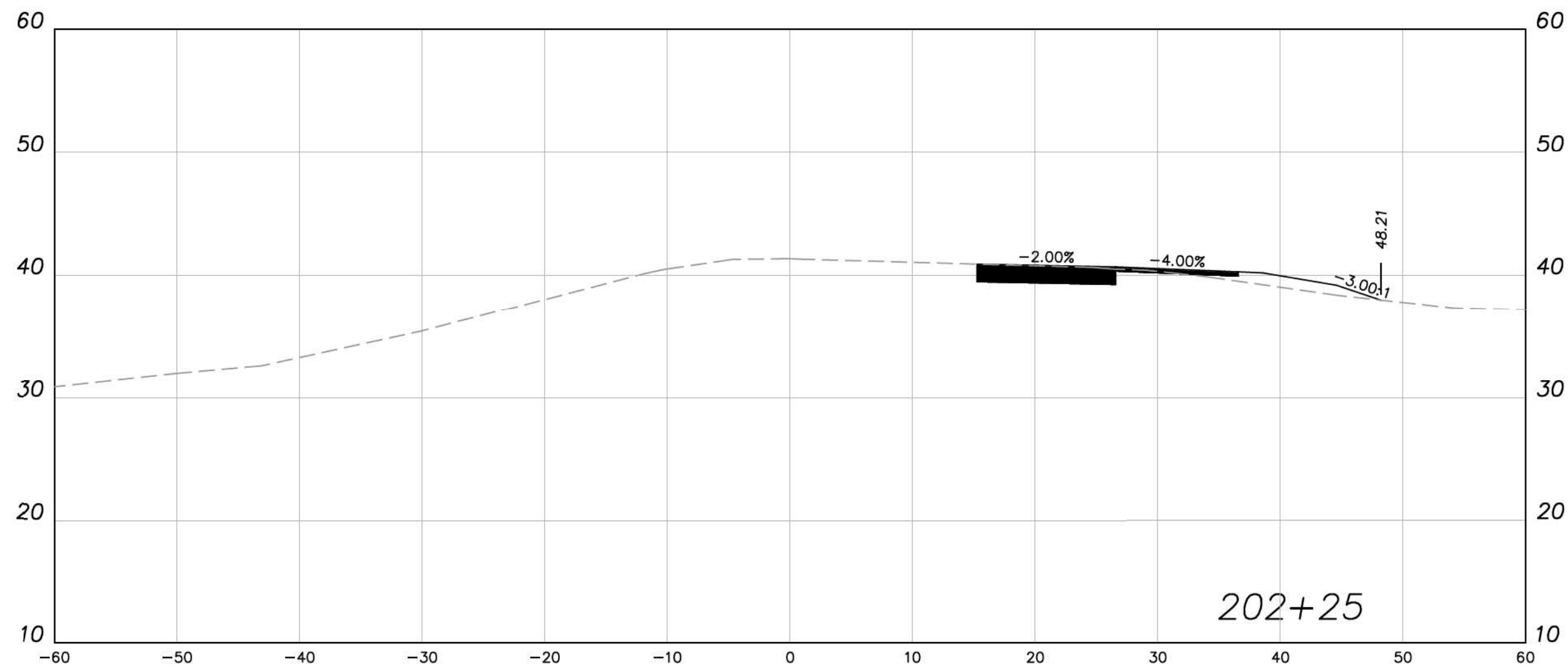
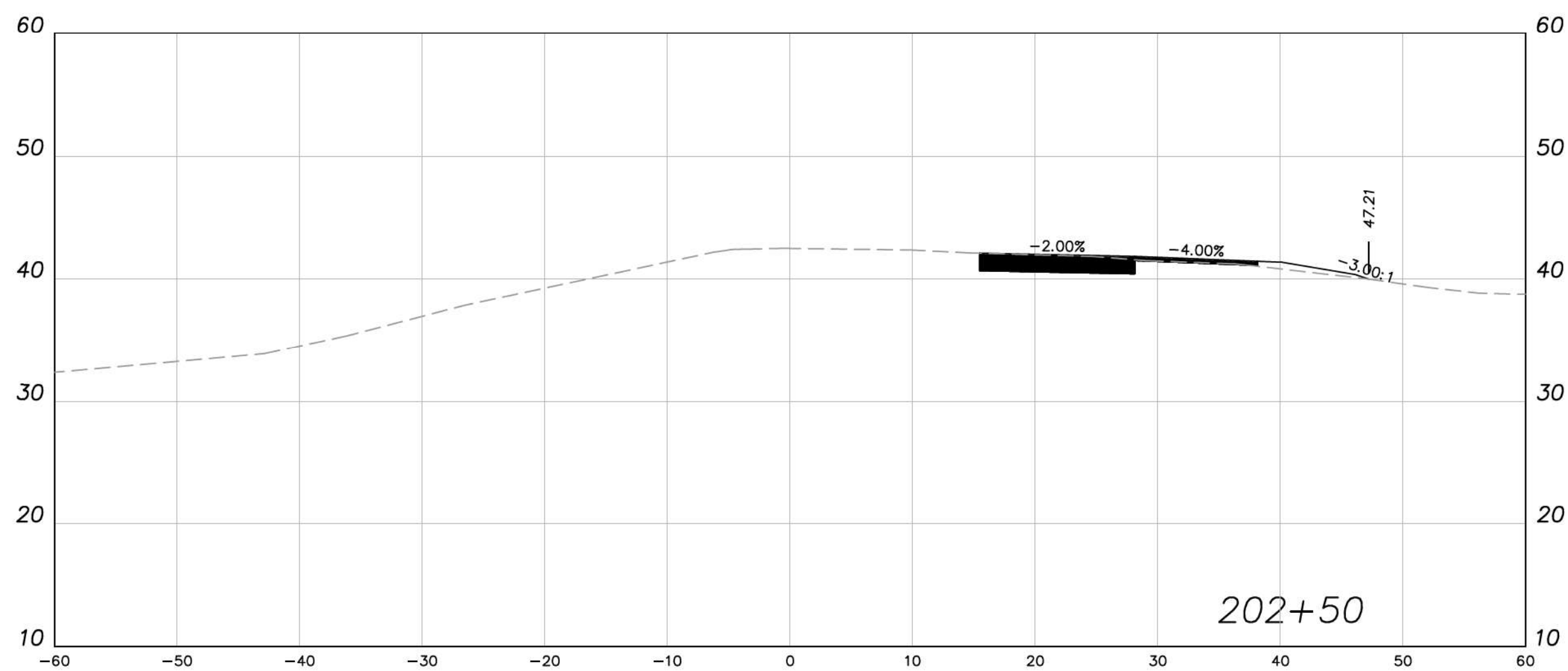
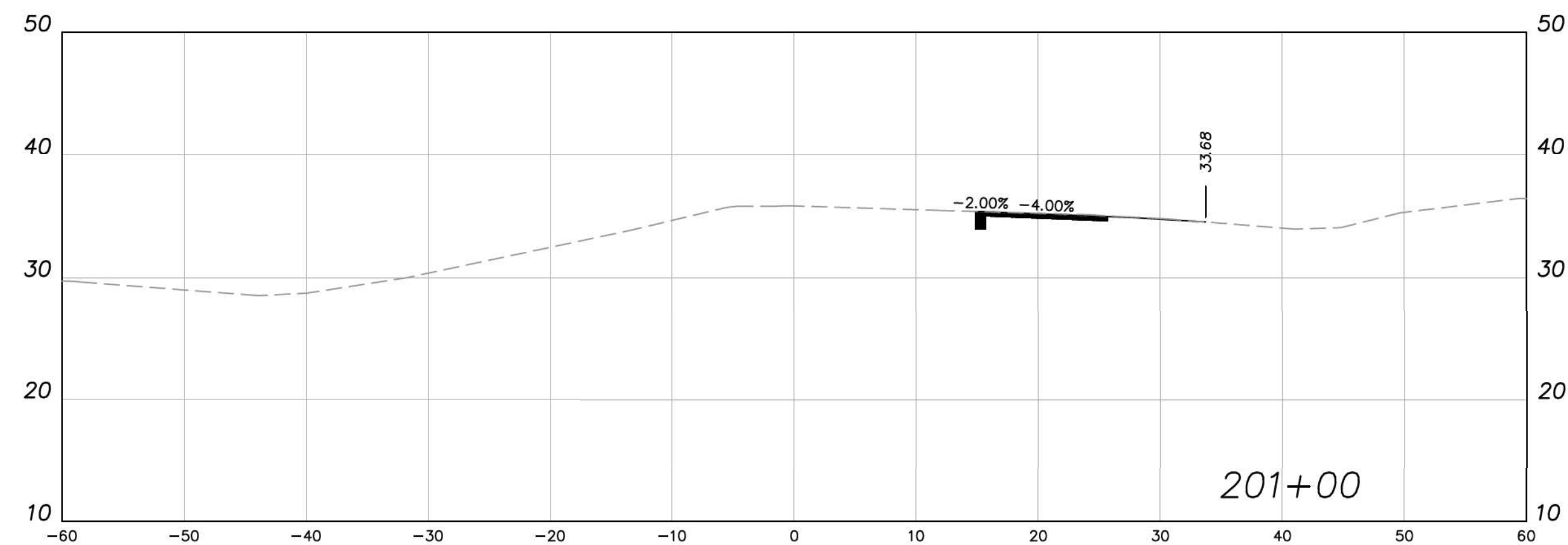
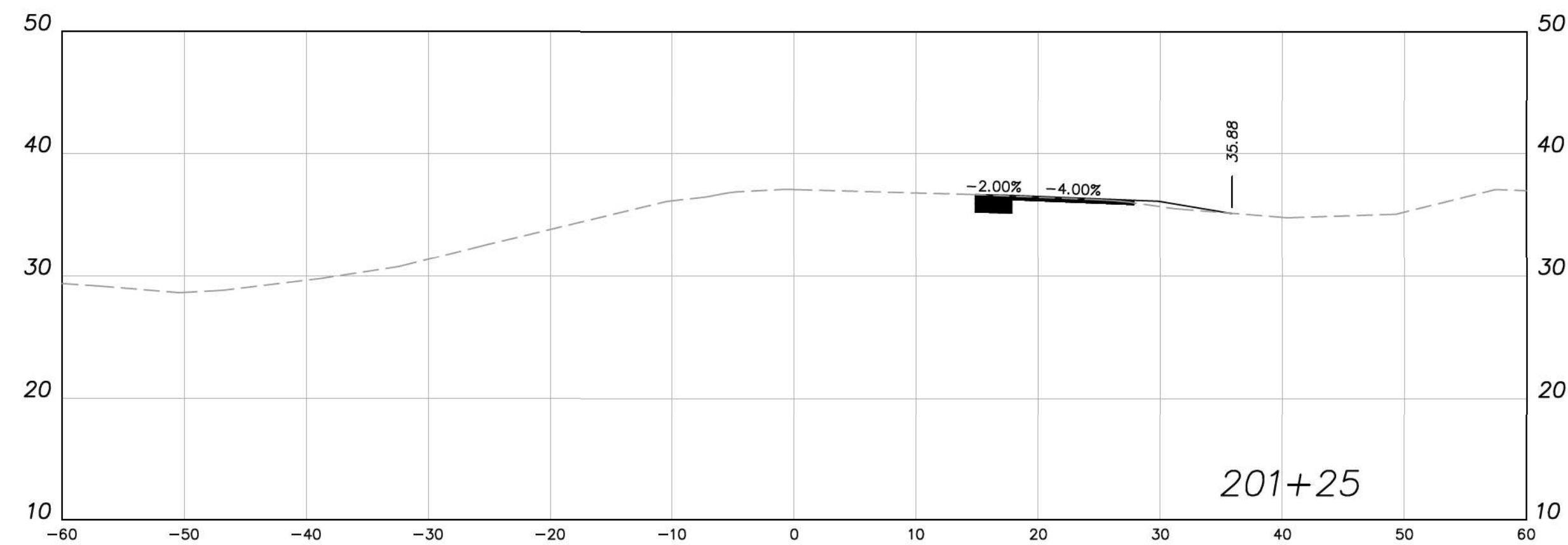
