

A. 3.0" (2-1 1/2" LIFTS) OF TYPE 3 ASPHALTIC CONCRETE WEARING COURSE WITH INCIDENTAL PAVING PG 64-22 LIQUID ASPHALT (AGGREGATE 3/8" TO 3/4" MIX) IN ACCORDANCE WITH THE LOUISIANA DEPARTMENT OF TRANSPORTATION & DEVELOPMENT (LADOTD) STANDARD SPECIFICATIONS FOR ROADS & BRIDGES, SECTION 501 OF THE 2000 EDITION.

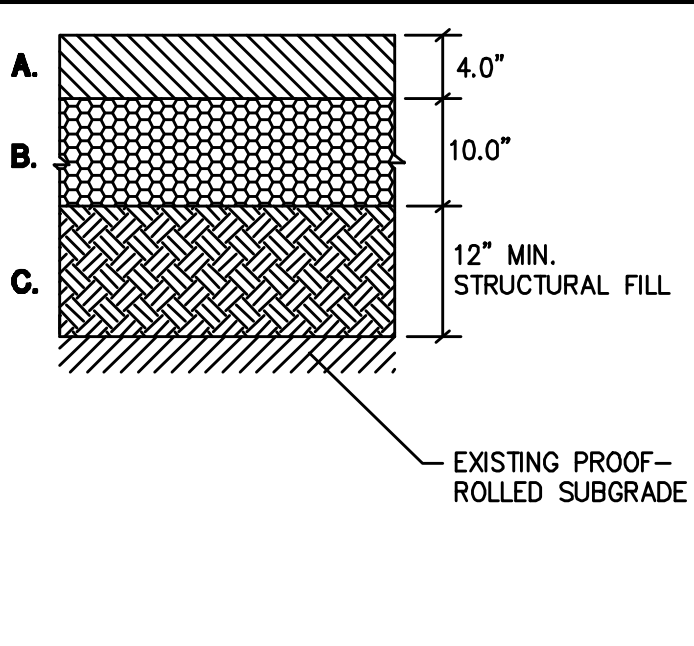
B. 6.0" OF COMPACTED 610 LIMESTONE BASE. THE CRUSHED LIMESTONE BASE SHALL MEET THE REQUIREMENTS OF THE LATEST EDITION OF LOUISIANA STANDARD SPECIFICATIONS FOR ROADS & BRIDGES (LSSRB) SECTION 1003.03, & BE COMPACTED TO AT LEAST 95 PERCENT OF THE MAXIMUM DRY DENSITY DETERMINED BY ASTM D698 (STANDARD PROCTOR) WITHIN 3 PERCENT OF OPTIMUM MOISTURE CONTENT.

C. 12.0" MIN. STRUCTURAL FILL. THE STRUCTURAL FILL SHALL BE COMPACTED TO AT LEAST 95% OF THE SOIL'S MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D698 (STANDARD PROCTOR) AND WITHIN THE RANGE OF 1% BELOW TO 3% ABOVE THE OPTIMUM MOISTURE CONTENT VALUE.

NOTES:

- 1) ALL STRUCTURAL FILL SHALL BE FREE OF ORGANIC AND OTHER DELETERIOUS MATERIALS, HAVE A MAXIMUM PARTICLE SIZE LESS THAN 2 INCHES, A LIQUID LIMIT LESS THAN 40, AND A PLASTICITY INDEX LESS THAN 18. SANDY CLAY OR CLAYEY SANDS ARE RECOMMENDED FOR USE AS STRUCTURAL FILL. THE ON-SITE SILTY SANDS MAY BE SUITABLE FOR USE AS FILL.
- 2) FILL SHALL BE PLACED IN A MAXIMUM OF 8" LOOSE LIFTS & COMPACTED WITHIN 1% BELOW TO 3% ABOVE OPTIMUM MOISTURE CONTENT VALUE. IF WATER MUST BE ADDED, IT SHALL BE UNIFORMLY APPLIED AND THOROUGHLY MIXED INTO THE SOIL BY DISKING OR SCARIFYING.
- 3) EXISTING SUBGRADE IN PARKING AREAS SHALL BE PROOF-ROLLED WITH A LOADED TANDEM AXLE DUMP TRUCK OR SIMILAR HEAVY RUBBER Tired VEHICLE. SOILS THAT RUT OR DEFLECT EXCESSIVELY UNDER THE MOVING LOAD SHALL BE UNDERCUT AND REPLACED WITH PROPERLY COMPACTED STRUCTURAL FILL.
- 4) ALL SUBGRADE, BASE & PAVEMENT CONSTRUCTION OPERATIONS SHOULD MEET MINIMUM REQUIREMENTS OF THE LOUISIANA DEPARTMENT OF TRANSPORTATION.

LIGHT DUTY ASPHALTIC PAVEMENT
N.T.S.



A. 4.0" (2-2" LIFTS) OF TYPE 3 ASPHALTIC CONCRETE WEARING COURSE WITH INCIDENTAL PAVING PG 64-22 LIQUID ASPHALT (AGGREGATE 3/8" TO 3/4" MIX) IN ACCORDANCE WITH THE LOUISIANA DEPARTMENT OF TRANSPORTATION & DEVELOPMENT (LADOTD) STANDARD SPECIFICATIONS FOR ROADS & BRIDGES, SECTION 501 OF THE 2000 EDITION.

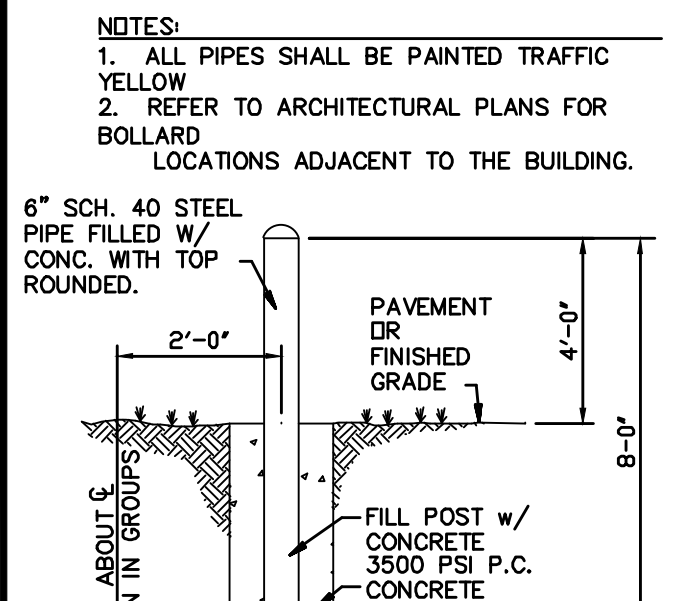
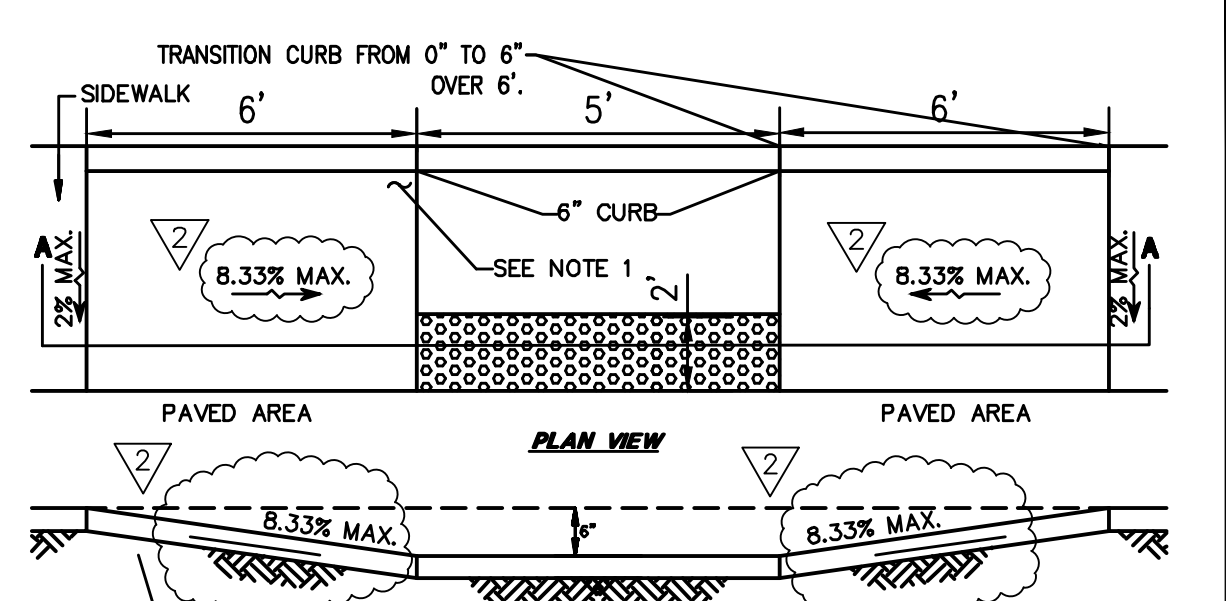
B. 10.0" OF COMPACTED 610 LIMESTONE BASE. THE CRUSHED LIMESTONE BASE SHALL MEET THE REQUIREMENTS OF THE LATEST EDITION OF LOUISIANA STANDARD SPECIFICATIONS FOR ROADS & BRIDGES (LSSRB) SECTION 1003.03, & BE COMPACTED TO AT LEAST 95 PERCENT OF THE MAXIMUM DRY DENSITY DETERMINED BY ASTM D698 (STANDARD PROCTOR) WITHIN 3 PERCENT OF OPTIMUM MOISTURE CONTENT.

C. 12.0" MIN. STRUCTURAL FILL. THE STRUCTURAL FILL SHALL BE COMPACTED TO AT LEAST 95% OF THE SOIL'S MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D698 (STANDARD PROCTOR) AND WITHIN THE RANGE OF 1% BELOW TO 3% ABOVE THE OPTIMUM MOISTURE CONTENT VALUE.

NOTES:

- 1) ALL STRUCTURAL FILL SHALL BE FREE OF ORGANIC AND OTHER DELETERIOUS MATERIALS, HAVE A MAXIMUM PARTICLE SIZE LESS THAN 2 INCHES, A LIQUID LIMIT LESS THAN 40, AND A PLASTICITY INDEX LESS THAN 18. SANDY CLAY OR CLAYEY SANDS ARE RECOMMENDED FOR USE AS STRUCTURAL FILL. THE ON-SITE SILTY SANDS MAY BE SUITABLE FOR USE AS FILL.
- 2) FILL SHALL BE PLACED IN A MAXIMUM OF 8" LOOSE LIFTS & COMPACTED WITHIN 1% BELOW TO 3% ABOVE OPTIMUM MOISTURE CONTENT VALUE. IF WATER MUST BE ADDED, IT SHALL BE UNIFORMLY APPLIED AND THOROUGHLY MIXED INTO THE SOIL BY DISKING OR SCARIFYING.
- 3) EXISTING SUBGRADE IN PARKING AREAS SHALL BE PROOF-ROLLED WITH A LOADED TANDEM AXLE DUMP TRUCK OR SIMILAR HEAVY RUBBER Tired VEHICLE. SOILS THAT RUT OR DEFLECT EXCESSIVELY UNDER THE MOVING LOAD SHALL BE UNDERCUT AND REPLACED WITH PROPERLY COMPACTED STRUCTURAL FILL.
- 4) ALL SUBGRADE, BASE & PAVEMENT CONSTRUCTION OPERATIONS SHOULD MEET MINIMUM REQUIREMENTS OF THE LOUISIANA DEPARTMENT OF TRANSPORTATION.