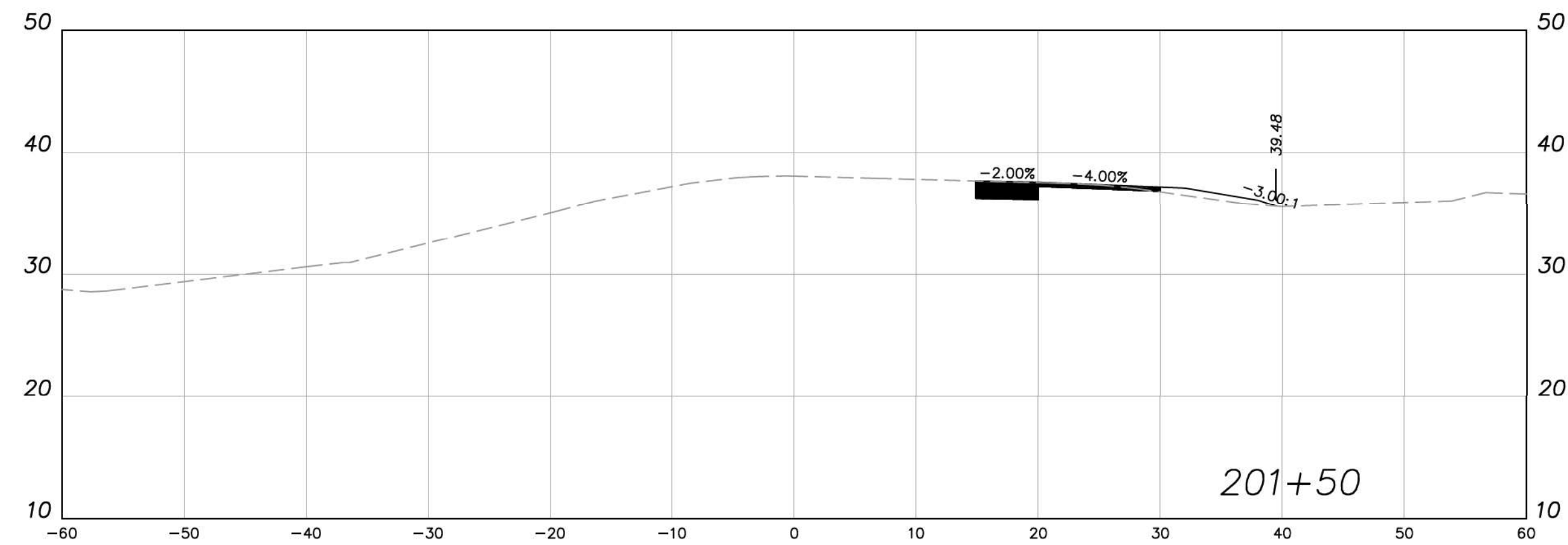
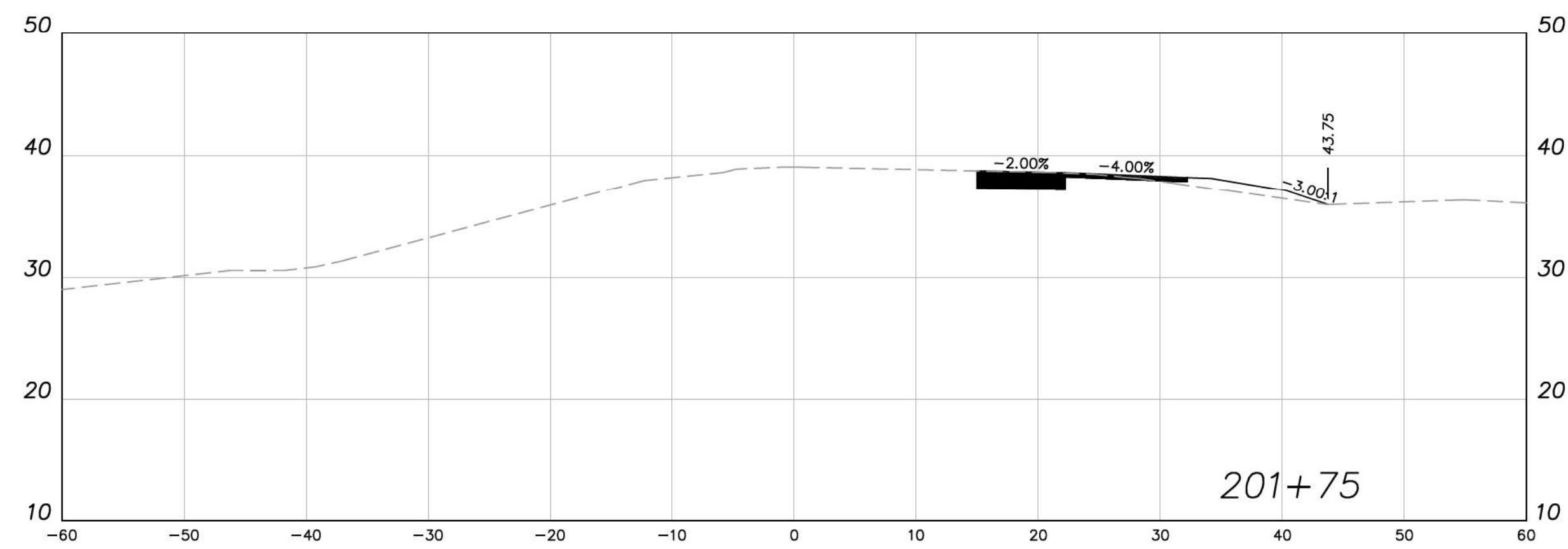
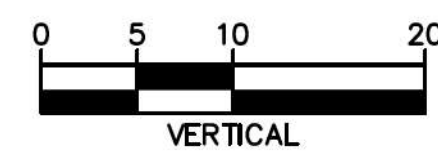
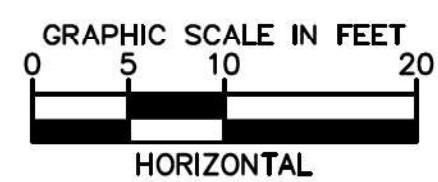
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DATE	2/2022
SCALE	AS SHOWN
DESIGNED BY	IST
DRAWN BY	IST
CHECKED BY	CBB