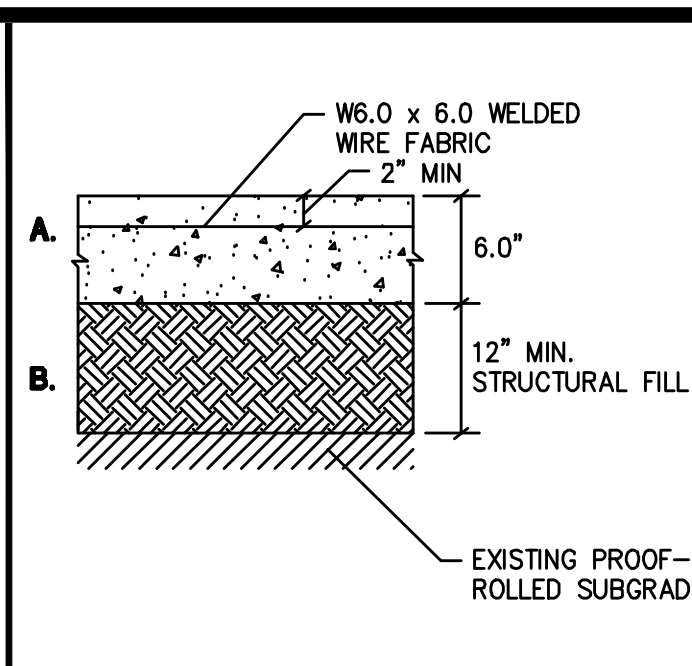
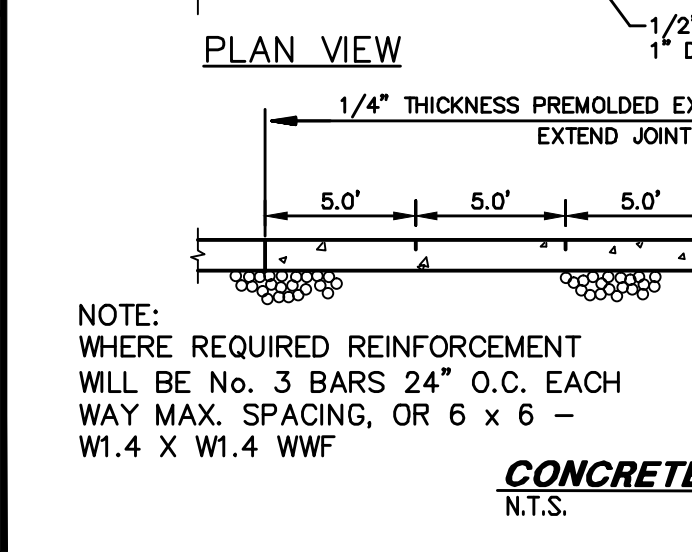
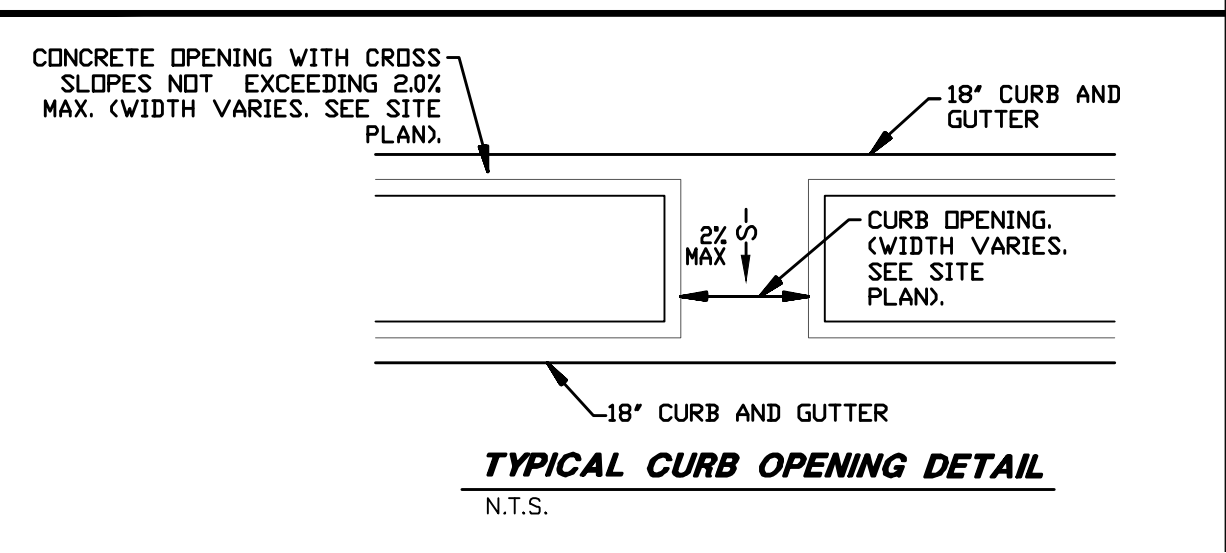
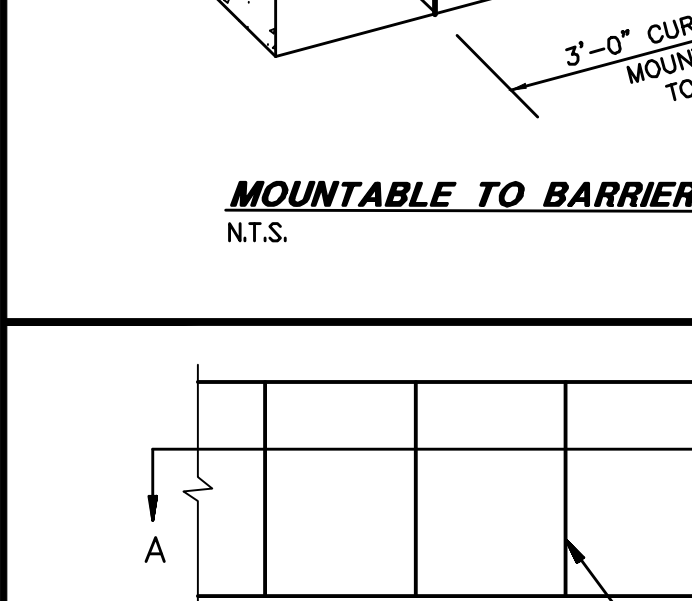
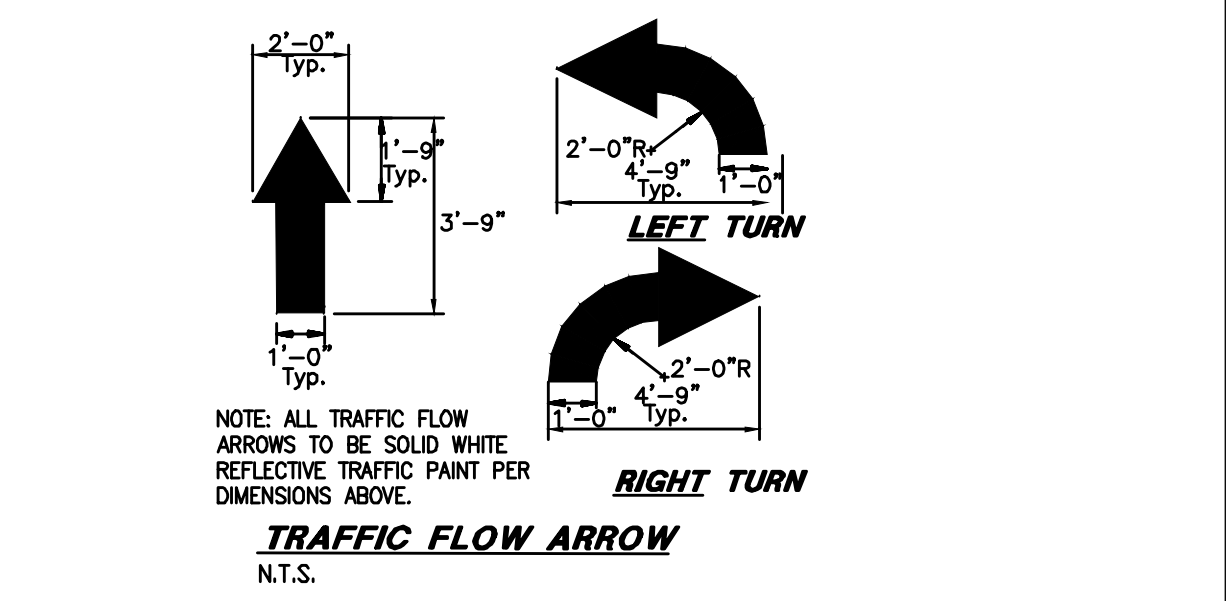
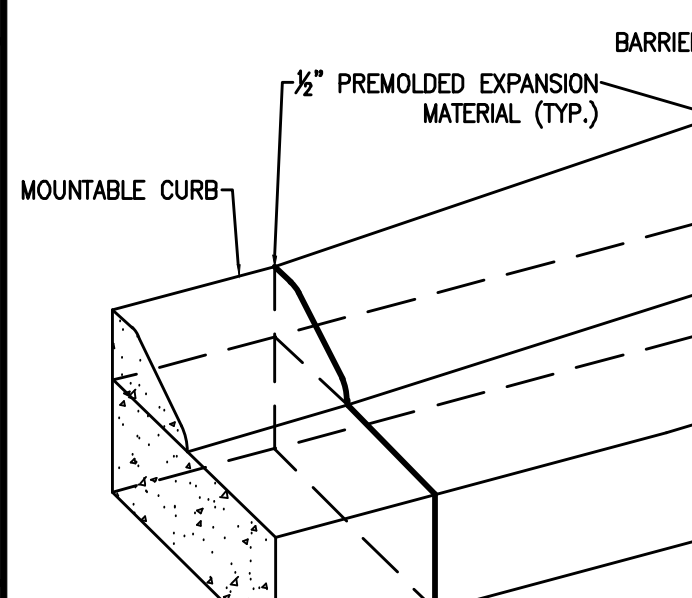
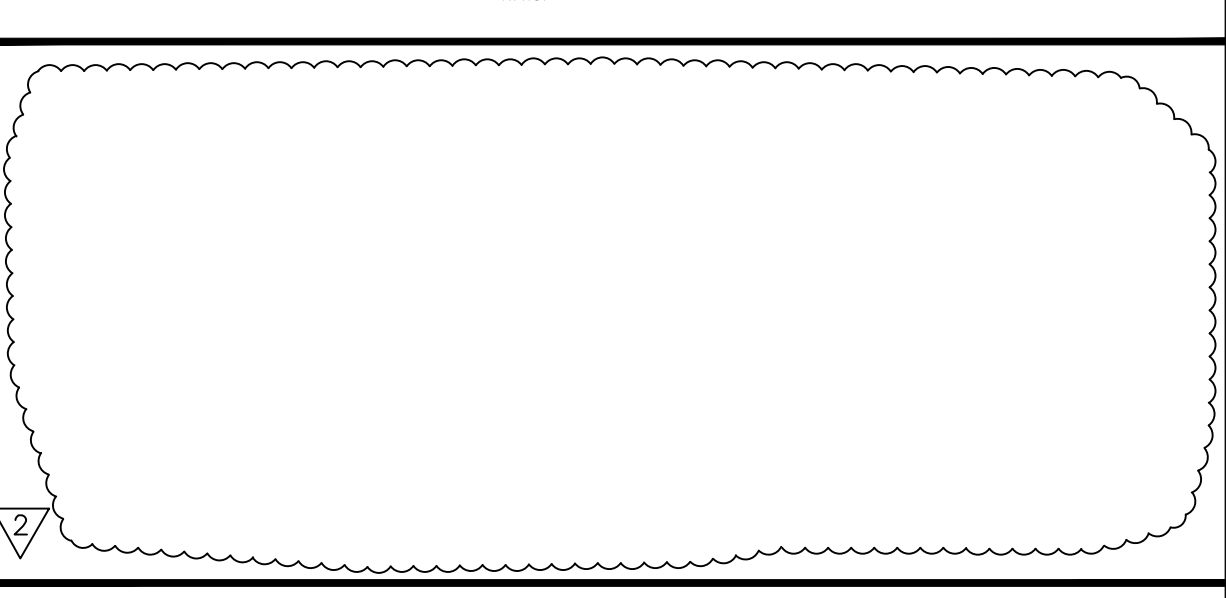
HEAVY DUTY ASPHALTIC PAVEMENT
N.T.S.



NOTES:

1. WHERE SHOWN AS DOTTED PATTERN, THE SURFACE OF RAMP SHALL HAVE A TERRA COTTA TILED TRUNCATED DOME BRICK MAT, SET FLUSH WITH TOP OF CONCRETE SURFACE AND INSTALLED AS PER MANUFACTURER'S RECOMMENDATIONS.
2. CONSTRUCT PER A.D.A. STANDARDS.

PIPE BOLLARD DETAIL
N.T.S.



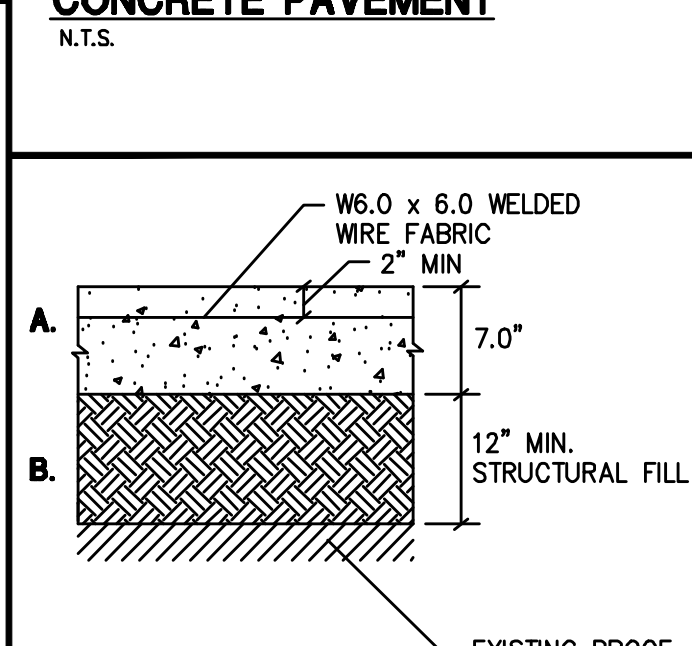
A. 6.0" REINFORCED PORTLAND CEMENT CONCRETE TYPE B (4000 PSI COMPRESSIVE STRENGTH & A FLEXURAL STRENGTH AT 28 DAYS) COMPLYING WITH THE LOUISIANA DEPARTMENT OF TRANSPORTATION SPECIFICATIONS FOR ROADS & BRIDGES. THE CONCRETE SHALL ALSO BE DESIGNED WITH 5-1/2% - 1 PERCENT ENTRAINED AIR. THE PORTLAND CEMENT SHALL ALSO CONFORM TO THE REQUIREMENTS FOR PORTLAND CEMENT CONCRETE PAVEMENT SECTION 601 & 901 OF THE LADOTD STANDARD SPECIFICATIONS FOR ROADS & BRIDGES.

B. 12.0" MIN. COMPACTED GRANULAR STRUCTURAL FILL. GRANULAR FILL SHALL MEET THE REQUIREMENTS OF THE LOUISIANA STANDARD SPECIFICATIONS FOR ROADWAY AND BRIDGE CONSTRUCTION AND BE COMPACTED TO 95% OF THE MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D698.

NOTES:

- 1) ALL STRUCTURAL FILL SHALL BE FREE OF ORGANIC AND OTHER DELETERIOUS MATERIALS, HAVE A MAXIMUM PARTICLE SIZE LESS THAN 2 INCHES, A LIQUID LIMIT LESS THAN 40, AND A PLASTICITY INDEX LESS THAN 18. SANDY CLAY OR CLAYEY SANDS ARE RECOMMENDED FOR USE AS STRUCTURAL FILL. THE ON-SITE SILTY SANDS MAY BE SUITABLE FOR USE AS FILL.
- 2) FILL SHALL BE PLACED IN A MAXIMUM OF 8" LOOSE LIFTS & COMPACTED WITHIN 1% BELOW TO 3% ABOVE OPTIMUM MOISTURE CONTENT VALUE. IF WATER MUST BE ADDED, IT SHALL BE UNIFORMLY APPLIED AND THOROUGHLY MIXED INTO THE SOIL BY DISKING OR SCARIFYING.
- 3) EXISTING SUBGRADE IN PARKING AREAS SHALL BE PROOF-ROLLED WITH A LOADED TANDEM AXLE DUMP TRUCK OR SIMILAR HEAVY RUBBER Tired VEHICLE. SOILS THAT RUT OR DEFLECT EXCESSIVELY UNDER THE MOVING LOAD SHALL BE UNDERCUT AND REPLACED WITH PROPERLY COMPACTED STRUCTURAL FILL.
- 4) ALL SUBGRADE, BASE & PAVEMENT CONSTRUCTION OPERATIONS SHOULD MEET MINIMUM REQUIREMENTS OF THE LOUISIANA DEPARTMENT OF TRANSPORTATION.
- 5) CONTROL JOINT SPACING SHALL BE A MAXIMUM OF 12 FEET. IF SAWCUT, CONTROL JOINTS SHALL BE CUT WITHIN 6 TO 12 HOURS OF CONCRETE PLACEMENT.
- 6) EXPANSION JOINT SPACING SHALL BE A MAXIMUM OF 75 FT.
- 7) DOWELS AT EXPANSION JOINTS SHALL BE 3/4" INCH BARS, 18 INCHES IN LENGTH, WITH ONE END TREATED TO SLIP, SPACED AT 12 INCHES ON CENTERS AT EACH JOINT.

LIGHT DUTY CONCRETE PAVEMENT
N.T.S.