ROADWAY CROSS SECTION

LOVE'S TRAVEL STOP
 TRAFFIC SIGNAL DESIGN
 PREPARED FOR
MDOT
 PICAYUNE MS

SHEET NUMBER
C7

Plotted By: Tibbs, Seth - Sheet: Srt.Loves.MDOT - Layout: C=8 - February 23, 2022 - 08:58:42am - K:\BIR-TD\142362033-Loves Feed River MS\CAD\C=8-ROADWAY CROSS SECTIONS.dwg
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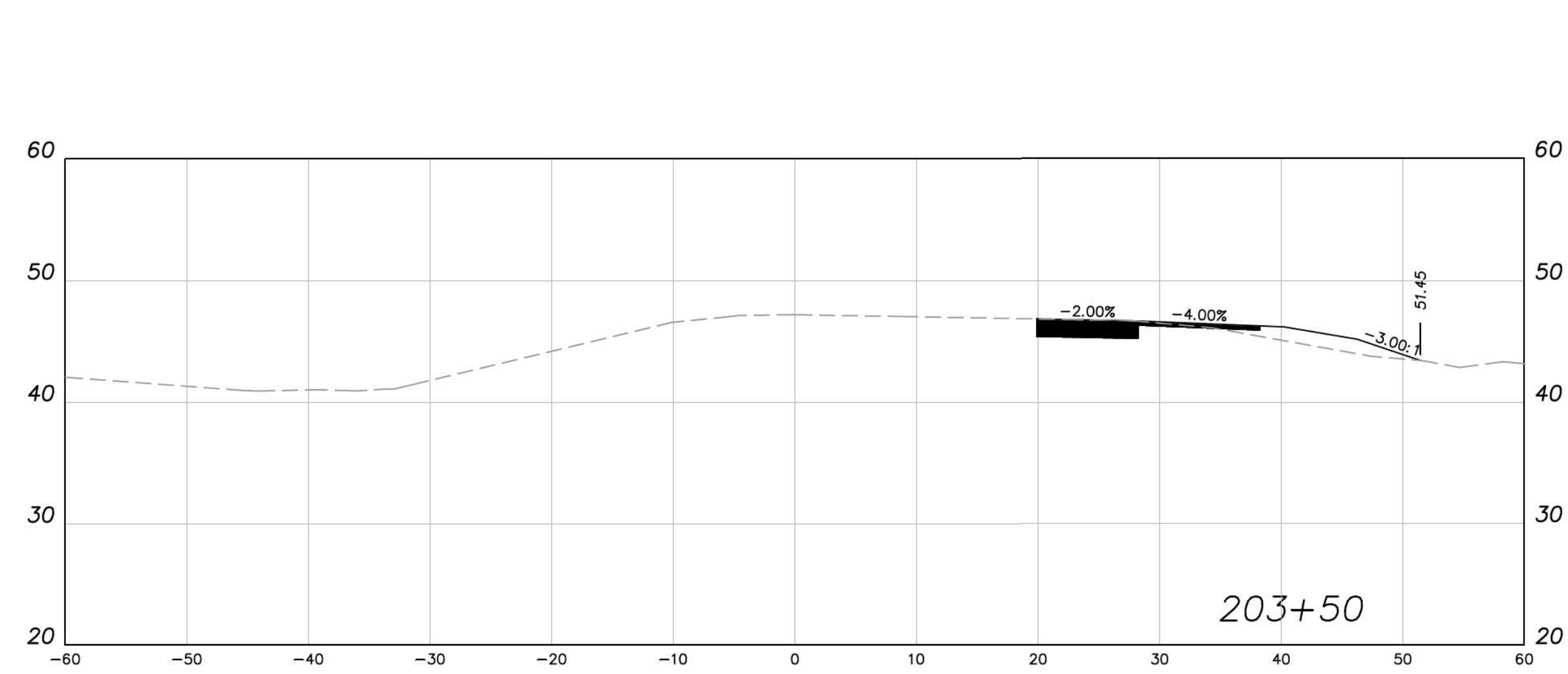
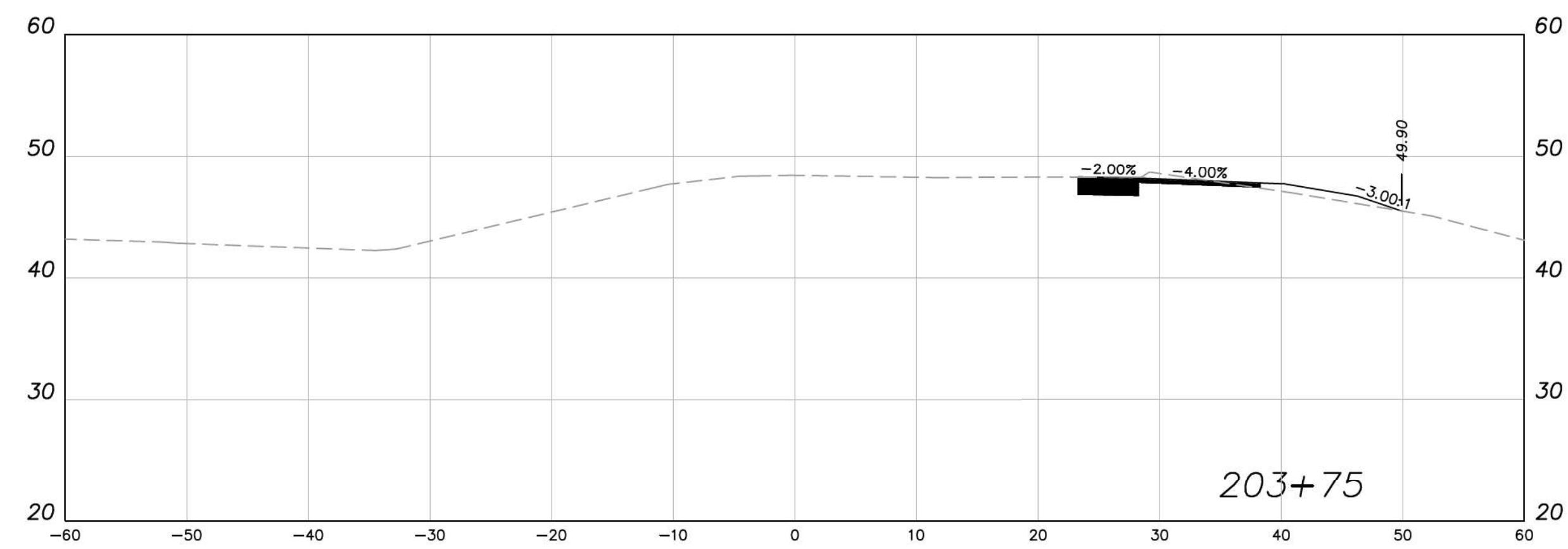
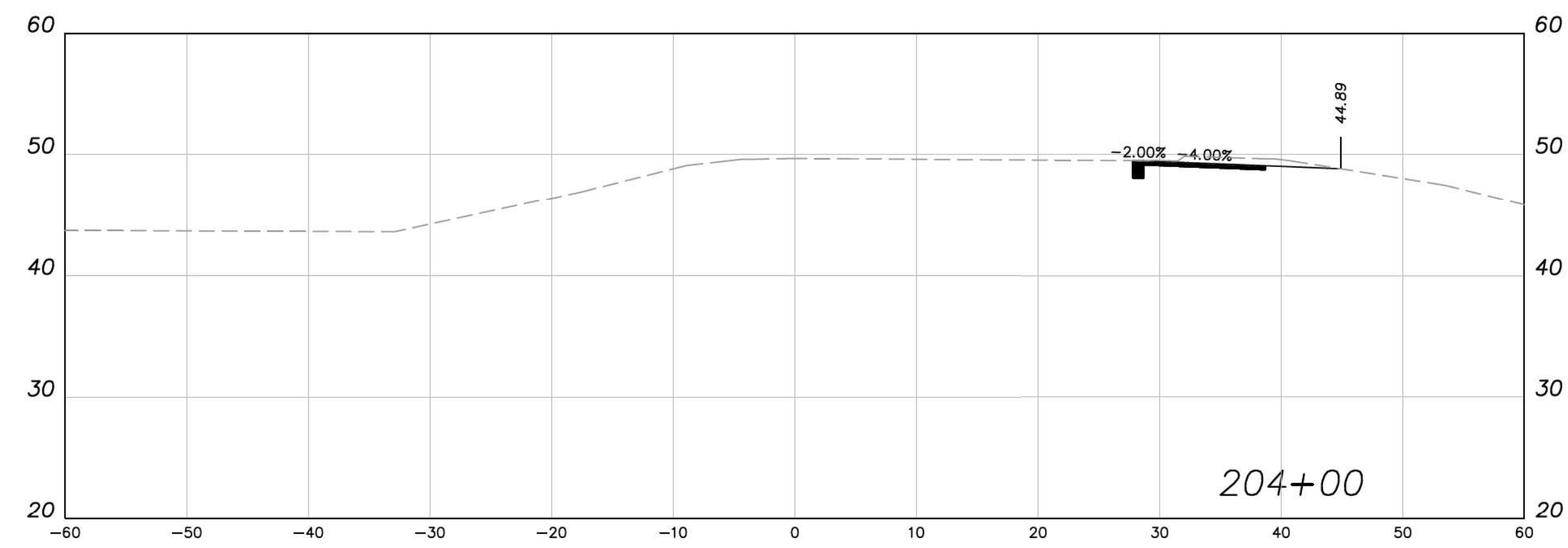
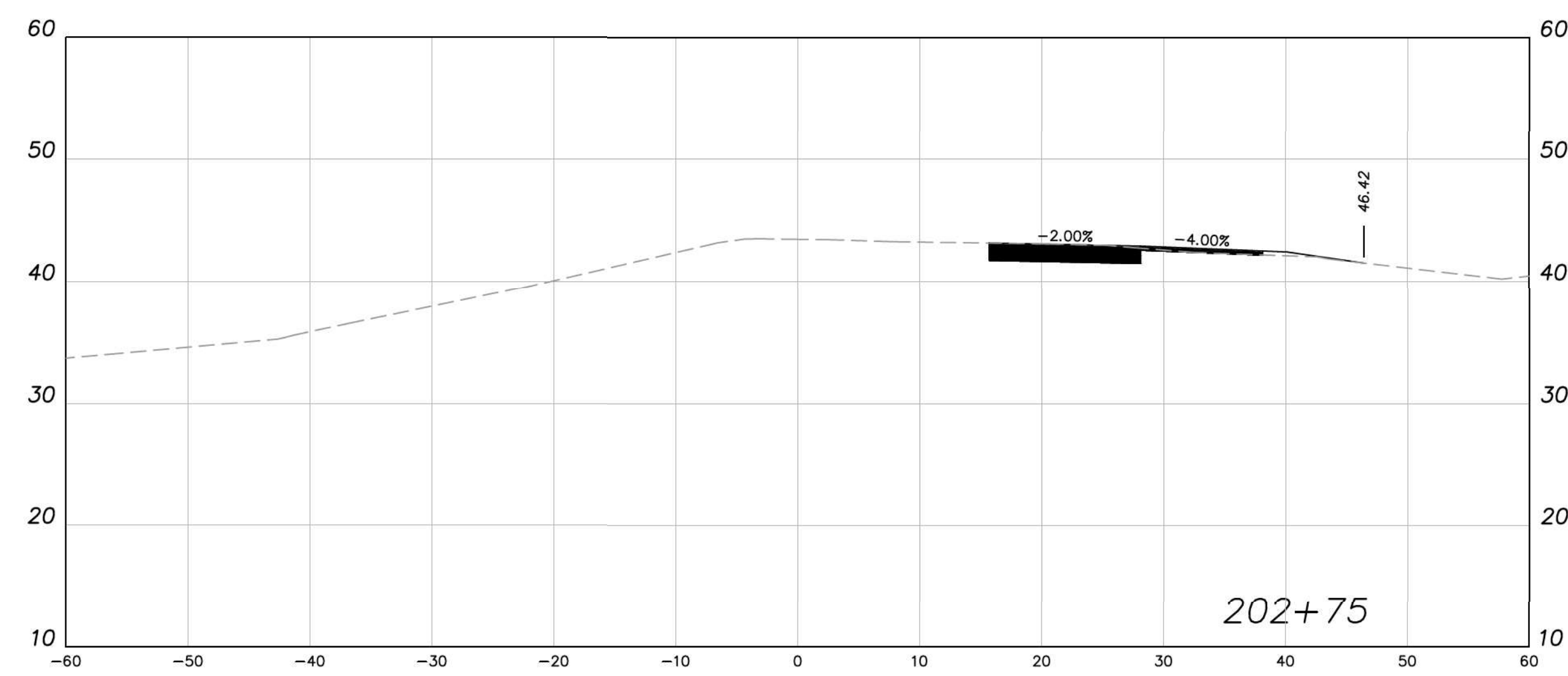
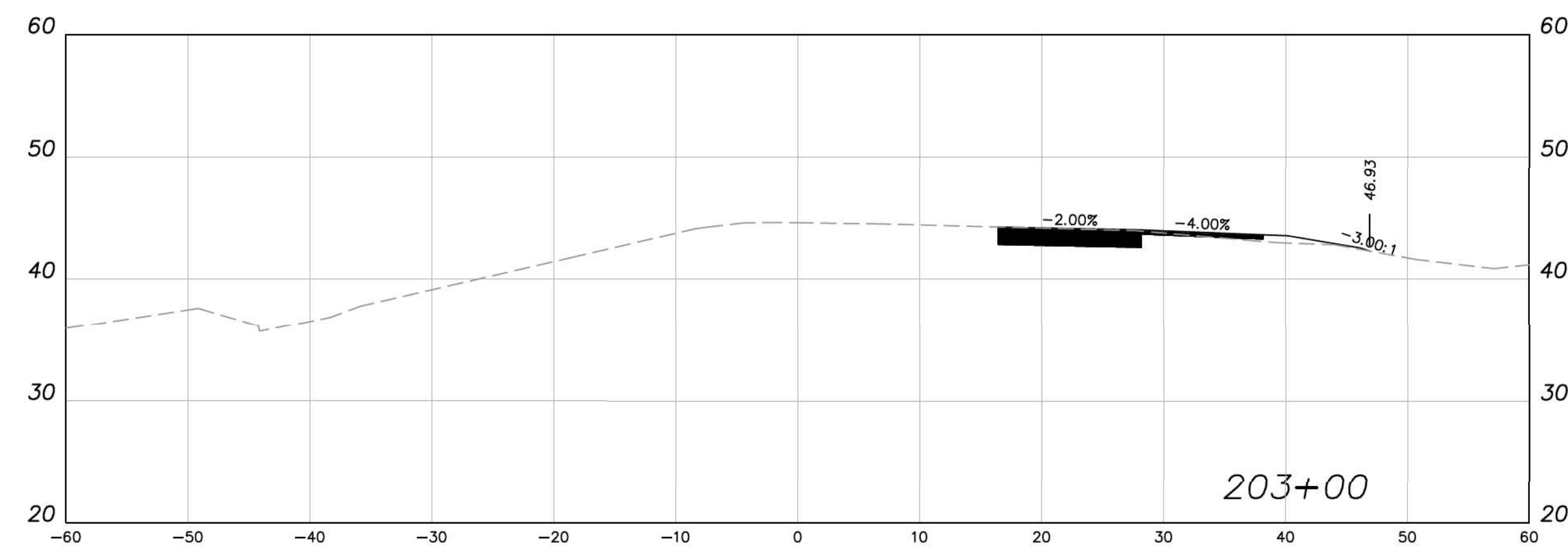
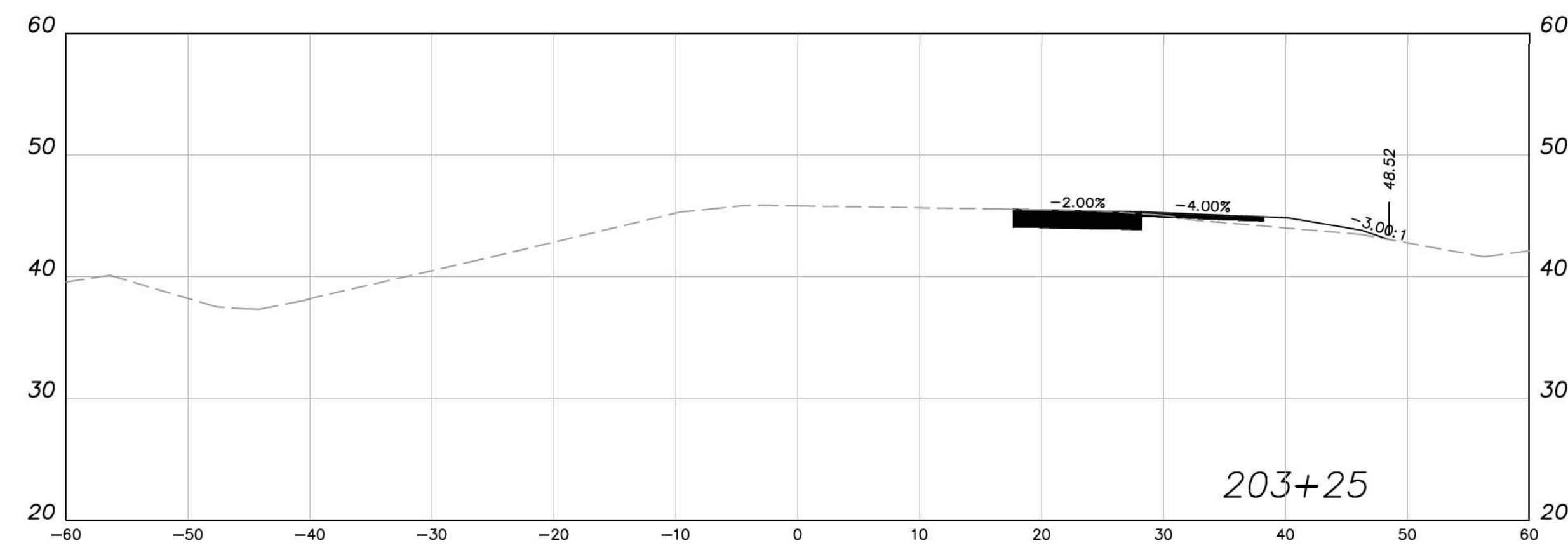
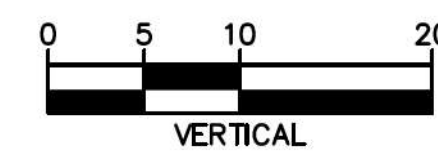
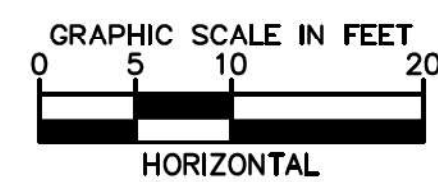
KHA PROJECT	142362033
DATE	2/2022
SCALE	AS SHOWN
DESIGNED BY	BST
DRAWN BY	BST
CHECKED BY	CBB