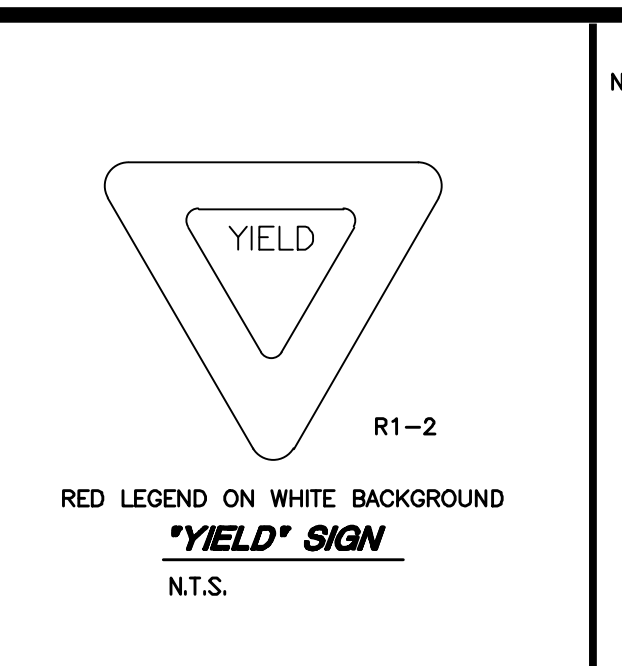
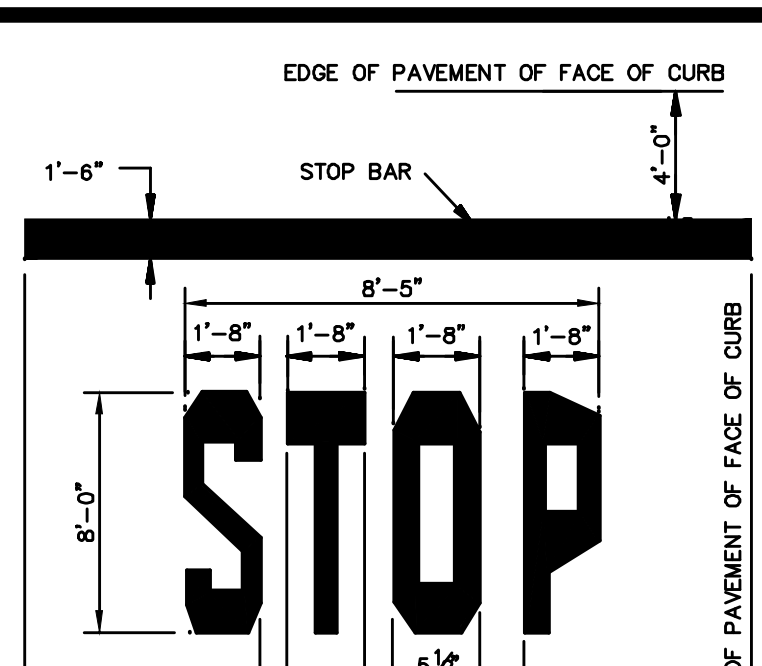
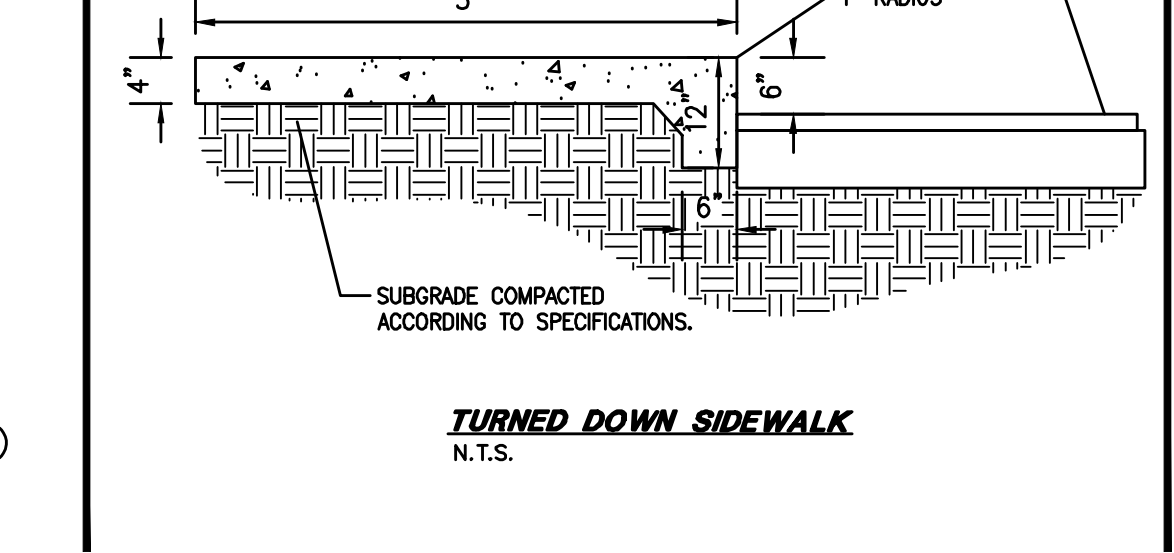
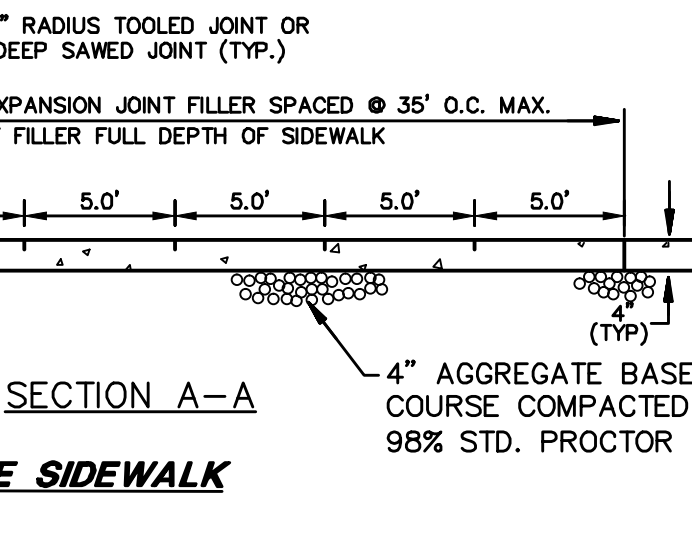
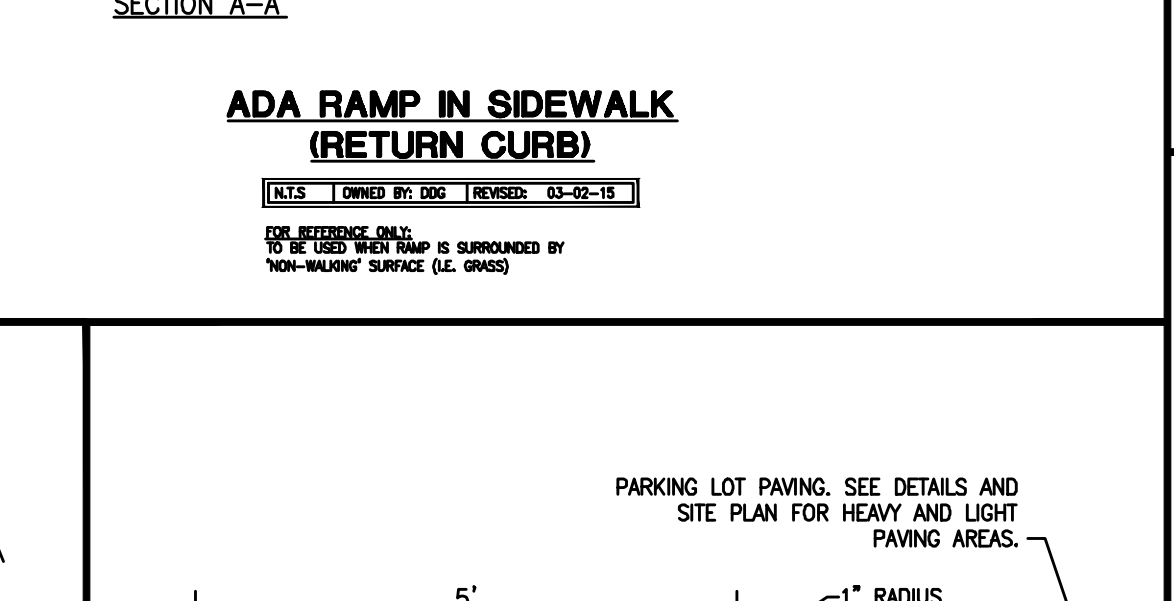
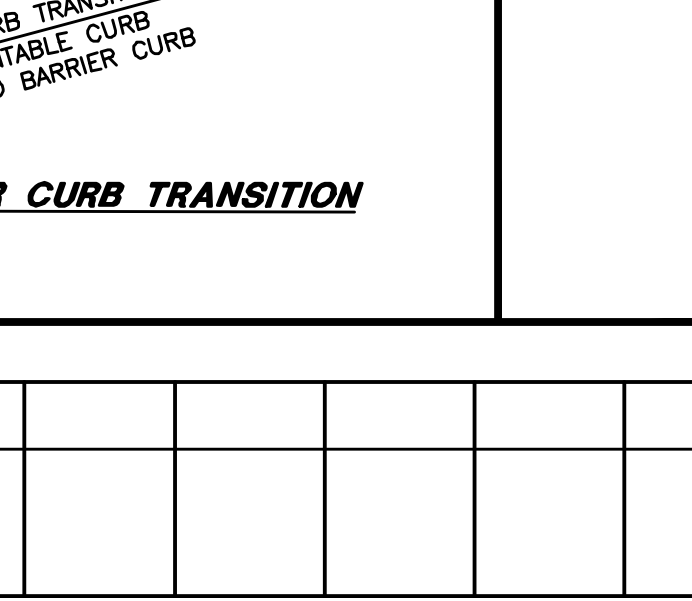
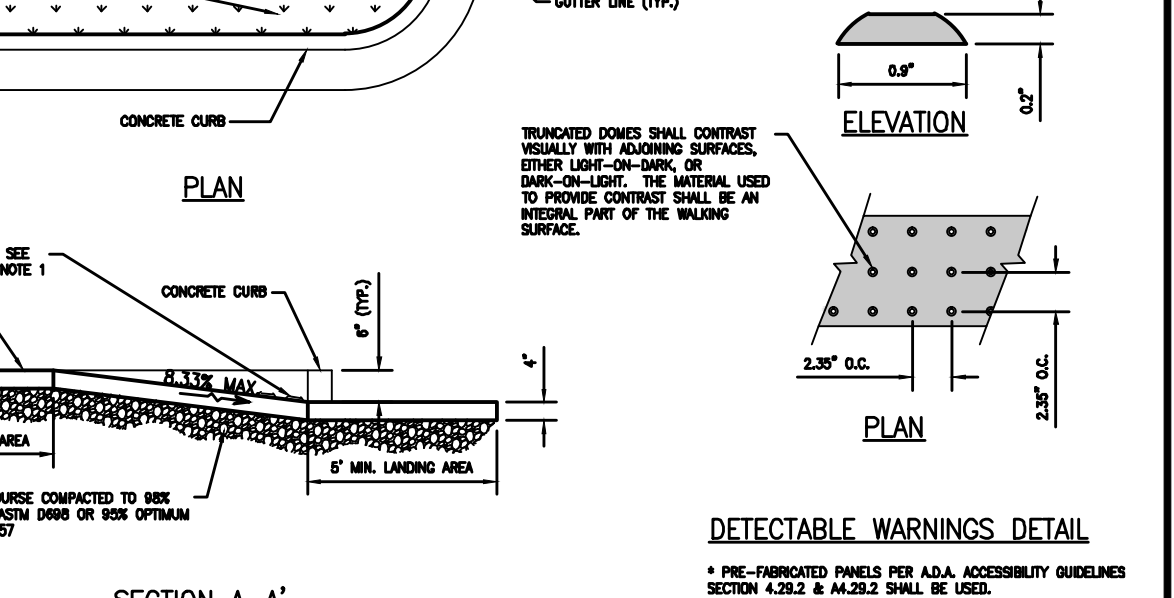
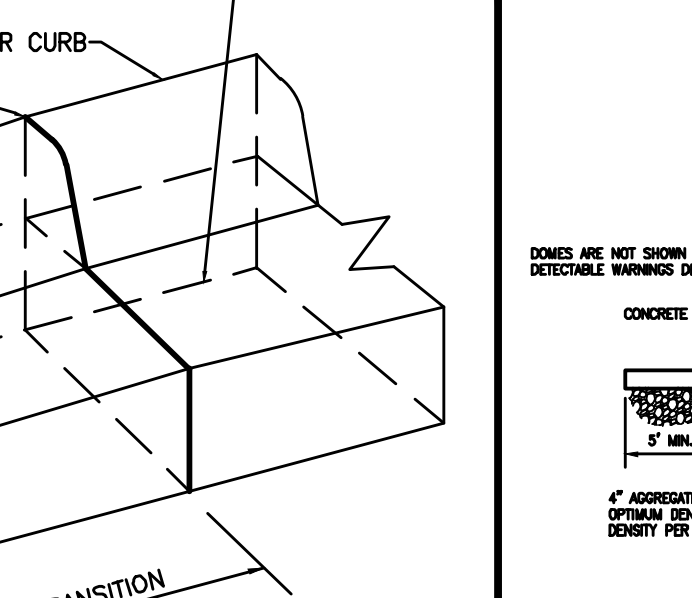
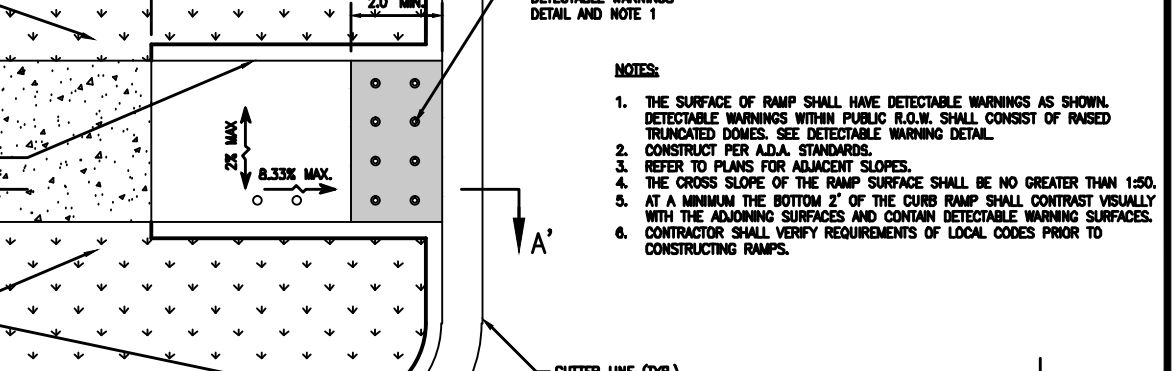
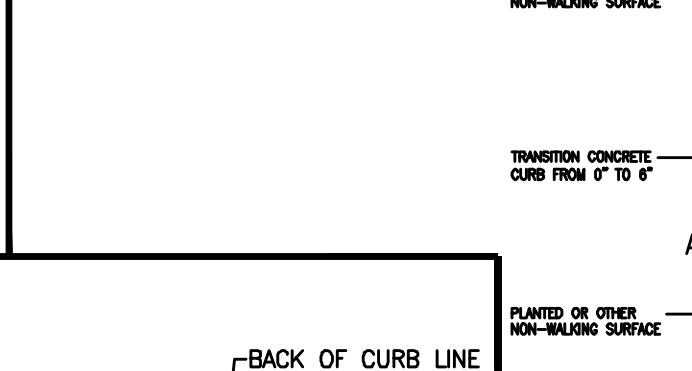
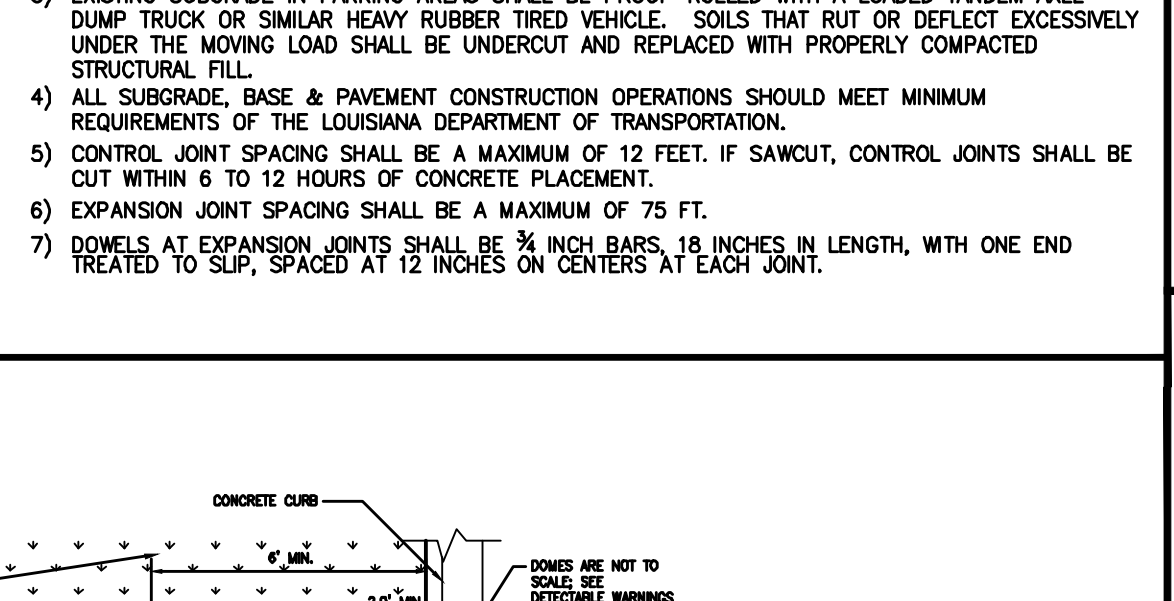
A. 7.0" REINFORCED PORTLAND CEMENT CONCRETE TYPE B (4000 PSI COMPRESSIVE STRENGTH & A FLEXURAL STRENGTH AT 28 DAYS) COMPLYING WITH THE LOUISIANA DEPARTMENT OF TRANSPORTATION SPECIFICATIONS FOR ROADS & BRIDGES. THE CONCRETE SHALL ALSO BE DESIGNED WITH 5-1/2% - 1 PERCENT ENTRAINED AIR. THE PORTLAND CEMENT SHALL ALSO CONFORM TO THE REQUIREMENTS FOR PORTLAND CEMENT CONCRETE PAVEMENT SECTION 601 & 901 OF THE LADOTD STANDARD SPECIFICATIONS FOR ROADS & BRIDGES.

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NOTES:

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HEAVY DUTY CONCRETE PAVEMENT
N.T.S.

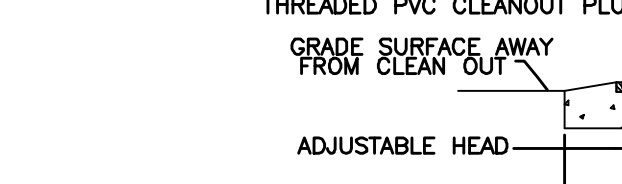
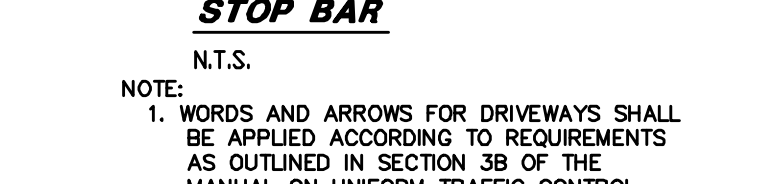


NOTE:

1. WORDS AND ARROWS FOR DRIVEWAYS SHALL BE APPLIED ACCORDING TO REQUIREMENTS AS OUTLINED IN SECTION 3B OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREET AND HIGHWAYS.
2. THESE WORDS AND ARROWS ARE TO BE PAINTED REFLECTIVE WHITE.

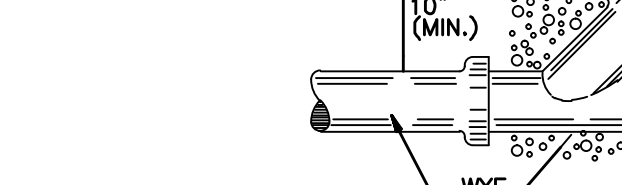
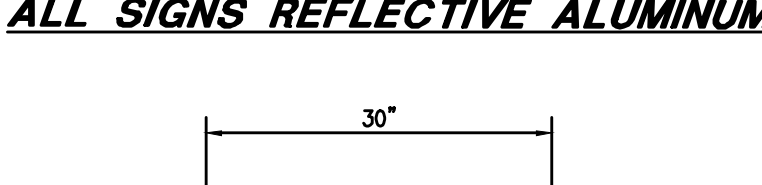
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SANITARY/STORM SEWER CLEAN-OUT
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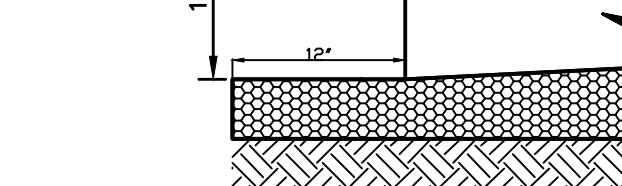
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TYPE "B" CURB & GUTTER
(FOR USE EXCLUSIVELY WITH CONCRETE PAVEMENT)
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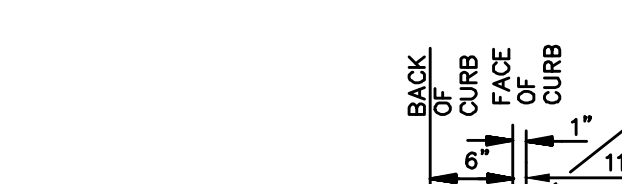
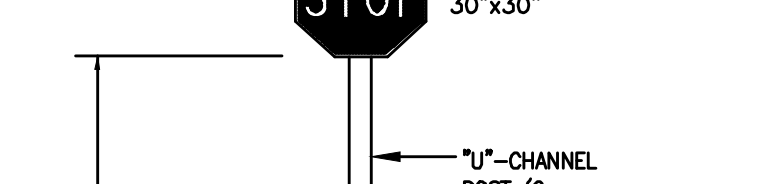
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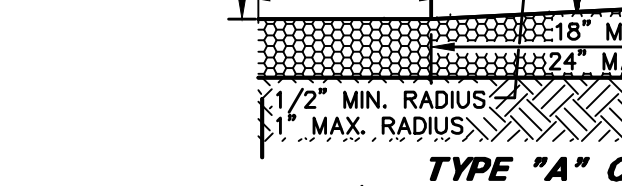
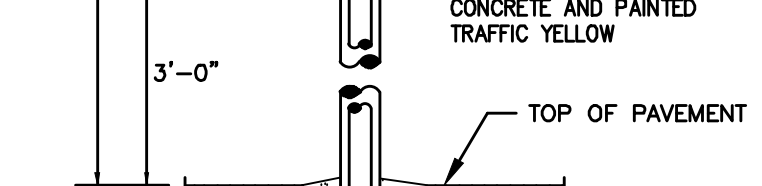
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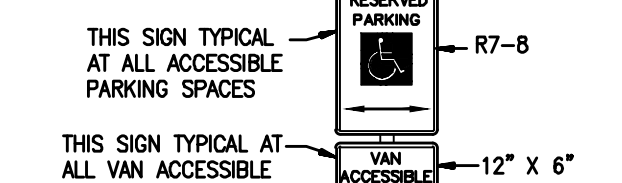
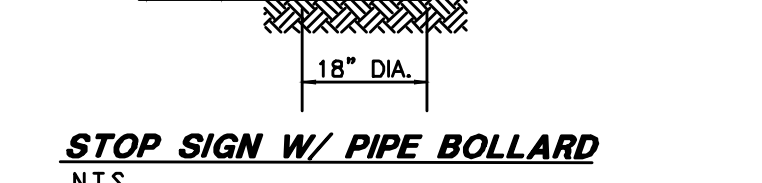
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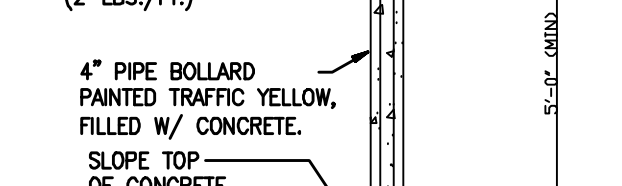
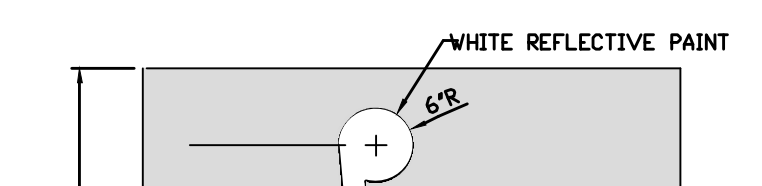
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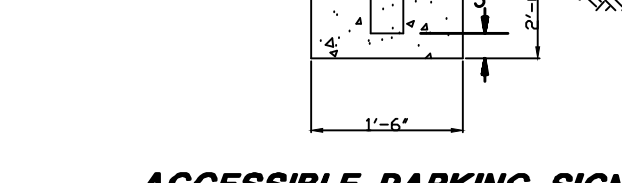
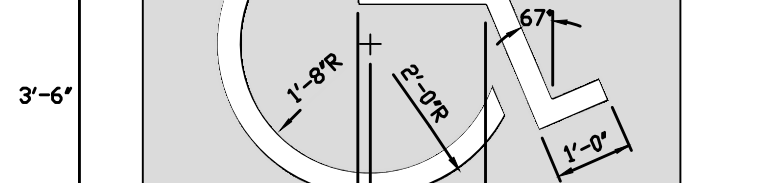
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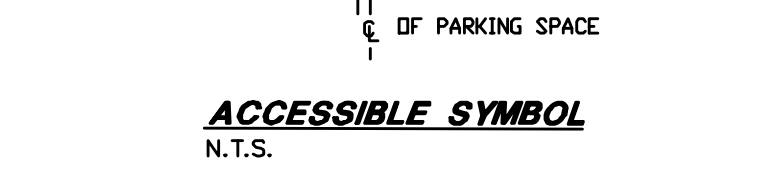
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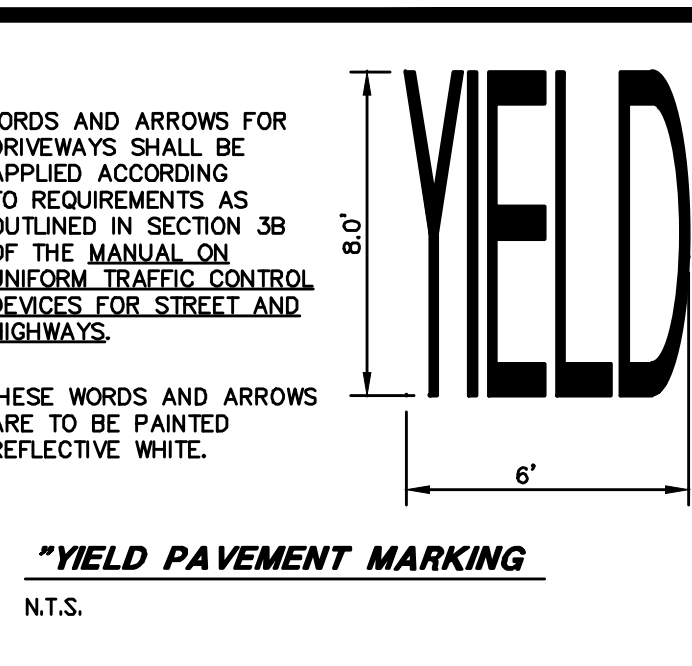
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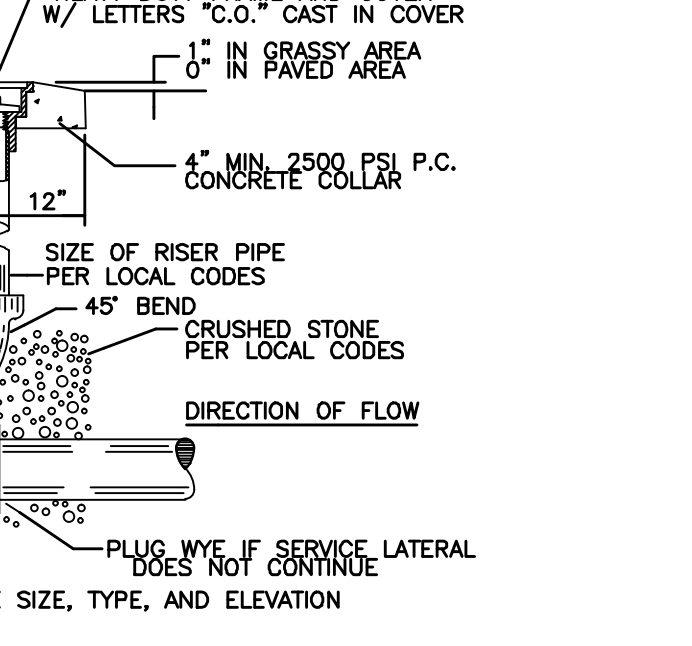