ROADWAY CROSS SECTION

LOVE'S TRAVEL STOP
 TRAFFIC SIGNAL DESIGN
 PREPARED FOR
MDOT

PICAYUNE MS

Plotted By: Tibbs, Seth - Sheet: Sct.Loves.MDOT - Layout: C-9 - February 23, 2022 08:58:44am. K:\BIR-TD\142362033-Loves Feed River MS\CAD\C-9 ROADWAY CROSS SECTIONS.dwg
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NORTHBOUND OFF RAMP RIGHT TURN LANE ADDITION

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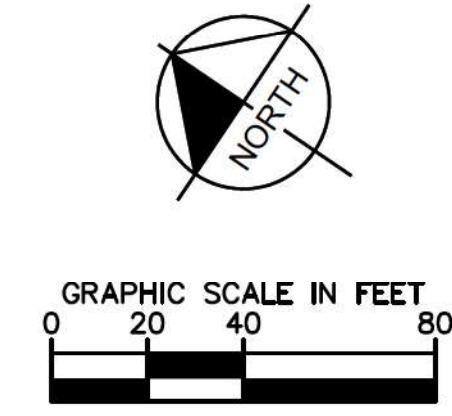


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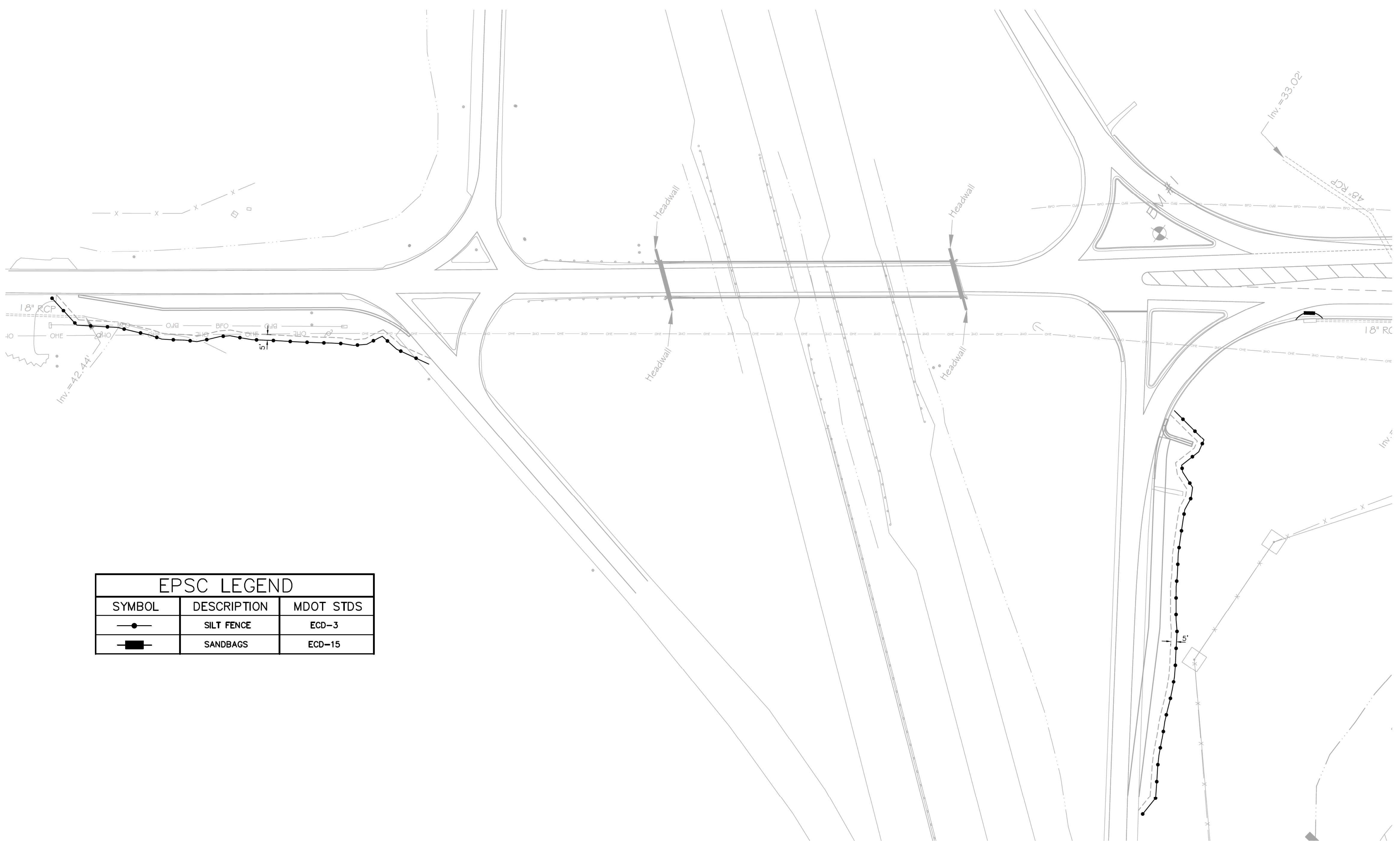
ROADWAY CROSS SECTION

LOVE'S TRAVEL STOP
 TRAFFIC SIGNAL DESIGN
 PREPARED FOR
MDOT
 PICAYUNE MS

Plotted By: Tibbs, Seth - Sheet: Silt, Loves MDOT - Layout: C-10 - February, 23, 2022 - 09:48:59am - K:\NBR-1770\142362033-Loves Pearl River, MS\CAD\10-EPSC PLAN.dwg
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EPSC LEGEND		
SYMBOL	DESCRIPTION	MDOT STDS
	SILT FENCE	ECD-3
	SANDBAGS	ECD-15



SHEET NUMBER
C10

LOVE'S TRAVEL STOP
TRAFFIC SIGNAL DESIGN
PREPARED FOR
MDOT

**EROSION
PREVENTION AND
SEDIMENT CONTROL
PLAN**

KHA PROJECT
142362033
DATE
2/2022
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DESIGNED BY BST
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