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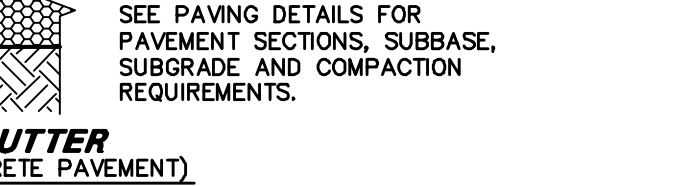
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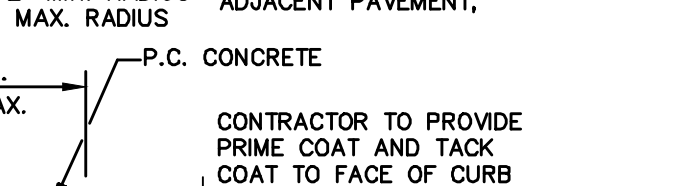
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N.T.S.



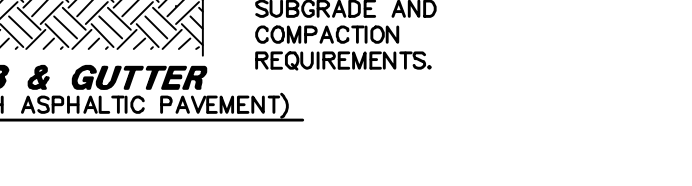
TYPE "B" CURB & GUTTER
(FOR USE EXCLUSIVELY WITH CONCRETE PAVEMENT)
N.T.S.



TYPE "A" CURB & GUTTER
(FOR USE EXCLUSIVELY WITH ASPHALTIC PAVEMENT)
N.T.S.



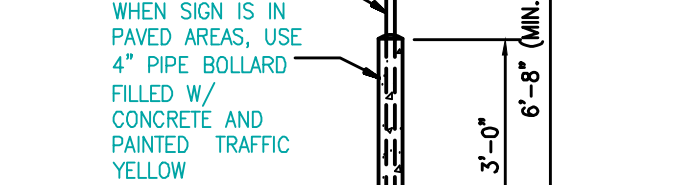
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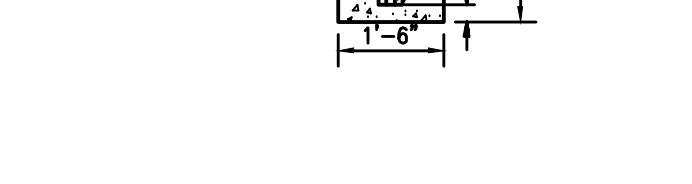
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REVISION	BY
10-28-2015 CONSTRUCTION SET	SMT

DDG
DUPLANTIS DESIGN GROUP, PC
CIVIL ENGINEERING & ARCHITECTURE
34 LOUIS PRIMA DRIVE COVINGTON, LA 70433
WWW.DDGP.COM PHONE: 985-249-6986 FAX: 985-249-6190
THIBODAUX | COVINGTON | HOUSTON | BATON ROUGE | HOUMA | DALLAS

THOMAS H. BUCKEL
PROFESSIONAL ENGINEER
CIVIL ENGINEERING
10/28/2015
DATE

SPRINGS @ RIVER CHASE
COVINGTON, LA
ST. TAMMANY PARISH
FOR CONTINENTAL 339 FUND LLC
COVINGTON, LA

DRAWN DJG
CHECKED THB
ISSUED DATE 10/28/2015
ISSUED FOR CONSTRUCTION
PROJECT NO. 14-697
FILE DETAILS
SHEET C-5.0

DETAIL SHEET